

Godden Mackay Logan
Heritage Consultants



Campbell's Stores, The Rocks Conservation Management Plan

Report prepared for the Sydney Harbour Foreshore Authority
July 2014



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Report Register

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Executive Summary

Background

Campbell's Stores is owned and managed by Sydney Harbour Foreshore Authority, now an agency in the Land and Property Management Authority (LPMA).

This Conservation Management Plan (CMP) reviews and updates a 1996 CMP and a 2004 CMP for Campbell's Stores to reflect the changes that have occurred since then to 2010. The CMP has been prepared as a policy document to guide future planning for the site with regard to its heritage value and to provide clarity on future heritage requirements.

Dates of Construction and Alterations

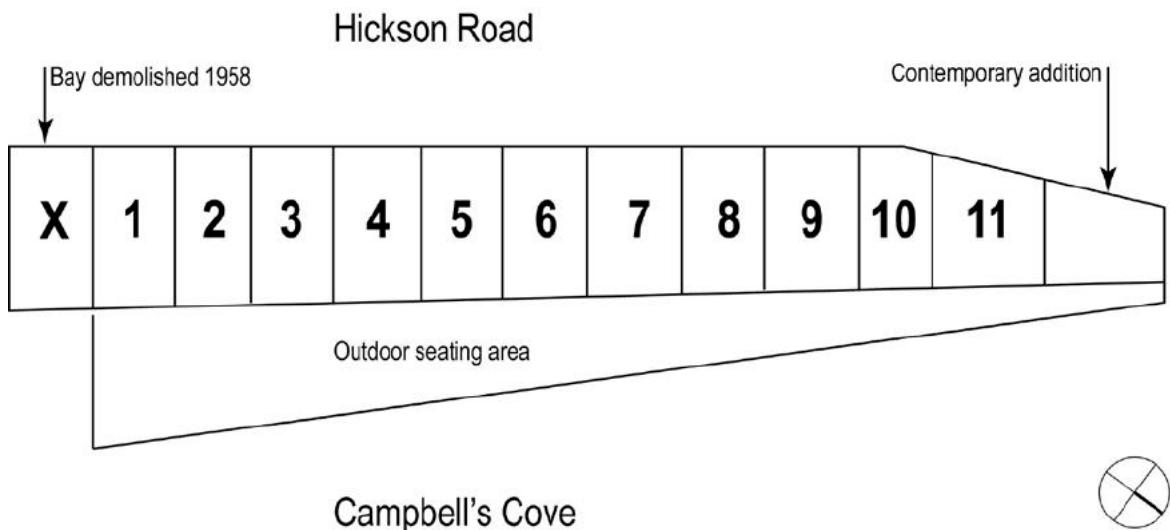


Figure 0.1 Campbell's Stores, diagram illustrating bay numbers used in this report

The first five two-storey bays (present Bays 6–10) of Campbell's Stores were constructed for Robert Campbell (Junior) in 1851–52. A further six two-storey bays (present Bays X to 5) were constructed, possibly in two stages, in 1858–1861. The third level addition was constructed in brickwork in 1882–1887. The Government Printers office was built as a separate building (present Bay 11) between 1895 and 1915. The southern bay (Bay X) was demolished in 1958 to make way for the Overseas Passenger Terminal.

The Stores were originally constructed as a series of discrete spaces, with no connecting penetrations, except on Level 3. The first-floor storage areas were accessed via external timber stairs to each bay.

During the 1970s, major changes were made to the building to change its use from maritime related activities to use as a series of restaurants. These changes corresponded with major changes in The Rocks. The 1970s alterations included many new penetrations, atriums, removal of sections of Levels 2 and 3, stairs and removal of posts. The decaying outer layer of sandstone was removed and refaced. In 1986, the floor levels of the former Government Printing Office (Bay 11) were altered and linkages were constructed via new openings for the fit-out of the Italian Village restaurant.

Statutory Heritage Listings

Campbell's Stores are listed on the following statutory registers:

- NSW State Heritage Register, database number 5053151
- Sydney Harbour Foreshore Authority Heritage Register
- Register of the National Estate (statutory until 2012)

Heritage Management

The challenge for the future of Campbell's Stores is not only the physical conservation of its heritage fabric and movable heritage collection, but how to continue to make the building accessible and meaningful to the community in general and to future users. The conservation policies for Campbell's Stores are underpinned by an obligation to undertake conservation works to the heritage building and movable items, including both repair and cyclic maintenance programs.

A core aspect of the conservation policy vision for Campbell's Stores is to reflect more meaningfully in the use and conservation of the place, and particularly in relation to the movable heritage collection, the working history, contemporary social significance and community interest that has been identified in this CMP. By strengthening the connection between this special place and the people who cherish it, future owners and users can simultaneously benefit themselves and achieve outstanding conservation.

This CMP concludes that the continued use of the building for restaurants is acceptable. However, re-evaluation is required in regard to the outdoor seating area, its location and shading device, the decorative masts and the glass pavilion associated with the Italian Village. These elements are intrusive and detract from the heritage significance and appreciation of the Campbell's Stores.

Therefore, another key aspect of the conservation policy vision is that the low significance and intrusive building elements should be removed and/or replaced with more sympathetically designed additions that complement the original character of the Campbell's Stores, but are distinguishable as new. Also, wherever possible, the original fabric and spatial layouts should be reconstructed in order to reinstate the original spatial qualities of the Stores.

The location of the outdoor seating area further from the façade would provide for a more creative use of the building's spatial volumes and would also provide an opportunity to tell the story of the historic use of the building and machinery more powerfully.

Better integration from Hickson Road via a redesigned staircase on the southern end of the site and a pedestrian walkway in front of the Campbell's Stores would also provide for increased and improved public access to Sydney Harbour and Campbell's Stores and an increasing value to the site in both community and economic terms.

An interpretation strategy needs to be developed and implemented for Campbell's Stores to make the historic fabric and movable objects of heritage meaningful, by connecting them with the intangible aspects of its working history and social meanings.

Recommendations

This CMP recommends the following actions:

Setting

The existing setting of Campbell's Stores does little to evoke the strong historic association between the building and the past maritime activity of this area. The existing awning over the outdoor seating area is a dominant horizontal element which obscures views of the vertical bays of Campbell's Stores and is causing physical damage by directing rainwater onto the façade of the building.

- The outdoor seating area, ship's masts and weatherproof covering associated with the restaurants should be removed. A covered seating area should be constructed beyond Campbell's Stores, towards the water, in a similar configuration to the outdoor seating provisions at East Circular Quay. This would allow for a generous public walkway between Campbell's Stores and the outdoor eating area. It would also facilitate views of the harbour elevation of Campbell's Stores from the public domain.

Interpretation of Campbell's Stores

Interpretation of historic places essentially reveals long-term connections and cohesions which underpin our cultural identity. To 'interpret' a historic place, in its geographic and physical setting, is to bring its history to life to increase the public's understanding, and, through this extended understanding, to give them an enhanced perception of the significance of the place.

Due to its history, significance and setting in The Rocks area, Campbell's Store is capable of being interpreted for promotional and educational purposes.

- An interpretation plan for Campbell's Stores should be prepared, to provide authentic information and tell the story of the historic fabric and movable industrial equipment.
- Interpretation should take into account all the historic phases of Campbell's Store, including the use of the building as a series of restaurants over the past 30 years.
- The building's tenants should be actively involved in developing and implementing an Interpretation Plan for Campbell's Stores.

Interpretation of the Demolished Southern Bay of Campbell's Stores

- The southernmost bay (Bay X), demolished in 1958, could be reinterpreted in an addition. Any alterations to the southern end of Campbell's Stores should provide a clearly legible pedestrian link between the promenade and Hickson Road.

Removal of the Intrusive Temporary Structure at the Northern End of Campbell's Stores

- The existing, intrusive temporary structure at the northern end of Campbell's Stores should be removed. A contemporary, well designed addition, could be constructed. The new design should aim to maintain and enhance the distinctive identity of the Campbell's Stores precinct; it should be consistent with the existing rhythm established by Campbell's Stores; it should recognise the scale and form of Campbell's Stores, and respond sympathetically and it

should use appropriate materials and colours, by using existing materials and colours as a point of reference.

1.0 Introduction

1.1 Context of the Report

In March 2010 the Sydney Harbour Foreshore Authority (SHFA) commissioned Godden Mackay Logan Pty Ltd (GML) to prepare a Conservation Management Plan (CMP) for Campbell's Stores, The Rocks, Sydney, as an update of the one prepared by GML in 2004 (itself an update of the original Conservation Plan prepared by GML in 1996).

This CMP for Campbell's Stores has been structured to fit within the framework of The Rocks Heritage Management Plan (adopted February 2002 and revised 2010). It conforms to the current guidelines of the New South Wales Heritage Council and has been prepared to a standard suitable for endorsement by the Heritage Council and adoption by SHFA. The CMP has been prepared as a high-level policy document to guide future planning for the site with regard to its heritage value and to provide clarity on future heritage requirements, as well as identifying future development opportunities.

Since the 1960s, Campbell's Stores has been recognised as being a rare survivor of the commercial fabric of early-nineteenth-century Sydney. The site's association with Robert Campbell, the first major, free merchant to prosper in Sydney, only adds to its perceived worth. The site is listed on the NSW State Heritage Register (SHR) as item No. 01536.

Between 1970 and 1998 the property was under the control of the Sydney Cove Authority (SCA). The SCA undertook major conservation and building works on the largely derelict property in the mid-1970s. Further refurbishment work, in some cases quite substantial, was undertaken by restaurant lessees to facilitate new restaurant uses. Campbell's Stores have largely been in restaurant usage since that time.

The Campbell's Stores Conservation Plan, November 1996, also included archival photographs of the exterior and interior of the stores. This component of the work was submitted to the SCA as a separately bound volume in September 1996 (Dataworks Ref 179697).

1.2 Objectives

The main objective of this CMP is to provide guidelines for the conservation, re-use, interpretation and management of Campbell's Stores, ensuring the heritage values of the place are maintained and, where appropriate, enhanced.

1.3 Site Identification

The site is located within the area known as Campbell's Cove in The Rocks. The address of Campbell's Stores is 7–27 Circular Quay West, and the site is comprised of the following titles: Lot 3 – DP 264534; Lot 4 – DP 264534; Lot 5 – DP 264534; Lot 6 – DP 264534; Lot 7 – DP 264534; Lot 8 – DP 264534; Lot 9 – DP 264534 and Lot 12-DP 264534. The formal study area is confined to the leased area of Campbell's Stores and adjacent outdoor eating areas. This area is shown in Figure 1.2. However, as Campbell's Stores were intimately connected with operations in Campbell's Cove, this report also addresses the broader context of Campbell's Stores, particularly in relation to how to best interpret its setting within The Rocks.

1.4 Listings

1.4.1 Statutory Listings

Campbell's Stores, 7-27 Circular Quay West, is listed on the NSW SHR (Item # 01536). It is also listed on the SHFA Heritage Register under Section 170 of the *NSW Heritage Act 1977* (the Heritage Act) (Building # 4500031).

1.4.2 Non-Statutory Listings

Campbell's Stores, 7-27 Circular Quay West, is listed individually and as part of The Rocks Conservation Area on the National Trust of Australia Register, where it is identified as 'Campbell's Warehouse formerly Metcalfe Bond Stores' (Item # 6928 and Item # 10499). Campbell's Stores is 'Registered' on the Register of the National Estate (RNE) as 'Campbell's Storehouse' (Place ID # 2122).

1.5 Methodology and Structure

This CMP has been prepared in accordance with guidelines outlined in *The Burra Charter: the Australia ICOMOS Charter for Places of Cultural Significance, 1999* (the Burra Charter), the NSW Heritage Office's guidelines on conservation management documents, and James Semple Kerr's, *The Conservation Plan* (sixth edition) 2004.

The Burra Charter proposes processes and principles for the conservation of an item. The *NSW Heritage Manual* explains and promotes the standardisation of heritage investigation, assessment and management practices in New South Wales. The key methodology of both documents is to identify the nature of any heritage significance as a basis for making decisions which will affect the future of the place.

The initial sections of the CMP provide an analysis of the site and buildings, based on documentary and physical evidence. This analysis includes a historical summary, developing an understanding of the history of the site and place, together with a descriptive analysis of building components and elements.

A grading of significant elements and spaces has been provided to identify their differing levels of contribution to the significance of Campbell's Stores.

The following sections address various management issues, and the role and objectives of the relevant heritage authorities. They provide the framework for the formulation of the conservation policies and implementation guidelines.

Much of the history known about the property was derived from an architectural thesis prepared by C Prince in 1967. Material found in the course of the preparation of the 1996 Conservation Plan allowed a further refinement of Prince's findings, including some alteration of his dating of the erection of the stores. Additional historical research was undertaken for the 2004 revision of the CMP.

1.6 Terminology

The terminology used in this report, when referring to conservation processes and practices, follows the definitions as presented in the Burra Charter. Article 1 gives the following definitions¹:

Place means site, area, land, landscape, building or other work, group of buildings or other works, and may include components, contents, spaces and views.

Cultural significance means aesthetic, historic, scientific, social or spiritual value for past, present or future generations.

Cultural significance is embodied in the place itself, its fabric, setting, use, associations, meanings, records, related places and related objects.

Places may have a range of values for different individuals or groups

Fabric means all the physical material of the place, including components, fixtures, contents, and objects.

Conservation means all the processes of looking after a *place* so as to retain its *cultural significance*.

Maintenance means the continuous protective care of the *fabric* and *setting* of a *place*, and is to be distinguished from repair. Repair involves restoration or reconstruction.

Preservation means maintaining the *fabric* of a *place* in its existing state and retarding deterioration.

Restoration means returning the existing *fabric* of a *place* to a known earlier state by removing accretions or by reassembling existing components without the introduction of new material.

Reconstruction means returning the *place* to a known earlier state and is distinguished from *restoration* by the introduction of new material into the *fabric*.

Adaptation means modifying a *place* to suit the existing use or a proposed use.

Use means the functions of a *place*, as well as the activities and practices that may occur at the *place*.

Compatible use means a use which respects the *cultural significance* of a *place*. Such a use involves no, or minimal, impact on cultural significance.

Setting means the area around a *place*, which may include the visual catchment.

Related place means a place that contributes to the *cultural significance* of another place.

The first bay of the Waterfront Restaurant at the southern end of the site is referred to as Bay 1 and the former Government Printers Office at the northern end of the site is referred to as Bay 11. The lowest floor of the building is referred to as Level 1 and the top floor is referred to as Level 3.

1.7 Authorship

This updated report has been prepared by a specialist team assembled by GML. Susan O'Neill, Associate, managed the project and was responsible for the preparation in updating the assessment of significance, conservation policy and interpretation guidelines. Randa Cotterell and Julian Siu, Research Assistants, assisted with preparation of the report. Geoff Ashley, Senior Associate, reviewed the report.

1.8 Limitations

Physical analysis was undertaken without excavation or physical intervention in the fabric.

1.9 Acknowledgements

The study team wishes to acknowledge the assistance provided by the lessees in making the site available for inspection and for any responses made to the requests for information from SHFA for this CMP.

The assistance of the Heritage & Design team at SHFA is gratefully acknowledged.

1.10 Abbreviations

AONSW	Archives Office of NSW
ASN Co	Australasian Steam Navigation Company
Bk	Book
CCM	Sydney Cove Authority photo reference
CT	Certificate of Title, LTO
DP	Deposited Plan, LTO
LTO	Land Titles Office
LTOD	Land Titles Office, Deeds Registers
ML	Mitchell Library
MLMSS	Mitchell Library, Manuscript
RAB	Rate Assessment Book
SCC	Sydney City Council
SMH	<i>Sydney Morning Herald</i>

1.11 Image Attribution

All photographs included in this report were taken by GML in 2003, unless otherwise stated.

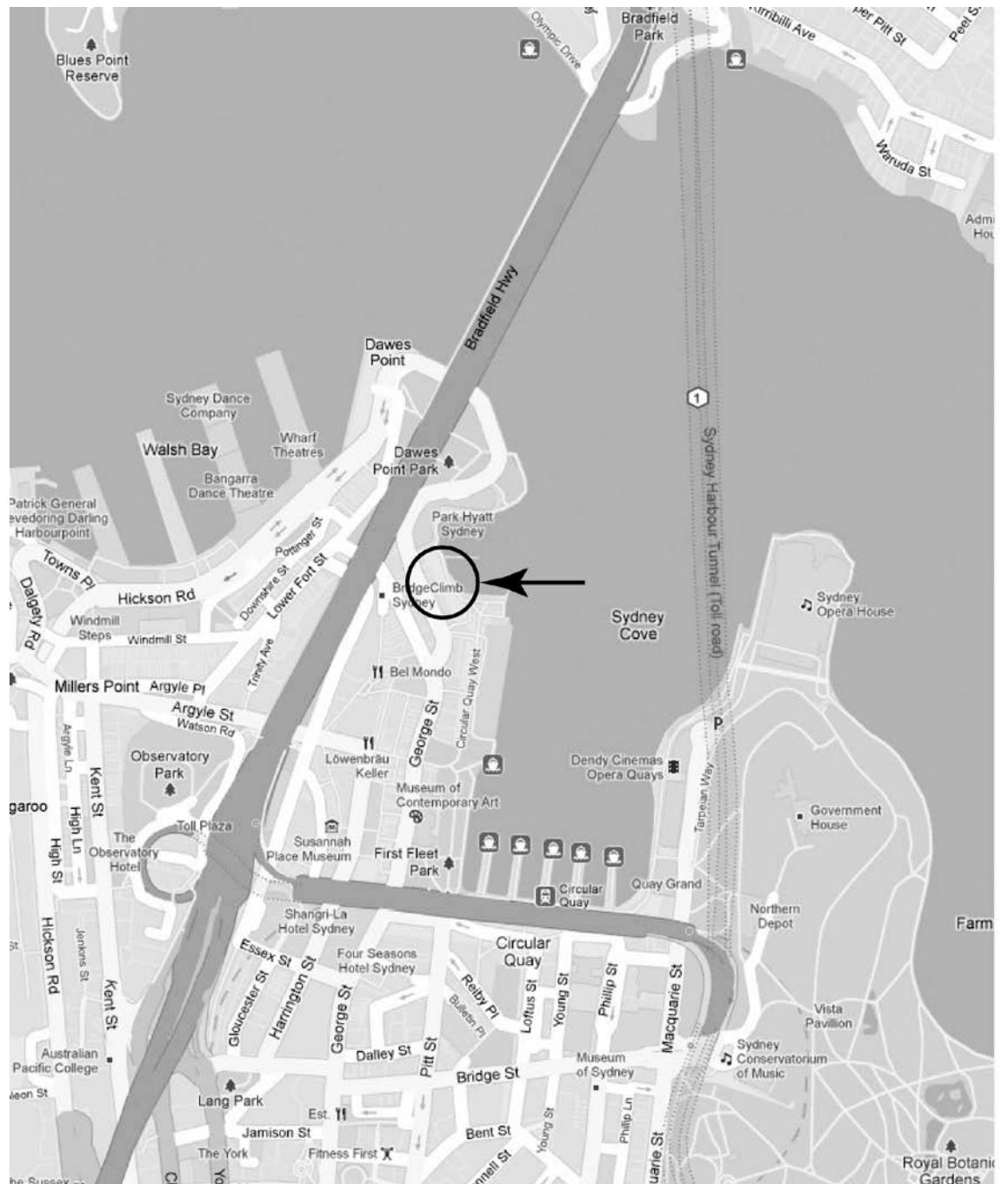


Figure 1.1 Location of Campbell's Stores.

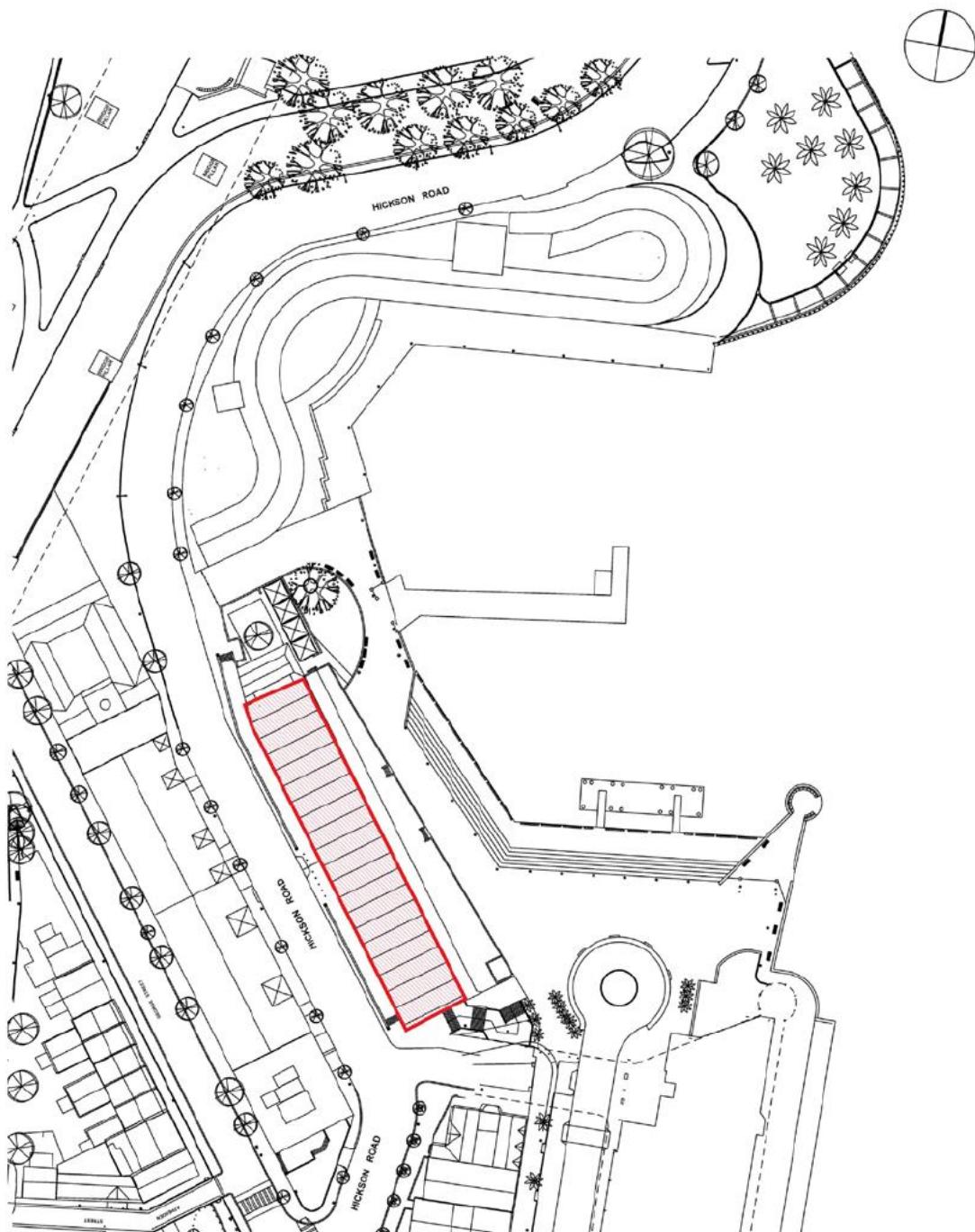


Figure 1.2 Location of Campbell's Stores in The Rocks. Primary study area shown outline.

1.12 Endnotes

¹ Australia ICOMOS Burra Charter 1999, p. 2.

2.0 Documentary Evidence

2.1 Thematic History

In order to better understand how Campbell's Stores developed, this history has been approached thematically; such a framework offers multiple storylines for the place to assist in understanding all of its cultural values. This method of approach can provide contextual patterns and associations, especially in relation to human activities in the environment, which would not be immediately obvious were a strictly descriptive or chronological approach taken.

The NSW Heritage Branch has developed a thematic framework for use in heritage assessment and management. The thematic framework identifies 38 principal themes.¹ The organising principle for the thematic framework is the dynamism of human activity.

The historical development of an area or item can be understood as occurring in a thematic way. A physical illustration of this can be seen when we think about a landscape or building or arrangement of artefacts as a series of layers, each one representing a progressively earlier or later theme, or historical influence. Thinking about a place in terms of themes can help us understand its significance.²

The State historical themes of 'Peopling—peopling the continent' and 'Economy—developing local, regional and national economies', are used in this history to guide research questions, interpret the history and structure the narrative of the development of Campbell's Stores within the context of the development of The Rocks.

2.1.1 Development of The Rocks

Indigenous Sydney—The Cadigal

Archaeological evidence for the Indigenous People of the Sydney Region suggests they have been in the area for 15,000 years.³ However earlier evidence may have been lost when the sea levels and the harbour shoreline rose to approximately its current level around 6000 years ago. The people of the Port Jackson area, around where the future city of Sydney would be built, were of the coastal Dharug (or Darug) Language and were called, erroneously by the Europeans, the Eora. The clan or subgroup who inhabited the southern side of the Harbour called themselves the Cadigal (or Gadigal). The Cadigal's name for The Rocks, or more specifically the western shore of Sydney Cove between Globe Street and Argyle Street, was *Tallawolodah*. Sydney Cove itself was known as *Warrang* or *Warrane* and Dawes Point called *Tarra*.⁴ In May 1788, Captain Arthur Phillip wrote to the Colonial Secretary estimating that the local Indigenous population was 1500 people.⁵

While the evidence may have been destroyed, it is unlikely that the Cadigal occupied The Rocks as a permanent base because of the rugged nature of the topography. They did use the area extensively, however, and flat stones by the shore at Dawes Point were used for cooking fish. In the 1870s a rock engraving of a whale was reported, but it appears to have disappeared when the seawall was constructed around 1880. A campfire dated to the late 1400s was excavated on the site of the Shangri-la Hotel in Cumberland Street in 1989. The campfire was made for a single meal of rock oysters, bream, schnapper and other shell fish.⁶

After the arrival of the Europeans in 1788, the Cadigal people were decimated by introduced diseases, the most notable event being the outbreak of Smallpox in April and May of 1789, which it

is estimated killed at least half of their population⁷. However some Cadigal survived and continued to occupy the area, and although they appear to have largely disappeared by the 1860s a group continued to camp around the boatsheds until they were moved out to La Parouse in 1881.⁸ Some may have stayed, integrated into colonial society or moved back into the area in later years, and Aboriginal men were employed on the docks and wharves and other maritime occupations.

The Convict Settlement (1788–1820)

When the British arrived in the First Fleet in 1788, the land around Sydney Cove was loosely segregated into different areas for the Government, Military, Administration, and the convicts' shelter. Government House was set up on what is now the corner of Bridge and Phillip Streets, where the Museum of Sydney now stands; the Military Barracks were approximately where Wynyard is now; and administrative necessities, such as the Hospital, dockyards, marketplaces and store were built along what is now George Street on the shoreline of Sydney Cove. The convicts were directed to set up their small tents 'on the rocks' of the peninsula on the western side of the Cove. Hence the name 'The Rocks' was coined and it stuck, despite attempts in later years to change it. At the northern end of the peninsula, Lt Dawes built a small observatory, and named the area Port Maskelyne after the Astronomer Royal. The guns from the Sirius were set up to defend the infant colony and later a fort was constructed, Dawes Point Battery, and Dawes Point became the accepted name.

The first huts constructed on The Rocks were of wattle and daub, a simple technique of digging in postholes and intertwining branches between them for a frame, with clay daubed over it to attempt to make the structure weatherproof. These crude dwellings were soon replaced with small cottages and modest houses built of wood, stone and locally produced brick. As the colony grew in size and prosperity, several wealthy merchants and others constructed mansions on the upper levels of The Rocks. At first all the dwellings faced outwards to the harbour, perhaps indicating the reliance on the shipping that was the lifeline of the colony.

Archaeological investigations carried out in The Rocks since the late 1980s have found evidence of the building techniques and lives of the convicts. Although the evidence for the first wattle and daub buildings did not survive later development, the earliest house found in the archaeological excavations dates to 1795. The material culture recovered has given a greater insight into the lives of the convicts who lived in The Rocks and has helped to dispel the stereotype of the convict lifestyle as being desperately impoverished. Many of the convicts who settled in The Rocks were trades people from British cities who were transported for crimes against property such as forgery and theft. After the 1798 Rebellion in Ireland, numerous Irish were exiled and many settled in The Rocks. Regardless of why the convicts found themselves sent to Sydney, the archaeological evidence suggests that they led far better lifestyles here than many could have hoped for in the homelands. Their diet was more varied and included more meat and varieties of fruit and vegetables than their contemporaries at home. They also had the means to equip themselves with household items such as crockery, some of it quite fine and expensive, and to decorate their homes with figurines and exotic curios. Some made substantial fortunes in pursuits such as mercantile activities and shipping, opportunities they may not have had in their homelands.

Although land at Parramatta, or Rose Hill as it was initially known, had been settled soon after the arrival of the First Fleet, the initial concentration of the population was around The Rocks. Free settlers began arriving in the Second Fleet, landing in Sydney in 1790, and land grants further west of Sydney were taken up by them and the convicts to start farming. The concentration of the

population in The Rocks began to thin and those who remained consolidated larger areas around themselves for yards and gardens on permissive occupancies; there were no title deeds. When Governor Macquarie arrived in 1810, he began to formalise the ownership, a daunting task as land had been bought and sold, passed along in families, and leased out. Permanent occupiers of land in The Rocks had to present submissions to the Government to prove their long-term occupancy and/or the legitimacy of their claims to the land. Town leases were set up and people were forced to register the land they occupied and pay a fee to the colonial secretary.

Governor Macquarie also formalised the town layout and named the streets. The main road, High Street, became George Street after the ruling Monarch, King George III. Other streets in The Rocks lost their local names: Church Row, Windmill Row and were replaced with regal or viceregal titles after various Lords and Aristocrats in Britain.

The Port Town (1800–1900)

Although Sydney was founded as a penal colony, it soon became a busy bustling Port Town. Robert Campbell led the way for free merchants; many would make fortunes from the dependence of the colony on goods brought in by ship. Campbell was fundamental in initiating the first export industries by starting the whaling and sealing fisheries. Along with trade to Southeast Asia, the fisheries attracted mariners and The Rocks became the centre of the maritime industry. Soon after the wool industry began to boom, increasing the amount of shipping from Sydney. From the early nineteenth century the waterfront was extensively developed with wharves and warehouses. Numerous men from The Rocks were involved in the maritime industry, either directly employed on ships and boats or employed in jobs that supported the shipping. Many of the Hotels operating through the nineteenth century reflected the importance of the maritime industry, with names such as 'The Whalers Arms', 'Marine' and 'British Seaman's Hotel'.

The arrival of Governor Macquarie saw the formalisation of land ownership and the opening of areas beyond Sydney for free settlers and time-expired convicts. Promises of large tracts of land for well-off settlers encouraged immigration and 'assisted immigration' began in the 1820s. This caused a population boom, and landowners in The Rocks and other parts of Sydney began to subdivide their land and build tenements and terraces. The pressure for housing became greater in the 1850s when the Gold Rushes began. Absentee landlords exploited this and ran up housing that was quick and cheap to build, although unfortunately they did not always keep up the maintenance of their buildings. By the late nineteenth century the population density in The Rocks was among the highest in Australia. The area was considered a slum from around the mid-nineteenth century—a typical waterfront area with its numerous pubs and prostitutes, rundown housing and buildings. Meanwhile Sydney had begun to spread into suburbs with freestanding homes and gardens. This and other older working class suburbs were not considered healthy by the middle classes.

When the Bubonic Plague arrived in Sydney in 1900 it was thought that the densely occupied Rocks would be hard hit. Of the 103 deaths from the plague only 3 were people in The Rocks. The plague had been brought to Sydney by ship and the Government was forced to clean up the waterfront because of the panic the disease caused. The Rocks, Darling Harbour, Millers Point and other waterfront areas were resumed, and the cleansing operation and demolitions of substandard buildings and wharf areas commenced. The Sydney Harbour Trust was formed to bring the waterfront up to world-class standards and along with this came the responsibility to construct workers housing to ensure a local labour force could respond to the irregular hours required on the wharves.

Redevelopment (1900–current)

The newly-formed State Government Housing Board designed and built workers housing in 1911–1913. Rows of new terrace-style houses were occupied by waterside workers and their families. Shops, pubs and other commercial buildings were simultaneously constructed following the resumption. In 1913 Sydney Council opposed the construction of ‘terrace’ housing for being unhealthy; this being the period of sub-urban expansion and the development of the ‘Garden Suburb’ ideal. The First World War halted much of the Government’s redevelopment plans for the area and effectively stopped further demolition.

In the inter-war period (1918–1939) little construction occurred in The Rocks; with notable exceptions of a few pubs (Fortune of War & Glenmore), and some NSW Government offices (Housing Board, Dept of Labour & Industry, State Clothing Factory). Some private factories were also built in The Rocks at this period (Playfair’s butchers, Bushell’s, Cadbury’s). These factories and the waterfront employed locally-based labour. One of the last buildings to be designed, and commenced, was the new offices for the Maritime Services Board (MSB) which, interrupted by World War II, was not completed until 1953 and now serves as the Museum of Contemporary Art.

The construction of the Sydney Harbour Bridge between 1923 and 1932 saw the demolition of some 400 buildings in The Rocks, including housing, shops, churches and pubs. The western side of Cumberland Street, much of Lower Fort Street and all of Princes Street were completely cleared for the building of the Bradfield Highway.

With the exception of the MSB, almost no development occurred in The Rocks in the period from c1932–1970. Further demolition of houses for the Cahill Expressway occurred in 1956–57, along with the removal of Little Essex St (Brown Bear Lane). The other significant construction at this time was the Overseas Passenger Terminal at Circular Quay between 1959 and 1962, serving as one of the post-war immigration gateways to Sydney.

In the 1960s it was planned to demolish all the buildings in The Rocks and build ‘high-rise’. The ‘Wallace Scheme’ was accepted as the successful tender but the plans did not eventuate. The building boom in the 1960s changed the CBD with high-rise construction and land at The Rocks, seen as the gateway to the CBD, became more valuable, although the building stock had been left run down. In 1970 the Sydney Cove Redevelopment Authority was formed with the charter to plan redevelopment:

in an economically feasible way, with a maximum financial return, at the same time taking advantage of its location, setting and historical links.

The local residents, planners, historians and sociologists, with the support of the Builders Labourers Federation, opposed the re-development and in 1973 a ‘Green Ban’ was placed on any demolition in The Rocks, although restoration of buildings was allowed. SCRA was forced to give in to their demands and local residents were re-housed in the area. Whilst SCRA originally planned to restore historic buildings north of Argyle Street, the Green Ban ensured that much more of the historic built environment also remained. The area south of the Cahill Expressway, known as the ‘sacrificial zone’ was redeveloped with high-rise commensurate with the rest of the city from 1979 onwards.

2.2 Historic themes

The *NSW Heritage Manual* identifies a specific set of ‘Historical Themes relevant to New South Wales’ within which the heritage values of the place can be examined. Relevant themes for the Campbell’s Store site are outlined in the table below.

Table 2.1 NSW Historical Themes

Australian Historical Theme	NSW Historical Theme	Campbell’s Store
Developing local, regional and national economies	Industry—Activities associated with the manufacture, production and distribution of goods	Campbell’s Stores were a typical warehouse built for commercial storage purposes by its owners; the Campbell family. The Campbell’s Stores show evidence of the changing role of Sydney Cove as a maritime ‘hub’.
Developing local, regional and national economies	Transport—Activities associated with the moving of people and goods from one place to another, and systems for the provision of such movements	The construction (in 1851–52) and later expansion of the Stores (1858–61) demonstrate the growth in commercial traffic and the increasing needs of Sydney and New South Wales for commercial storage, for goods shipped from overseas for the increased colonial population, as well as for exports out of the country.
Working	Labour—Activities associated with work practices and organised and unorganised labour	Campbell’s Stores were built at Sydney Cove on the site of the first privately-owned wharf in Australia. They demonstrate the proximity of the wharf to the original hub of commerce and international shipping traffic into Sydney: Sydney Cove.
Marking the phases of life	Persons—Activities of, and associations with, identifiable individuals, families and communal groups	The Stores were built for the Campbell family, one of the earliest free immigrant merchant dynasties to be established in Sydney. Although not built for the founder of the dynasty, Robert Campbell, it was erected for the firm he created which was then managed by his sons. Their pre-eminent position as merchants was widely acknowledged and their firm was one of the most prestigious in Sydney.

2.3 History of the Occupation of the Site

2.3.1 Robert Campbell and His Wharf

Robert Campbell was born in Greenock, Scotland, and went to India in 1798 to join his elder brother in the Calcutta business partnership of Campbell, Clarke and Co, into which he was soon accepted as a partner.⁹ The firm became Campbell and Co, after the Clarkes withdrew in July 1799. The firm had sent a speculative cargo to Sydney Cove in 1796. Robert Campbell followed in 1798, with another cargo. He bought land (from John Baughan) at Dawes Point overlooking Sydney Cove and commenced trading gradually building up a reputation as a shrewd but honest merchant.

In September 1801, he married Sophia Palmer, sister of John Palmer, who became Commissary of New South Wales. By November 1801, some of Campbell’s Storehouses were complete, as was a

stone wall and small wharf at right angles to the main warehouse. It was claimed to be the first privately-owned wharf in Australia. In 1802, Campbell and Sophia moved into Wharf House (then incomplete). Beneath their house, vaults to store goods were excavated in the sandstone rock face. John Lewin's watercolour of 1808 showed the jetty completed out from the vaults, a two-storey storehouse and an access road to Wharf House running behind this jetty.

Governor Lachlan Macquarie was anxious to grant land to settlers who were building large and substantial improvements on their Sydney leases.¹⁰ Hence on 29 June 1814, Robert Campbell was granted 3 acres, 3 roods, bounded on the south by the premises occupied by the Naval Officer, on the southwest by a road leading to Dawes Point Battery, and on the east by Sydney Cove, 'in consequence of his having erected thereon several large and expensive Buildings'. A right to make streets was reserved to the Crown.¹¹

In May 1807, Campbell had been appointed Naval Officer and a magistrate. As a result of his sympathy for Governor Bligh, he was marked for persecution by the rebel administration after Bligh was deposed. Campbell was later restored to full control of his business by the arrival of Governor Macquarie in 1810, but he was forced to leave it in the hands of Charles Hook, when he went back to Britain to give evidence at the trials following the rebellion. He returned on 18 March 1815 to find his business bankrupt and many of his ships wrecked. He sought compensation from the government.¹² Campbell operated as a commission agent until he was able to operate as merchant again.¹³ Sophia, his wife, was an accomplished painter and despite the vicissitudes of Robert's career, continued to paint. In 1817, she completed a work titled 'Sydney in all its Glory', a view of Sydney Cove from Dawes Point, which showed her family's land and wharfs in the foreground.¹⁴

On 4 January, 1822 Campbell formally received compensation from the government for the loss of his ships while he was in England from 1812–15, and the money may have helped fund work on warehouses he built along the edge of the water. In June, 1822 Campbell advertised for masons to work on his wharf, warehouses and sea wall. These warehouses were all complete by 1825 and they are shown in Stewart's 1825 copy of Harper's map of 1823 which shows no Campbell's Stores (Figure 2.1).¹⁵

Robert Campbell (Senior) signed a partnership agreement with his sons John, Robert, Charles, George who were gradually coming into the management of the firm in 1828. By about 1830 his son, John Campbell, had virtually taken over the business.

Robert Campbell objected to government gangs making a road through his land on 17 September, 1831. He claimed that the land he bought in 1799 only had provision for a road 66 feet wide to Dawes battery.¹⁶ On 3 August 1832, he supplied a sketch of the land to which he was entitled. The plan shows his house and the stores along the High Water Mark.¹⁷ A sketch in the Surveyor-General's sketch book shows the original land granted to Campbell and the store along the front of the grant, as in Stewart's 1823 plan (Figure 2.2). This plan was drawn as part of the investigation into Campbell's title for his land.¹⁸ Robert Campbell successfully upheld his right to his land and his objection to the widening of George Street in this spot, and this objection was successfully used by his heirs and then his successors in title to refuse the widening of George Street here without compensation, a refusal which hamstrung government attempts at road-making in this vicinity.¹⁹

Equipment for the wharf was also acquired. The Magistrates Returns of Manufactories, Mill etc, in 1831 showed Robert Campbell senior, of Campbell's Wharf, has one hydraulic pump at wharf.²⁰ By 1838, R Campbell & Co was listed as possessing one crane.²¹

John Campbell officially took over as head of the firm in 1836, but it occurred when the firm was in financial difficulty. On 6 May 1836, Campbell's Wharf was advertised for sale in the press.²² It was followed by an injunction to prevent the sale of Campbell's Wharf shortly afterwards.²³ A press report of a case in the Supreme Court of Campbell versus Hartwell and others, reported that Campbell had obtained an injunction preventing the defendants from selling his wharf under the power of sale granted to them in a legal deed. Although the nature of the deed was not specified in the report, it is likely to have been a mortgage held by Hartwell and others over Campbell's Wharf. A clause giving the mortgagees power of sale is a standard insertion in any mortgage deed. There was some legal argument in the court regarding the validity of the competing claims of the parties and the case was adjourned. No further press report of this case has been located.²⁴ Whatever further action was necessary seems to have been successful, since the wharf remained in the hands of the Campbell family.

Although control of the firm was largely in the hands of Campbell's sons, the title to the land on which the wharf was situated was in Robert's name, and so he continued to be involved in matters regarding the land. On 21 January 1841 Robert Campbell formally applied to the Colonial Secretary for permission to enlarge part of his wharf so that ships could unload at low tide, by using a large rock which could not be removed by dredging as the foundation for the enlargement of the wharf. The Colonial Engineer, George Barney, minuted that he could find no objection to the enlargement of the wharf.²⁵ Over the years, the area of Campbell's Wharf increased as more land was reclaimed. Much of the site occupied by the Campbell's Stores is in fact reclaimed land.

A lithograph showing Campbell's Wharf by JS Prout published in 1842 showed no sign of the current Campbell's Stores (Figure 2.4). The accompanying text noted that 'Campbell's Wharf in particular, which forms the principal object in the picture, has been sketched with minute attention to particulars'.²⁶

On 29 June 1843, Robert Campbell, of George Street, Sydney, mortgaged the wharf to The Australian Trust Company for £10,000 for three years with interest. The area was specified as being 3 acres 3 roods as in the 1814 grant.²⁷ The need to mortgage the wharf indicated two alternative strategies being applied by the family to their interest. One was that they were mobilising all their available assets for improvements to their property and business to create a better liquidity. Alternatively, the dating of the mortgage, in the wake of one of the most devastating financial depressions in early Australia, may indicate an attempt to salvage their business using their land as a source of working capital. Whatever the reason for the initial mortgage, it was renewed again and again in following decades, so that the Campbell family did not hold complete equity in the wharf again. There eventually appears to have been some dispute between the Campbells and later mortgagees who took over the loan in later years. Only in 1877, after taking the matter to the Supreme Court, were the Campbells to regain possession of Campbell's Wharf again.

Robert Campbell was not to know anything of these travails regarding the wharf. On 11 October 1845, he drew up his will leaving his property in six parts to be divided among his sons John, Robert, Charles, George, and daughter Sophia Ives Campbell and Arthur Jeffreys, the husband of his daughter, Sarah.²⁸ On 15 April 1846, Robert Campbell (Senior) died at Duntroon.

2.4 Development of Building Components²⁹

The documentary and physical evidence suggests five key phases of site ownership and associated development of Campbell's Stores and its setting within Campbell's Cove. These phases of development are associated with four different periods of ownership. The development phases are:

- The early Robert Campbell development of the site from 1801 to the 1830s;
- The later period of the Campbell family ownership under John Campbell 1830s–1875 during which the first part of the current Campbell's Stores were constructed in 1851–1852;
- The Australian Steam Navigation Company development of the site 1875–1887;
- NSW Government (including Sydney Harbour Trust and Maritime Services Board) 1887–1972; and
- Sydney Cove Redevelopment Authority, Sydney Cove Authority and now SHFA management 1973 to present.

The urban landscape that existed during each of these periods is shown graphically on Figure 5.1. This figure indicates how the historical features of the different periods of ownership are reflected in the built form around, as described below.

2.4.1 Site Development Phase 1: Robert Campbell, 1801–1830s

This phase, shown on Figure 5.1 at 1820, was one of establishment. Development included the first private wharf in Australia, the substantial Wharf House of Robert Campbell and several store buildings in a strip of land along the water's edge. The phase included a small degree of dredging and reclamation associated directly with the wharf and store.

2.4.2 Site Development Phase 2: John Campbell, 1830s–1875

This phase, shown on Figure 5.1 at 1865, was one of consolidation and then expansion during the gradual transfer of site management from Robert Campbell to his two sons John and Robert Campbell (Junior). This phase resulted in a largely increased area of reclamation and new wharfage to Campbell's Cove. The construction of Campbell's Stores in 1851–1852 and extension by 1861, themselves provide evidence of confidence on the part of the Campbell family. The construction of Campbell's Stores as a second generation stores building towards the end of this phase probably ensured their retention and use into the subsequent phases of ownership.

2.4.3 Site Development Phase 3: Australian Steam Navigation Company, 1875–1887

This phase, shown on Figure 3.1 at 1887 at the very end of the ASN & Co ownership, was one of significant capital investment. While the size of Campbell's Stores did not change during this period, the extent of wharf development and other building construction in the area (including the nearby ASN & Co building itself) reveals the large extent of ASN & Co investment in the area and the degree to which the setting around Campbell's Stores was being infilled, particularly with the storage sheds near the two Company wharves.

2.4.4 Site Development Phase 4: NSW Government/Sydney Harbour Trust/MSB, 1887–1972

This phase, shown on Figure 3.1 at 1920, was one of major reconstruction where the wharves and ASN & Co timber stores were replaced by new buildings constructed by the government (and in the case of the Norddeutscher Lloyd wharf, paid for by private owners). The two smaller timber store buildings in front of Campbell's Stores were removed by the Trust or the MSB some time around World War II, probably as a result of increased truck traffic in this area.

2.4.5 Site Development Phase 5: SCRA – SCA – SHFA, 1972 to Present

This phase has been one of major change in use from maritime related uses to leisure related uses and is also associated with a period of major change in The Rocks that occurred from the 1970s. The built form of the setting around Campbell's Stores is associated with the works undertaken during the Bicentennial in 1988.

2.4.6 Historic Development of Phases 2–5

Phase 2 History: John Campbell, 1830s-1875

Robert Campbell (Senior) died on 15 April 1846. On 16 February 1847, Charles Campbell Esquire of Queanbeyan conveyed to John and Robert Campbell (Junior) his share in the will of his father to cover a sum of £15,000 owed to the firm of Campbell and Co.³⁰ On 15 April 1847, John Campbell, and other members of the family, conveyed all of their property to James Norton for the benefit of the six members of the family. The 3 acre and 3 rood grant on which Campbell's Wharf was situated, alone out of all the myriad parcels of land, was to be held by them as tenants in common, rather than as joint tenants. This meant that although the share was held as an undivided one, each individual could trade-in his or her share and could devise it by will.³¹

The mortgage of June 1843, taken out by Robert Campbell (Senior) with the Australian Trust Company, was transferred between a number of parties until, by 1847, it came into the hands of the Liverpool and London and Globe Insurance Company of Sydney.³² With periodic renewals of the trusteeship of the company by various individuals, the company continued to assert its interest over the property.³³

In the meantime, the brothers needed access to further liquidity to underpin their commercial transactions. On 26 June, 1848, John Campbell and Robert Campbell (Junior) and his wife Ann Sophia, mortgaged the wharf to the Union Bank of Australia, to cover £6,691/6/10 owed to the bank on Bills of Exchange and in preparation for a further loan of £6,000. This deed provided new and enlarged dimensions of the wharf. It was described as bounded on the southwest by Lower George Street along a wall, N 36.1° W, 10 chains and 4 links, on the northwest by Robert Cunningham's land along a wall, E 31.75° N, 4 chains 20 links, on the east by Sydney Cove and on the south by Crown land W 3° 15 ' S, 3 chains.³⁴ It was not until 29 August 1853, after the construction of the first part of Campbell's Stores, that the loan to cover the Bills of Exchange was paid off and the Union Bank re-conveyed the title of the wharf to the Campbells.³⁵

Within a few years, the commercial prospects of the partnership had grown so much that they were able to erect new stone warehouses which were to become known in time as Campbell's Stores. Between 25 August 1851 and 27 September 1852, the Sydney City Council Rate Assessment Book shows that on Campbell's Wharf, J Campbell had added five stores which were built of stone, with

slate roofs, all provisionally assessed at a net value per annum of £30.³⁶ These are the first five bays of Campbell's Stores.

Other evidence coincides with the data in the Rate Assessment Books. A sketch in Conrad Martens' Album dated at between 1835 and 1851 shows five bays of the warehouses.³⁷ Prince has used this illustration to argue for an early date for the erection of the stores, even one as early as 1838, but coupled with the rate information it tends to fit with the later date. Additionally, the effects of the 1850s gold rushes may also explain the failure of three maps from this decade to show any evidence of the new warehouses and only to show the older line of warehouses.³⁸

It would be tempting to attribute the construction of the stores to the discovery of gold in New South Wales on 12 February 1851, and the subsequent rush and boom conditions which ensued, as major factors inspiring the erection of these warehouses. However, that would be too simplistic an explanation. Although the wool industry suffered dramatically in the 1840s depression in New South Wales, and there is ample evidence to suggest that Campbell and Co also suffered, the later years of the 1840s saw a steady recovery that was accelerated in some ways and hampered in others by the 1851 discovery of gold.³⁹

According to Prince, another four stores were added to the five extant Metcalfe warehouses between 1854 and 1858.⁴⁰ However, this is not the case as shown by the rate evidence. The 1858 Rate Assessment Book shows that the three-storey stone warehouses on the water's edge from the 1820s were still extant. Among the array of other wharf buildings were five warehouses of stone with slate roofs, built as two storeys with two rooms in them. Three operate as warehouses while two are conducted as a combined warehouse and office, with four rooms.⁴¹ Soon afterwards, construction of additional warehouse bays commenced. Prince referred to Jevons' photograph of 1858 which showed eight stores in the Metcalfe Bond Store, plus the four-bay, three-storey warehouse to the north, as well as the original warehouse.⁴² This photograph has been missing from the Mitchell Library copy of this album since October 1975 and was hence not available for viewing.⁴³ Another photograph from the MF Moresby Album dated between 1856 and 1860 showed ten bays with the base of the eleventh bay under construction and awaiting its roof, as well as what appear to be construction materials in front of it (Figure 2.5).⁴⁴ The construction of the additional bays is confirmed by the 1861 Rate Assessment Book which shows that a further six bays had been added to the five bays of the warehouse which were extant in 1858, making eleven bays in all. All bays are of two storeys with two rooms in each.⁴⁵ This work is further confirmed by the 1865 Trig Survey of Sydney, completed by the City Council as the prelude to constructing sewers (Figure 2.7). All maps up to 1865 only show the earlier range of stores at the waterfront and those partially across this land. The 1865 Detail Survey held at the NSW Archives Office exists in two versions. The black and white version shows the older layout of buildings on the site with no Campbell's Stores, while the coloured print shows the current Campbell's Stores.⁴⁶

As construction work on the stores was being completed, Robert Campbell (Junior) died at Duntroon, (30 March 1859) leaving his elder brother John in charge of the firm.⁴⁷

A wide range of tenants took up warehouse space at Campbell's Wharf, some of them in Campbell's Stores. In 1858–59, Sands Directory lists the following at Campbell's Wharf: Campbell & Co; Sugar Company's stores; JC Dibbs & Co, commission agents and wharfingers; Robert Nash, storekeeper; WH Eldred, Capt, Chili Flour Co; Chilian Consulate—Consul, WH Eldred.⁴⁸ In 1861, it shows: Campbell & Co; Colonial Sugar Refining Co, stores; Peruvian guano stores; George Lloyd & Co stores; Robey & Co's stores; George Lewis custom house officer; and WH Eldred, broker &

general agent.⁴⁹ In 1863, it shows: 4. Merry Willis and Co, merchants; 3. Henry Fisher & Son, sugar factors; 2. Brown & Co, merchants; 1. EM Sayers & Co, merchants; Robert Nash; WH Eldred, merchant; 4. Joseph Kendall, marine surveyor; JA Buttrey & Co, merchants; 5 and 6, Daniel Thacker & Co, merchants; 22 Campbell's stores; and John Campbell, merchant.⁵⁰

Commercial tenants continued to occupy the stores. A photograph of 1870 showed the eleven bays of the completed warehouse (Figure 2.8).⁵¹ As late as 1875, the cemetery which had been established on part of Campbell's land in the 1790s, before he took possession of the site, was still visible at the rear of the store, where a tombstone of a marine, John Jones, who died in 1792, could be made out.⁵²

A description of Campbell's Wharf in November 1875 stated that it had a landing area of 100 feet which was backed by 22 very strong stone stores which, along with two sheds on the wharf, could hold 22,000 tons. An accompanying engraving based upon some of the extant photographs showed eleven bays of the stores which are two-storeys high (Figure 2.9).⁵³

Phase 3 History: Australian Steam Navigation Company, 1875–1887

Negotiations for the purchase of Campbell's Wharf by the Australasian Steam Navigation Company appear to have been well advanced by early 1876. The company's old wharf at Sussex Street had become too small for its expanding trade and it needed a newer more central one.⁵⁴ On 18 February 1876, the company applied to the Minister of Lands to extend Campbell's Wharf, which they had recently purchased, by running out jetties on piles into the harbour. An accompanying diagram showed that they had already reached the specified limit for reclamation from the harbour. Their application was approved on 1 May 1876.⁵⁵ A plan was drawn by Surveyor Wansborough of reclaimed land claimed for purchase by Australasian Steam Navigation Company on 19 October 1877.⁵⁶

In May 1876, the Australasian Steam Navigation Company was stated by the company secretary to have bought Campbell's Wharf for £100,000.⁵⁷ However, the legal conveyance was not signed for some months, so it is likely that this was the date when the contract of sale was signed. Before the sale could be finalised there were some impediments in the title which the Campbells had to eliminate, most notably the outstanding mortgage from June 1843, now held by the Liverpool and London and Globe Insurance Company of Sydney.

On 4 November 1876, all the beneficiaries of the will of Robert Campbell (Junior) conveyed their interest in the wharf to John Campbell and Robert Campbell Close for 10/-. All had reached the age of 21 years old. The area of the wharf was given as 5 acres and 12 perches.⁵⁸ They were to undertake most of the tidying up of outstanding matters which needed to be resolved before the company would accept the title.

John Campbell applied for the purchase of reclaimed land in front of Campbell's Wharf, measuring 2 roods 5 perches on 6 January 1877. The plan by JF Mann, dated 4 January 1877, which accompanied the application showed the water frontage and some buildings but not Campbell's Stores. The plan showed the original High Water Mark as being in front of the stores. Since no limit had been laid down for the High Water Mark in this area, the application was accepted.⁵⁹

Tidying up the matter of the June 1843 mortgage was also successful. John Campbell and Robert Campbell Close had brought an action in the Supreme Court, Equity Jurisdiction to have the land affected by the mortgage by Robert Campbell (Junior) to the Australian Trust Company in June 1843 and now held by the trustees of Liverpool and London and Globe Insurance Company to be

vested in them. The Supreme Court ruled in their favour. On 23 March 1877, the Vesting Order placed the property in their hands.⁶⁰ Some days previously, on 15 March 1877, the trustees of the Liverpool and London and Globe Insurance Company had conveyed their interest in the wharf to John Campbell and Robert Campbell Close.⁶¹ With all loose ends tied up, the formal conveyance to the company could proceed. It was completed on 4 April 1877 with the deed specifying an area of 5 acres 12 perches, with the dimensions as in the deed of June 1848 deed, for a total sum of £100,000.⁶²

A detailed press report of November 1877 outlined the changes made by the company to Campbell's Wharf. It built a new wharf, under the superintendence of Thomas Macredie. A wharf 320 feet long was under construction with two jetties 250 and 350 feet long. A new road 50 feet wide was planned to join George Street running in front of the Mariner's Church. For construction of the wharfs, turpentine was used for all timber exposed to seawater, while ironbark and other hardwoods were used for the braces, beams and planking. A seawall was being built from stone quarried from the company's old works at Pyrmont, as was stone for its new offices. An accompanying illustration (Figure 2.10) showed Campbell's Store with eleven bays and two storeys.⁶³ A proposal of 1878 to extend George Street North through the land of the Australasian Steam Navigation Company appears to have been linked with the development by the company.⁶⁴

Between 1877 and 1878, the tenancy of the stores changed. In 1877, they were called 'Campbell's Bonded Stores'. In 1878, the Sands Directory showed the Australasian Steam Navigation Company's wharf here. On the site was 'The Metcalfe Bond and Free Stores' with D Murray as the warehouse keeper. The stores retained the name 'Metcalfe Stores' thereafter.⁶⁵

John Campbell was permitted to receive a Grant by Purchase of Reclaimed Land under the Crown Lands Act of 1 acre 1 rood and 22 perches of land reclaimed from Sydney Cove in front of Campbell's Wharf for £100, and the grant was issued on 17 December 1878.⁶⁶ On 4 July 1879, he formally transferred the title to the reclaimed land to the Australasian Steam Navigation Company.⁶⁷ In October 1879, the Peninsular and Oriental Steam Navigation Company leased the southern part of the wharf at £1,750 per annum.⁶⁸ On 15 February 1881, a further lease of part of the wharf to the Peninsular and Oriental Steam Navigation Company was signed.⁶⁹

Parts of the land that had formed Campbell's property were hived off. In January 1880, a strip of land was sold to the City Council for £4,000 to widen George Street North.⁷⁰ Land facing George Street was put in the hands of various auctioneers for sale. Richardson and Wrench offered 19 lots between Horse Ferry Road and George Street for sale on 15 October 1880. To the east were shown 'Metcalfe Bonded and Free Stores' on the sale plan.⁷¹ The company secretary reported that in May 1881, the Australasian Steam Navigation Company had sold 'Campbell's Garden' for £25,000.⁷²

The company let the former Campbell's Stores, now known as the Metcalfe Stores, to a variety of tenants. In 1882, J Upward appeared in the Sands Directories as the proprietor of the 'Metcalfe Bonded Stores'.⁷³ A photograph of 'Metcalfe Bonded and Free Stores' dating from the early 1880s (Figure 2.11) shows J Upward as the proprietor while the building still had two storeys.⁷⁴ When a survey of the area was completed by Charles Bullock for the Surveyor-General in December 1882, his plan showed the buildings as 'Bonded and Free Stores' with the Branch Store of the Government Printing Office at the northern end (Figure 2.12).⁷⁵

In September 1884, the Australasian Steam Navigation Company accepted a contract for £30,000 for the building of new offices and four stores on the site of Campbell's old house.⁷⁶ The stores

known as Campbell's Stores were not demolished, probably since they were still relatively modern in layout and design being then only a little over thirty years old. Probably to enable them to finance the work, the company took out a mortgage on 31 March 1885 to the Australian Mutual Provident Society of their land.⁷⁷

Rather than demolish Campbell's Stores, the company enlarged them. HP Dove's plans drawn for insurance companies about 1882 showed the Metcalfe Bond as a two-storey masonry building, with eleven bays, with each divided into two floors, bond or free (Figure 2.13).⁷⁸ When the revisions to these plans were published in 1887, his plans showed the Metcalfe Bond as now being of three floors. It was still of eleven bays, but an extra level to each bay ensured there were three rooms. Interconnecting doors were shown within each group of three bays (Figure 2.13).⁷⁹ Prince believed that this enlargement took place in the 1890s. He also believed that the slates were re-used on the new roof and that the new roof was added as a trussed roof on the basis of comparing the timbers of the lower floors and the roof timbers.⁸⁰

Phase 4 History: NSW Government/Sydney Harbour Trust/MSB, 1887–1972

These works were completed before major changes in the ownership of the wharf. By 1886, the Australasian Steam Navigation Company had become over-extended due to its efforts to match the competition and prices of its rival shipping firms. A major tactical blunder had been the purchase of Campbell's Wharf and the subsequent cost of rebuilding the wharf. While it allowed the company to make some welcome profits from the capital values of the land, especially from the sale of the strip of land facing George Street, the relocation of the company away from its original base in Darling Harbour took it away from the hub of the coastal shipping trade. Additionally, it had to pay a higher cartage charge on its goods from Circular Quay.⁸¹ Matters became ever more difficult for the company, so that it soon made an overture to the Queensland Steam Navigation Company on 30 October 1886, which resulted in the amalgamation of the two companies to form the Australasian United Steam Ship Co Ltd.⁸² It took over all assets of the ASN Company except the engineering works.⁸³ The company already had adequate wharfage and found Campbell's Wharf redundant. On 21 May 1887, the Australasian Steam Navigation Company, then in liquidation, offered Campbell's Wharf to the government for £300,000. It had sold all of its steam ships. Most of the wharf was described as leased. The description of the wharf included among its listing 'Eleven 3-story [sic] stores built of brick on stone', which were known as the Metcalfe Bond and were let to J Upward at £1,650 with a lease running until July 1890.⁸⁴ The government responded on 26 July 1887 that it was only willing to offer £275,000 for the ASN wharf, an offer which was later accepted by the shareholders of the company.⁸⁵

On 27 September 1887, the Government Surveyor, SE Perdriau, surveyed the land and found that it comprised 3 acres 10 perches, of which 1 acre 2 roods and 28 perches were part of the 1814 grant and 1 acre 1 rood 22 perches were included in the 1878 grant of reclaimed land.⁸⁶ A plan which was filed with the papers for the sale to the government, and probably prepared by Perdriau, showed all the buildings on the site (Figure 2.15).⁸⁷ The formal conveyance of the wharf from the Australasian Steam Navigation Company to The Crown occurred on 28 October 1887 for £275,000. The plan accompanying the deed showed that part of Campbell's 1814 grant, along Lower George Street, had been sold to JW Cliffe and W Clarke. It also showed all buildings on the site. The Metcalfe Stores was shown as partially built on the reclaimed land and not wholly on the 1814 grant.⁸⁸

The government demolished most of the buildings to build a Navigation Board slipway, leaving only the Metcalfe Bond stores.⁸⁹ It ejected the P & O Company from its lease and leased the land to Blackwall and Company who demolished the P & O buildings and built new ones, which were occupied in 1888 by Flood and Company.⁹⁰

The 1889 Detail Survey showed an outline of the Metcalfe Bond stores (Figure 2.17).⁹¹ The 1895 version of the survey showed the outline of the stores shown with Branch Office, Government Printing Office at the northern end (Figure 2.18).⁹² Re-decking of the old ASN wharf was undertaken by the Public Works Department in 1890 at a cost of £454/1/3.⁹³ Upward & Co, of Circular Quay, continued to lease the Metcalfe Stores, at least until 1901.⁹⁴

In 1901, the Sydney Harbour Trust took over the Stores.⁹⁵ The 1901 Darling Harbour Resumption Plan shows the Metcalfe Stores as Bond and Free Stores with a branch of the Government Printing Office at the northern end (Figure 2.19). The survey appears to be based upon the 1895 Detail Survey.⁹⁶ The same year, the Sydney Harbour Trust came to an agreement with Norddeutscher Lloyd to build it a new wharf measuring 1,000 feet and 40 feet wide, with offices and other buildings which the company would lease for three years at £2,500 per annum (Figure 2.21). Associated with this work was the reconstruction of the twin piers into a substantial central jetty, with full length sheds.

Also in 1901, the Sydney Harbour Trust compiled a Register of Assets, which described for the former Campbell's Wharf '2 large store stone and brick, slate roof each 3 floors. NOTE The Southern Store has 11 divisions built of stone & brick. The Northern Store has 4 divisions built of Stone'. A memo regarding the Northern Store stated—'To be demolished'.⁹⁷ The tenant of the Southern Store was given as 'Upward & Coy' at £500 pa payable monthly. The Sydney Harbour Trust maintained the Metcalfe Stores and did not demolish it in the wholesale removal of buildings which accompanied the building of the Norddeutscher Lloyd wharf. In 1902, it undertook repairs costing £7/15/- to Metcalfe's Bond Store.⁹⁸ Small repairs were shown in later reports.

Construction of a major ring road commenced which would connect the new Sydney Harbour wharves and enable them to be reached easily without having to pass through the city. It was soon named Hickson Road, after RRP Hickson, the former Under-Secretary of the Public Works Department who had been the first President of the Sydney Harbour Trust. In 1914, Hickson Road was completed from Dawes Point to Parbury's Wharf.⁹⁹ In April 1915, it was completed from the Metcalfe Bond to the Dawes Point Horse Ferry.¹⁰⁰ To complete the work, the Sydney Harbour Trust had to cut off about 1200mm of the northern end of the Metcalfe stores to build Hickson Road.

During the twentieth century, a series of other modifications were made. In 1932, a fire broke out in the northern addition to the Metcalfe Bond stores which housed part of the Government Printing Office and the offices of Metcalfe and Upward. Metcalfe and Upward moved after the fire to the second floor in the centre of the stores.¹⁰¹ The brick section damaged by the fire was later rebuilt.¹⁰² In 1958, the southernmost bay of the Metcalfe Stores was removed to allow construction of the Overseas Passenger Terminal, leaving ten bays.¹⁰³

The move of commercial maritime activities out of Sydney Cove and into Darling Harbour and Pyrmont affected Campbell's Stores in the twentieth century. During the late nineteenth century, Circular Quay developed as a terminus for sightseers and day-trippers. The mix of commercial and recreational activities was causing Sydney Cove to become heavily congested, particularly at weekends. One of the first acts of the Harbour Trust after it gained control of the area in 1901 was

an attempt to relieve congestion by the resumption of foreshore land and constructing two jetties and a longshore wharf on the eastern side of Bennelong Point.

While the eastern side of the quay was devoted to recreational traffic by the 1930s and was completely remodelled for that purpose in the 1950s, commercial activity continued in the vicinity of Campbell's Wharf into the late 1950s and early 1960s. Yet the importance of that area for commercial shipping had declined and this was reflected in changing use patterns of Campbell's Stores. After the erection of the Overseas Passenger Terminal in the 1960s, the area was no longer used as a commercial shipping area, as all such activity had moved to Darling Harbour and Pyrmont.

Phase 5 History: SCRA – SCA – SHFA, 1972 to Present

Campbell's Stores subsequently came into the control of the Sydney Cove Redevelopment Authority (later Sydney Cove Authority) after being handed over by the Maritime Services Board (successor to the Sydney Harbour Trust) in the 1970s. The Sydney Cove Authority later redeveloped the stores and paved the way for the Store's current use as a restaurant area.¹⁰⁴ In the mid-1980s, substantial works were undertaken in the vicinity of Campbell's Stores as part of the Bicentenary celebrations that included the removal of some of the wharfage near Campbell's Stores. In 1998, SHFA assumed control of the area, including Campbell's Stores.

2.5 Analysis of History in Context

Campbell's Stores demonstrates a number of aspects of the history of Sydney, its harbour and its commercial life from the nineteenth century.

- Campbell's Stores was a typical warehouse built for commercial storage purposes by its owners. There were a number of other such stores built before them. Of these, Moore's Warehouse, which has been moved from its original location, still survives, as does the Argyle Bond Store in Argyle Street. These warehouses were built with little ornamentation and they were intended as purely functional buildings, so that in the periodic redevelopments which have taken place around Sydney's wharfs, many have been demolished. These few are all that remain of those built before 1860.
- Campbell's Stores were built at Sydney Cove on the site of the first privately-owned wharf in Australia. Although the buildings do not date from the earliest period of that wharf, they still demonstrate the proximity of the wharf to the original hub of commerce and international shipping traffic into Sydney: Sydney Cove.
- The Stores were built for the Campbell family, one of the earliest free immigrant merchant dynasties to be established in Sydney. Although not built for the founder of the dynasty, Robert Campbell, it was erected for the firm he created which was then managed by his sons.
- Construction of these stores in 1851–52 reflected the changing commercial fortunes of Sydney and the Campbell family. In the later 1840s, the economy had regained some of its earlier vigour, which was then accelerated by the impact of the gold rushes after 1851. The original construction of the Stores was probably as much a response to the economic revival of the late 1840s as to the initial impact of the gold discovery. The extension and enlargement of the Stores in 1858–61 demonstrates the growth in commercial traffic and the increasing needs of Sydney and New South Wales for commercial storage, for goods shipped from overseas for the increased colonial population, as well as for imports out of the colony.

- New research completed for this study—utilising a range of sources which was not available when C Prince completed his pioneering study of Campbell's Store—has further refined our understanding of the construction of these stores and their place in the history of Sydney as an evolving mercantile centre. There is no evidence that these stores were extant before 1850. For many years they have been associated intimately in public consciousness with Robert Campbell, the pioneer free merchant in Sydney. However, they were *not* built for Robert Campbell, but for his sons who continued their father's business. Their pre-eminent position as merchants was widely acknowledged and their firm was one of the most prestigious in Sydney. Rather than providing evidence of the pioneering phase of mercantile development in Sydney, these stores demonstrate the consolidation of the Campbell family as a merchant dynasty in Sydney.
- Enlargement of the stores by the addition of a third storey was previously attributed to the Public Works Department in the 1890s. Further research has shown that this is not the case. These stores were enlarged in the 1880s by the Australasian Steam Navigation Company as part of their refurbishing of the wharfs they had bought from the Campbell family. This redevelopment was intimately related to the company's attempts to reposition itself in the highly competitive world of coastal shipping. The enlargement of the store demonstrates the evolving commercial development of Sydney Cove and the expansions and contractions of the corporate world of Sydney shipping.

2.6 Chronology

1801	September, Robert Campbell married Sophia Palmer.
1801	November, some of Campbell's storehouses were complete, as was stone wall and small wharf at right angles to the main warehouse. It was claimed to be the first privately owned wharf in Australia.
1802	Campbell moved into Wharf House which was incomplete.
1807	Meehan's map does not show Campbell's house or the wharf.
1808	John Lewin's watercolour showed jetty completed out from the vaults, south of the two-storey storehouse, an access road to Wharf House running behind this jetty.
1812	Absalom West's etching of the Commissariat stores shows Campbell's Wharf.
1814	29 June, Town Grant to Robert Campbell, of 3 acres, 3 roods. Bounded on south by premises occupied by the Naval Officer, on southwest by road leading to Dawes Point Battery, N 36.5° W 629 feet, on northwest E31.25° N 200 feet, on the east by Sydney Cove, 'in consequence of his having erected thereon several large and expensive Buildings'. A right to make streets reserved to the Crown. Quit Rent £3/-.
1815	18 March, Campbell returned to Sydney after giving evidence in England at the trial of Lt-Col Johnston, to find his business bankrupt and many of his ships wrecked. Sought compensation from the government.
1817	Watercolour by Sophia Campbell, wife of Robert, titled 'Sydney in all its Glory', a view of Sydney Cove from Dawes Point, showed her family's land and wharfs in foreground. What later became the Metcalfe stores are not shown.
1820	28 February, Joseph Lycett watercolour also shows the single-storey stores at back of old warehouse
1822	4 January, Campbell received compensation for government losses of his ships while he was in England from 1812–15 and the money may have helped fund work on the warehouses.
1822	21 June, Campbell advertises for masons to work on his wharf, warehouses and sea wall.
1825	Stewart's copy of Harper's map shows no building in the area of the warehouses.

1828	Robert Campbell senior signed a partnership agreement with his sons who were gradually coming into the management of the firm.
1828	Warehouses on jetty shown in Map of North Shore.
c1830	John Campbell virtually taken over the business.
1831	17 September, Robert Campbell objected to the government gangs which were making a road through his land. He claimed that the land which he bought in 1799 only had provision for a road 66 feet to Dawes Point Battery.
1831	21 December, Engineer for Public Works, Charles Wilson, informed Colonial Secretary that Robert Campbell did not hold title to land on west of road apart from mere possession. In addition the grant of 1814 reserved the right to make roads through the land, and the present road is only 30 feet wide and not 66 feet wide. ¹⁰⁵
1831	Magistrates Returns of Manufactories, Mill etc, shows Robert Campbell senior, of Campbell's Wharf, has one hydraulic pump at wharf.
1832	3 August, Robert Campbell supplied a sketch of the land to which he is entitled. The plan shows his house and the stores along the HWM.
1832	Robert Campbell's wharf had one hydraulic pump and a wool press.
1832	Sketch in Surveyor-General's sketch book shows original land granted to Campbell and the store along the front of the grant, as in Stewart's 1823 plan. This plan was drawn as part of the investigation into Campbell's title for his land, in relation to the desire of the government to widen George Street through his land.
1835	Robert Campbell subdivided his land west of George Street for sale.
1836	John Campbell officially takes over as head of the firm.
1836	Robert Campbell no longer listed as holding a wool press or hydraulic pump.
1836	6 May, Campbell's Wharf advertised for sale.
1836	3 June, injunction to prevent sale of Campbell's Wharf was taken out.
1836	21 June, report of case in Supreme Court of Campbell versus Hartwell and others.
1838	R Campbell & Co listed as possessing one crane. ¹⁰⁶
1839	John Campbell of George Street listed as holding two cranes and one wool press. ¹⁰⁷
1836–1841	File regarding extension of George Street North in this vicinity does not reach as far as Campbell's property. ¹⁰⁸
1841	21 January, Robert Campbell applied to Colonial Secretary for permission to enlarge part of his wharf so that ships can unload at low tide, using a rock which cannot be removed by dredging. Notes that he has been building and extending his wharf for the last 40 years. Colonial Engineer, George Barney minuted that he could find no objection to the enlargement of the wharf. ¹⁰⁹
1842	Lithograph showing Campbell's Wharf by JS Prout shows no sign of the stone storehouses later known as Campbell's Stores
1843	29 June, Mortgage, Robert Campbell, senior esquire, George Street, Sydney, to The Australian Trust Company. Of 3 acres 3 roods as in 1814 grant, For £10,000 for three years with interest. ¹¹⁰
1845	First rate assessment of the City of Sydney. On Campbell's Wharf, it identified a house, stores, warehouse and wharf valued at £1,000. At 'north end of Campbell & Co wharf' were three stores plus an office and store, of three storeys, with slate roofs, value at £150 each, two of which were vacant and two occupied by Smith and Campbell. Additionally, there was a cottage for the overseer George Atherden, and an empty timber woolshed. ¹¹¹

1845	11 October, Robert Campbell draws up will. Leaves property in six parts to be divided among sons John, Robert, Charles, George, and daughter Sophia Ives Campbell and Arthur Jeffreys, husband of daughter, Sarah.
1846	April, plan of alignment of George Street from Charlotte Place to Fort Street drawn by surveyor Armstrong (S.38.907). Plan is recorded as 'Missing' at DLWC Plans Room.
1846	26 August, transfer of mortgage, Australian Trust Co to Commercial Banking Company of Sydney, for £10,000. ¹¹²
1847	16 February, conveyance, Charles Campbell, Queanbeyan esquire to John and Robert Campbell, merchants, of Sydney. Conveyance of his share in the will of his father to eliminate a sum of £15,000 owed to the firm of Campbell and Co. ¹¹³
1847	21 June, transfer of mortgage, Commercial Banking Co of Sydney to John Thacker, Adolphus William Young and Campbell Drummond Riddell. [These later prove to be trustees for the Liverpool and London and Globe Insurance Company of Sydney.] ¹¹⁴
1847	15 April, conveyance to uses, John Campbell, Wharf, George Street Sydney, and other members of the family convey to James Norton to uses for the benefit of the six members of the family. The 3 acre and 3 rood grant was to be held as tenants in common rather than as joint tenants, for the purpose of this deed. ¹¹⁵
1848	26 June mortgage, John Campbell, George Street, merchant, Robert Campbell, Sydney, merchant and his wife Ann Sophia to Union Bank of Australia. To cover £6,691/6/10 owed the bank on Bills of Exchange and in preparation for a further loan of £6,000. This deed provides new enlarged dimensions of the wharf— bounded on southwest by Lower George Street along a wall, N 36.1° W 10 chains and 4 links, on northwest by Robert Cunningham's land along a wall, E 31.75° N, 4 chains 20 links, on the east by Sydney Cove on the south by Crown land W 3° 15 ' S, 3 chains. ¹¹⁶
1848	26 June Deed of same date covers loan of £6,000. ¹¹⁷
1851	Between 25 August and 27 September 1852, the City Council Rate Assessment Book shows that in Campbell's Wharf, J Campbell had added five Stores, which were built of stone, with slate roofs, all provisionally assessed at a net value per annum of £30.
1852	25 February, change in trustees of Liverpool and London and Globe Insurance Company. ¹¹⁸
1853	29 August, reconveyance of mortgage to cover Bills of Exchange by Union Bank to the Campbells'. ¹¹⁹
1853	20 October, change in trustees of Liverpool and London and Globe Insurance Company. ¹²⁰
1855	20 December, change in trustees of Liverpool and London and Globe Insurance Company. ¹²¹
1858	Council's Rate Assessment Book shows that the three-storey stone warehouses on the water's edge are still extant. Among the array of other wharf buildings are warehouses of stone with slate roofs, built as two storeys with two rooms in them. Three operate as warehouses while two are conducted as a combined warehouse and office, with four rooms. ¹²²
1858	Jevons' photograph showed eight stores in the Metcalfe Bond Store, plus the four-bay, three-storey warehouse to the north, as well as the original warehouse.
1856–1860	Photograph from MF Moresby Album shows 10 bays with the base of the eleventh under construction and awaiting its roof.
1858–1859	Sands Directory shows at Campbell's Wharf—Campbell & Co; Sugar Company's stores; JC Dibbs & Co, commission agents and wharfingers; Robert Nash, storekeeper; WH Eldred, Capt, Chili Flour Co; Chilian Consulate – Consul, WH Eldred. ¹²³
1859	30 March, Robert Campbell Junior dies at Dunroon, leaving elder brother John in charge of the firm.
1861	Council's Rate Assessment Book shows that a further six bays had been added to the five bays of the warehouse which were extant in 1858, making eleven bays in all. They are all of two storeys with two rooms in each. ¹²⁴

1861	Sands Directory shows at Campbell's Wharf – Campbell & Co; Colonial Sugar Refining Co, stores; Peruvian guano stores; George Lloyd & Co stores; Robey & Co's stores; George Lewis custom house officer; WH Eldred, broker & general agent. ¹²⁵
1863	Sands Directory shows at Campbell's Wharf—4. Merry Willis and Co, merchants; 3. Henry Fisher & Son, sugar factors; 2. Brown & Co, merchants; 1. EM Sayers & Co, merchants; Robert Nash; WH Eldred, merchant; 4. Joseph Kendall, marine surveyor; JA Buttrey & Co, merchants; 5 and 6, Daniel Thacker & Co, merchants; 22 Campbell's stores; and John Campbell, merchant. ¹²⁶
1865	All maps up to this date only show the earlier range of stores at front and those partially across this land. The 1865 Detail Survey held at AO exists in two versions. The black and white shows the older buildings on the site, while the coloured print shows the current Campbell's Stores. ¹²⁷
1870	Photograph shows the eleven bays of the completed warehouse. ¹²⁸
1875	18 February, change in trustees of Liverpool and London and Globe Insurance Company. ¹²⁹
1875	Campbell's Wharf has landing area of 100 feet which is backed by 22 stone stores which are very strong, and along with two sheds on the wharf can hold 22,000 tons. Accompanying engraving [based upon some of the extant photographs] shows 11 bays of the stores which are two storeys high. ¹³⁰
1876	18 February, Australasian Steam Navigation Company applied to Minister of Lands to extend Campbell's Wharf which they have recently purchased by running out jetties on piles into the harbour.
1876	May, ASN bought Campbell's Wharf for £100,000. ¹³¹
1876	July, Campbell's sold wharf, warehouses, Wharf House and stable to Australasian Steam Navigation Company for £100,000. ¹³²
1876	4 November, conveyance in trust. All beneficiaries of the will of Robert Campbell have reached age of 21 years old and convey land to John Campbell and Robert Campbell Close for 10/-. Area given as 5 acres and 12 perches. ¹³³
1877	6 January, John Campbell, applied for the purchase of reclaimed land in front of Campbell's Wharf, measuring 2 roods 5 perches. Plan by JF Mann, of 4 January 1877, which accompanied application, showed water frontage and some buildings but not the Stores. ¹³⁴
1877	23 March, Vesting Order. John Campbell and Robert Campbell Close had brought an action in the Supreme Court, Equity Jurisdiction to have the land affected by the mortgage by Robert Campbell to the Australian Trust Company in June 1843 and now held by the trustees of Liverpool and London and Globe Insurance Company, to be vested in them. Court ruled in their favour. ¹³⁵
1877	15 March, conveyance by trustees of the Liverpool and London and Globe Insurance Company to John Campbell and Robert Campbell Close of 3 acres 3 roods. ¹³⁶
1877	4 April, conveyance, Hon John Campbell esquire, Campbell's Wharf, Sydney and Robert Campbell Close esquire, Sydney, to the Australasian Steam Navigation Company. 5 acres 12 perches, with dimensions as in June 1848 deed. For £100,000. ¹³⁷
1877	6 April, joint memo by Surveyor General, PF Adams and Engineer for Harbours and Rivers, EO Moriarty, that they have no objection to the land as is within limit of reclamation in plan P.40.574. They also note that the application does not cover all of the land actually reclaimed which measures about 1 acre 1 rood and 18 perches. ¹³⁸
1877	31 May, notice of Campbell's application published in Government Gazette. ¹³⁹
1877	19 October, plan drawn by Surveyor Wansborough of reclaimed land claimed for purchase by ASN Co. Plan not available at Land Titles Office or DLWC Plans Room.
1877	November, ASN built new wharf, under superintendence of Thomas Macredie. A wharf 320 feet long was being built with two jetties 250 and 350 feet long. A new road 50 feet wide would join George Street running in front of the Mariner's Church. ¹⁴⁰
1878	Proposal to extend George Street North through land of ASN.

1878	Sands Directory shows ASN Company's wharf here. On the site was 'The Metcalfe Bond and Free Stores' with D Murray as warehouse keeper. The previous year, the stores were called 'Campbell's Bonded Stores'. ¹⁴¹
1878	17 December, grant by Purchase of Reclaimed Land under Crown Lands Act by Hon John Campbell, Sydney, for £100, of 1 acre 1 rood and 22 perches of land reclaimed from Sydney Cove in front of Campbell's Wharf. ¹⁴²
1879	4 July, transfer of reclaimed land from John Campbell to ASN Co. ¹⁴³
1880	January, a strip of land was sold to City Council for £4,000 to widen George Street North.
1880	15 October, Richardson and Wrench offer 19 lots between Horse Ferry Road and George Street for sale. To east are shown 'Metcalfe Bonded and Free Stores'.
1881	15 February, lease of part of wharf from ASN to Peninsular and Oriental Steam Navigation Company. ¹⁴⁴
1881	May, ASN sold 'Campbell's Garden' for £25,000.
1882	J Upward appears as the proprietor of the 'Metcalfe Bonded Stores'. ¹⁴⁵
1882	December, survey of area by Charles Bullock for Surveyor-General completed. Shows 'Bonded and Free Stores' with Branch Store of Government Printing Office at northern end.
1882	HP Dove's plans drawn for insurance companies shows Metcalfe Bond as a two-storey masonry building, with eleven bays, with each divided into two floors, bond or free.
1884	September, ASN accepted contract for £30,000 for new offices and four stores on the site of Campbell's old house.
1885	31 March, mortgage by ASN to Australian Mutual Provident Society of reclaimed land. ¹⁴⁶
1886	The Australasian Steam Navigation Company amalgamated with the Queensland Steam Navigation Company, to create the Australasian United Steam Ship Co Ltd. It took over all assets of the ASN Company except the engineering works.
1887	H Percy Dove's revision of his plans show Metcalfe Bond is now three floors, with an added number for each bay, still eleven bays. Interconnecting doors are shown within each group of three bays. ¹⁴⁷ Prince believed that this enlargement took place in the 1890s. He also believed that the slates were re-used on the new roof and that the new roof was added as a trussed roof on the basis of comparing the timbers of the lower floors and the roof timbers. ¹⁴⁸
1887	21 May, offer by Australasian Steam Navigation Company of Campbell's Wharf to government for £300,000. Company is in liquidation and has sold all of its steam ships. Most of wharf is leased. Wharf includes 'Eleven 3-story stores built of brick on stone', which were known as the Metcalfe Bond and were let to J Upward at £1,650 with lease running until July 1890. ¹⁴⁹
1887	26 July, government only willing to offer £275,000 for ASN wharf, which was later accepted by shareholders of ASN. ¹⁵⁰
1887	27 September, Government Surveyor SE Perdriau surveyed land and finds that it comprised 3 acres 10 perches, of which 1 acre 2 roods and 28 perches were part of the 1814 grant and 1 acre 1 rood 22 perches were included in the 1878 grant of reclaimed land. ¹⁵¹
1887	28 October, re-conveyance of mortgage to AMP Society. ¹⁵²
1887	28 October, conveyance, Australasian Steam Navigation Company to The Crown for £275,000 of 3 acres 10 perches part of the Campbell land. The part along Lower George Street had been sold to JW Cliffe and W Clarke. Plan on the deed shows details of the buildings, etc. Metcalfe Stores shown as partially on the reclaimed land and not wholly on 1814 grant. ¹⁵³
1889	Outline of stores shown on Detail Survey. ¹⁵⁴
1890	Re-decking of the old ASN wharf by Public Works Department at cost of £454/1/3.
1895	Outline of stores shown with Branch Office, Government Printing Office at northern end.

1901	Metcalfe Stores, occupied by Upward & Co, Circular Quay.
1901	Sydney Harbour Trust came to an agreement with Norddeutscher Lloyd to build a new wharf for them.
1901	Darling Harbour Resumption Plan shows Bond and Free Stores here with Branch of Government Printing Office at northern end. It appears to be based upon the 1895 Detail Survey. ¹⁵⁵
1901	Asset Register of Sydney Harbour Trust describes the following '2 large store stone and brick, slate rf each 3 floors. NOTE The Southern Store has 11 divisions built of stone & brick. The Northern Store has 4 divisions built of Stone'. A memo regarding the Northern Store stated—'To be demolished'. Tenant of the Southern Store was given as 'Upward & Coy' at £500 pa payable monthly. ¹⁵⁶
1914	Hickson Road from Dawes Point to Parbury's Wharf completed
1915	April, Hickson Road from Metcalfe Bond to Dawes Point Horse Ferry completed.
1915	19 March, Survey of western side of Circular Quay completed and details used to compile later plan dated c1944 which shows Metcalfe Bonded and Free Stores. ¹⁵⁷
1932	Fire in northern addition to Metcalfe Bond stores which housed part of Government Printing Office and the offices of Metcalfe and Upward. Metcalfe and Upward moved after fire to second floor in centre of the stores.
Post-1932	Brick section, damaged by fire, rebuilt.
1958	Southernmost bay of Metcalfe stores removed to allow construction of Overseas Passenger Terminal, leaving ten bays.
1967	Brick extension repaired after 1932 fire, occupied by Commonwealth Film Censor. ¹⁵⁸
1974–1978	Sydney Cove Redevelopment Authority conservation program documented by Devine Erby Mazlin Architects. The works included re-slating of the roof, re-facing of sandstone to Level 1, east face, the construction of a service tunnel on Hickson Road to the rear of Level 1, the conversion of some windows to doors on Level 2 facing Hickson Road and paving on the forecourt in front of the building.
1981	Imperial Peking Restaurant, Lai and Wojtulewicz Architects.
1986	Italian Village Restaurant, Ercole Palazetti Architects.
1992	17–21 Hickson Road (Bays 6–4) Tim Throsby Architects.
2005	Tanner Associates Masterplan.
2008	Design 5 documentation for conservation.
2009	Dockside Group leases.



Figure 2.1 Sydney Cove, 1825. Stewart's map shows Campbell's Wharf with the older warehousing visible roughly where the store would later be built. The asterisk marks the site of the current stores. (Source: AO Map SZ469)

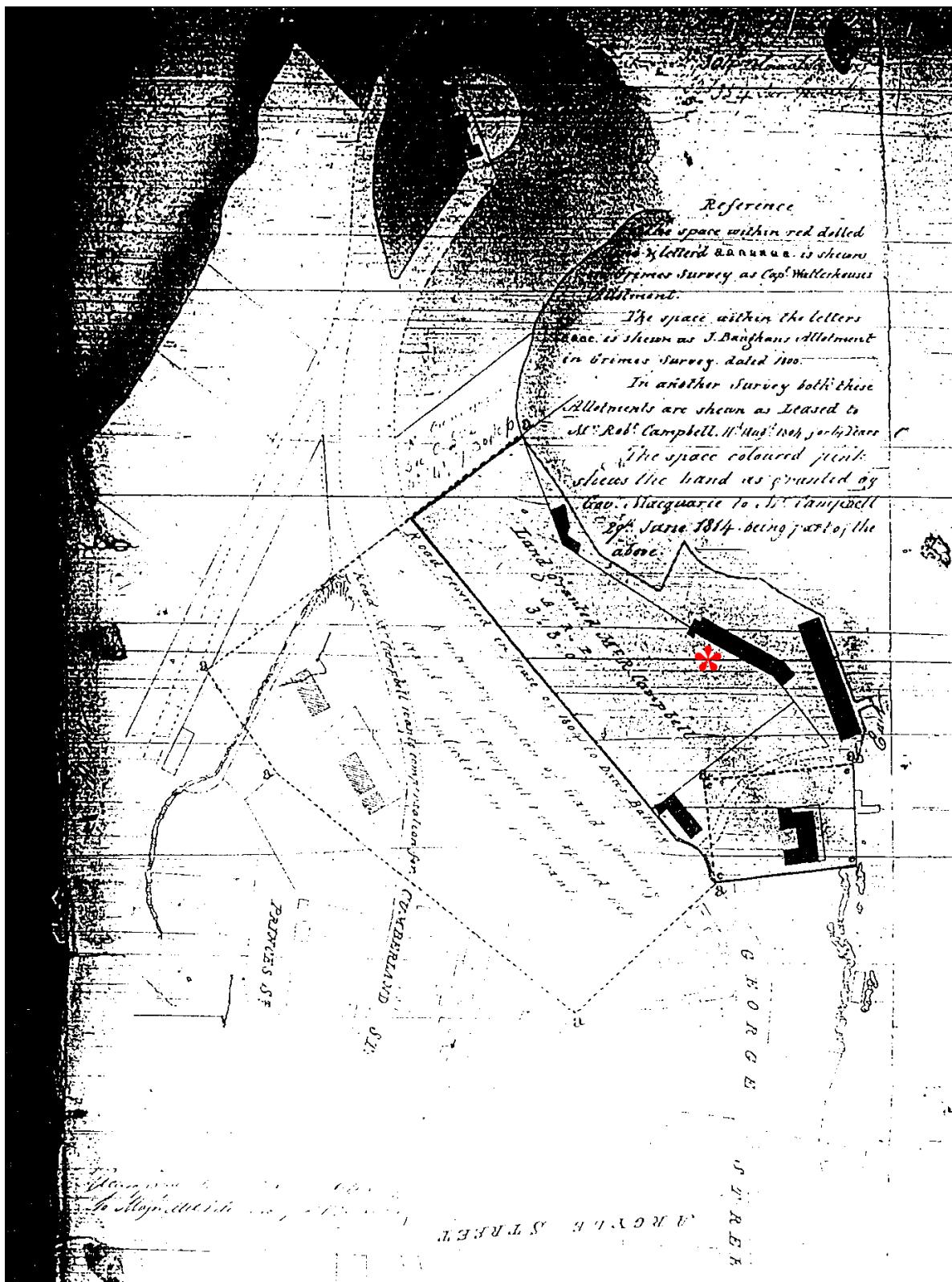


Figure 2.2 Campbell's landholdings, 1832. To clarify the location of the land that Robert Campbell held, this sketch was provided by Surveyor Lewis in 1832. The storehouses are not shown. The site where they would later be built is marked with an asterisk. (Source: Survey-General Sketch Book 2, f.9)

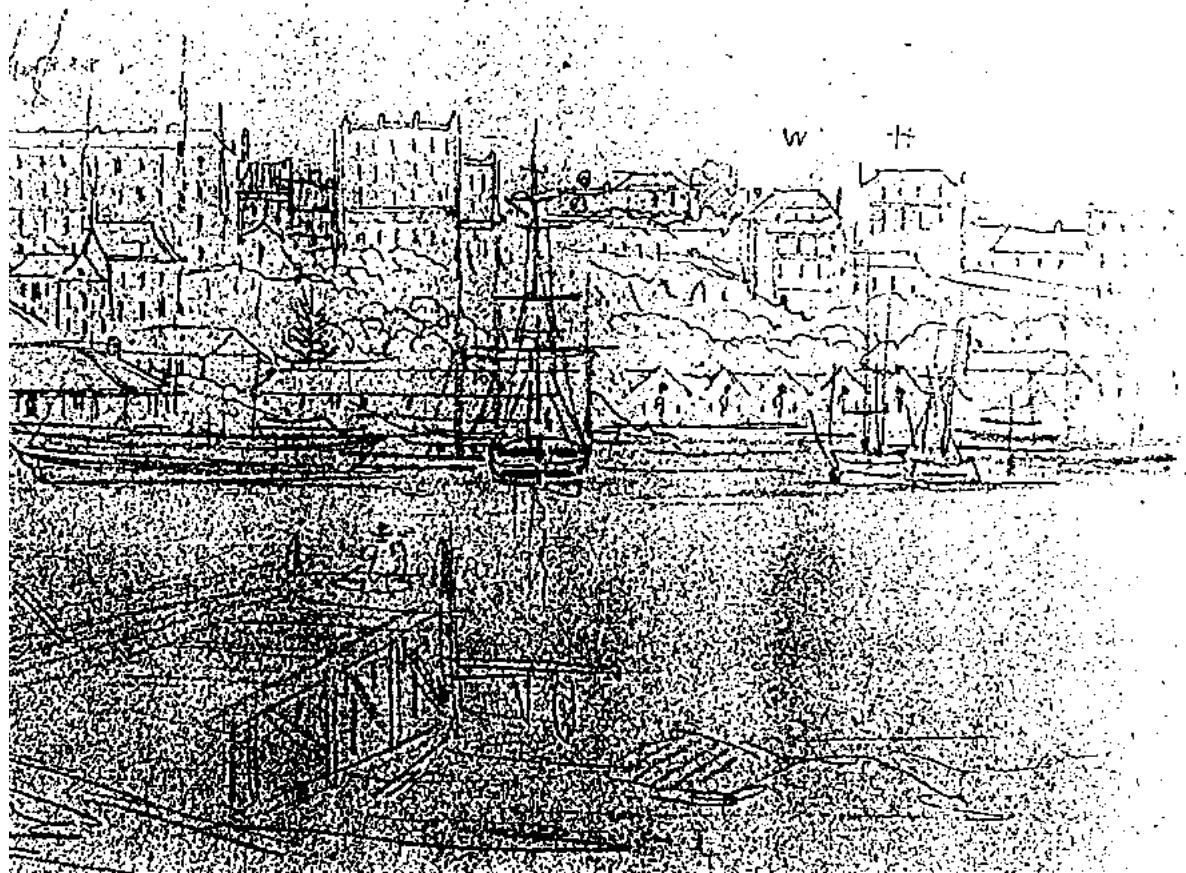


Figure 2.3 Sydney Cove, 1831. This sketch by Conrad Martens is used by Prince to ascribe a commencement date of 1838 for the storehouses, but other evidence suggests that the later date rather than the earlier one would be a more accurate estimate of the date of this sketch. (Source: SCA photo, Sydney Cove 7)



Figure 2.4 Campbell's Wharf, 1842. JS Prout's lithograph provides a view of the wharf. No buildings can be identified as part of the Campbell's Stores. (Source: JS Prout and John Rae, *Sydney Illustrated*, Part II, Sydney 1842, opp p 11)



Figure 2.5 Campbell's Wharf, 1856–60. This image from the Moresby Album shows ten bays of the warehouses complete with the eleventh one awaiting its roof. (Source: PXA 9, Mitchell Library)

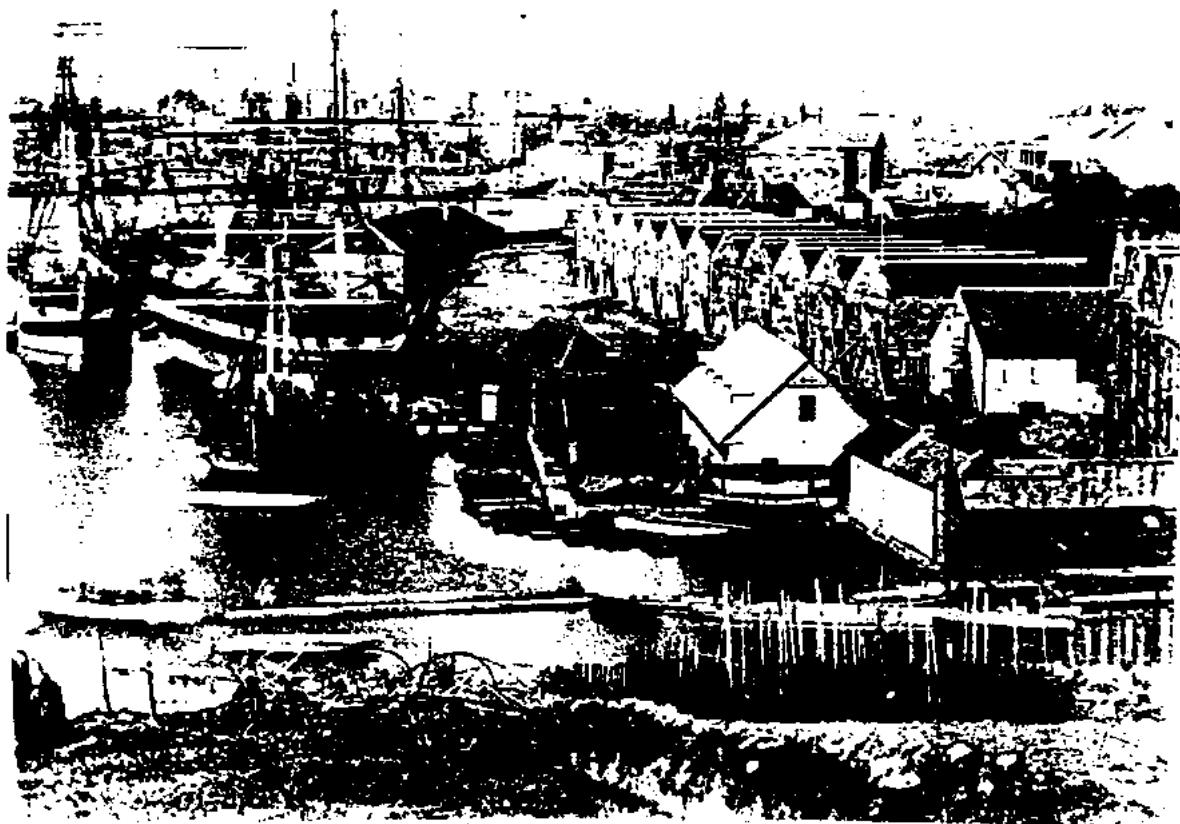


Figure 2.6 Campbell's Wharf, 1856–60. This image shows the storehouses as complete. (Source: PXA 4358, Mitchell Library)

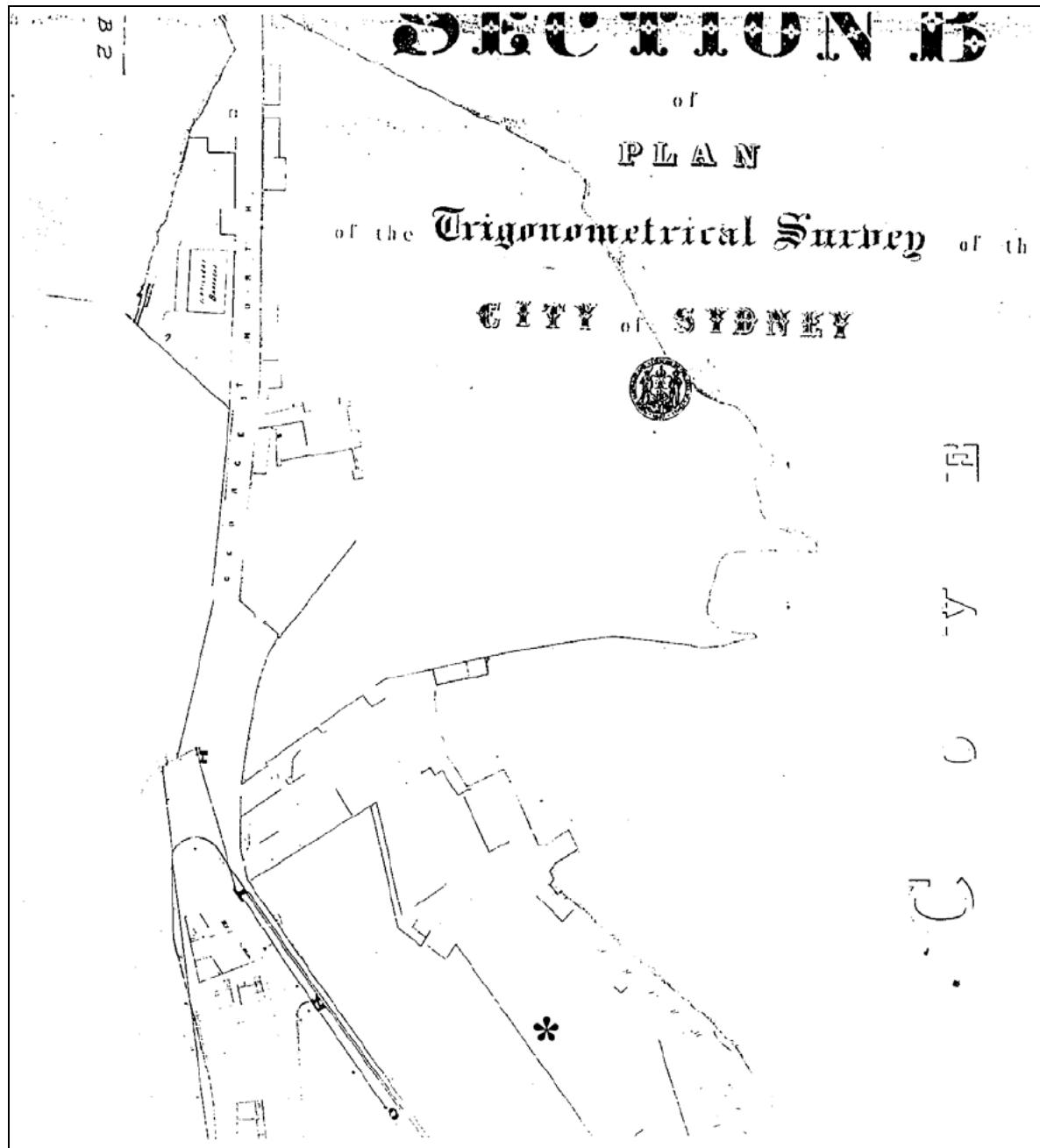


Figure 2.7 Trigonometrical Survey, 1865. The coloured edition of this plan shows the outline of the storehouses, marked with an asterisk. (Source: MWS&DB, 1865 Trig Survey, AONSW)



Figure 2.8 Sydney Cove, 1870. This view shows the eleven bays of the warehouse facing the Cove. (Source: AO, Photo 1077)

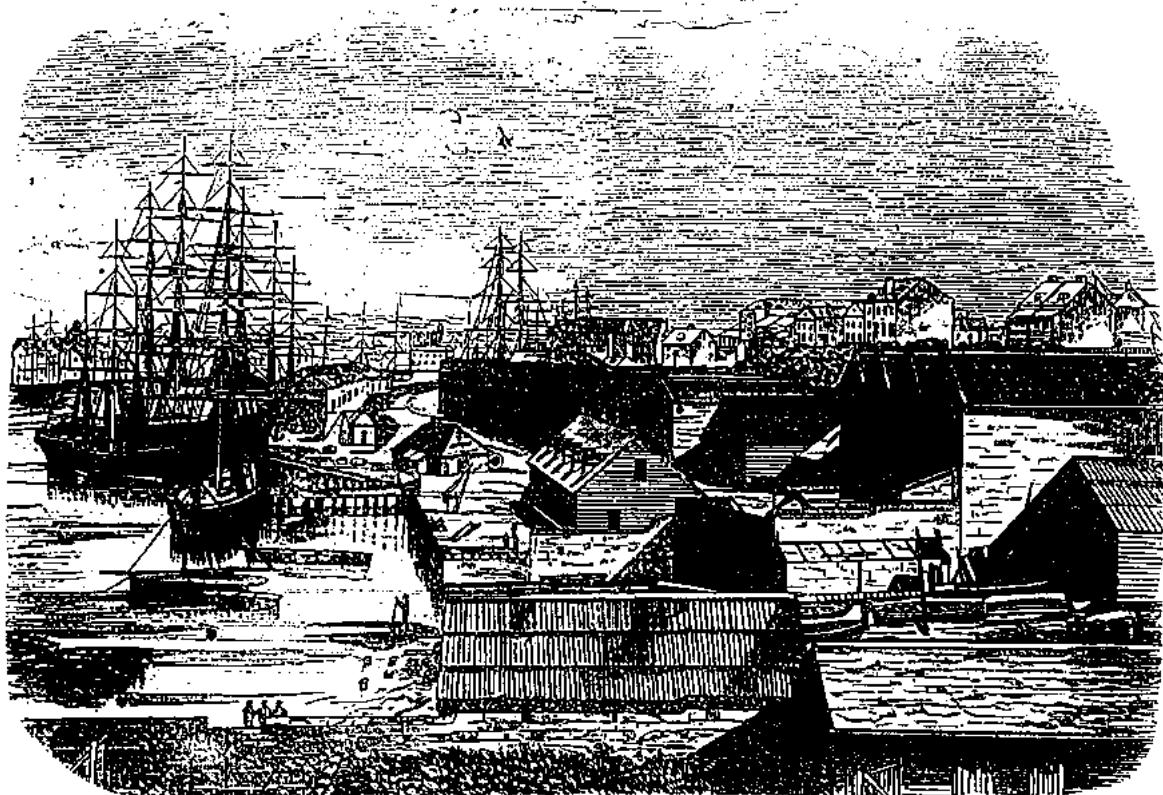


Figure 2.9 Campbell's Wharf, 1875. Most likely drawn from one of the earlier photographs, this lithograph purports to show Campbell's Wharf in 1875. (Source: *Australian Town and Country Journal*, 20 November 1875, p 20)

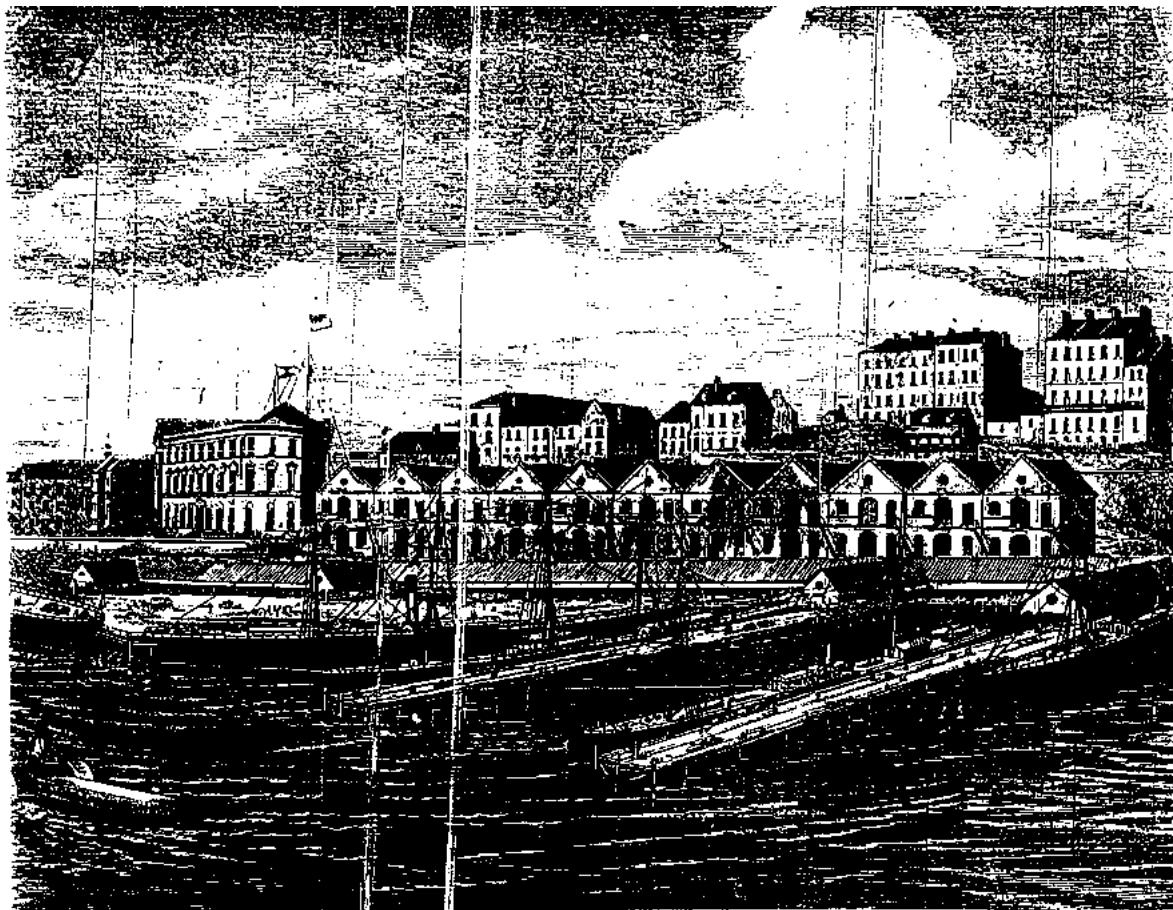


Figure 2.10 The ASN Company's new wharfs, 1877. Campbell's Stores, or the Metcalfe Bond Stores as they were now becoming known, are prominent in this view, and still only consist of two storeys. (Source: *Illustrated Sydney News*, 10 November 1877, p 4)



Figure 2.11 Metcalfe Bond, early 1880s. This photograph shows the stores still as two storeys. They would soon be enlarged to three storeys. (Source: SCA Photo, CC7)

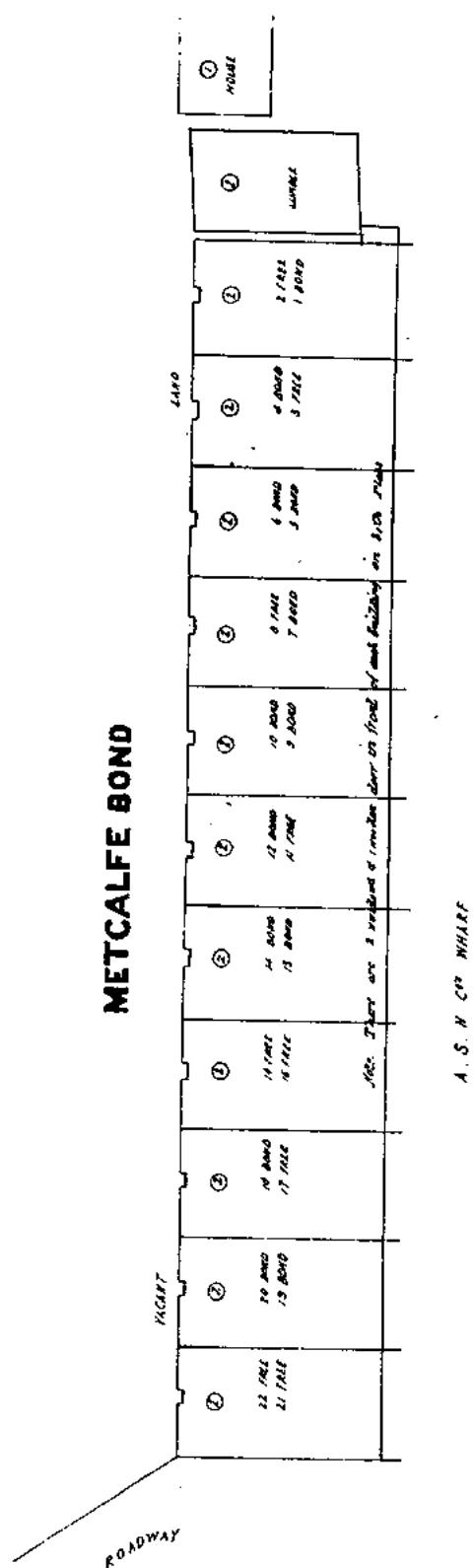


Figure 2.12 Metcalfe Bond, 1882. HP Dove's plan of 1882 showed the earlier layout of the Stores with two storeys before they were enlarged to three storeys by 1887. (Source: HP Dove, *Plans of Sydney Executed for Insurance Companies*, Sydney, 1882)

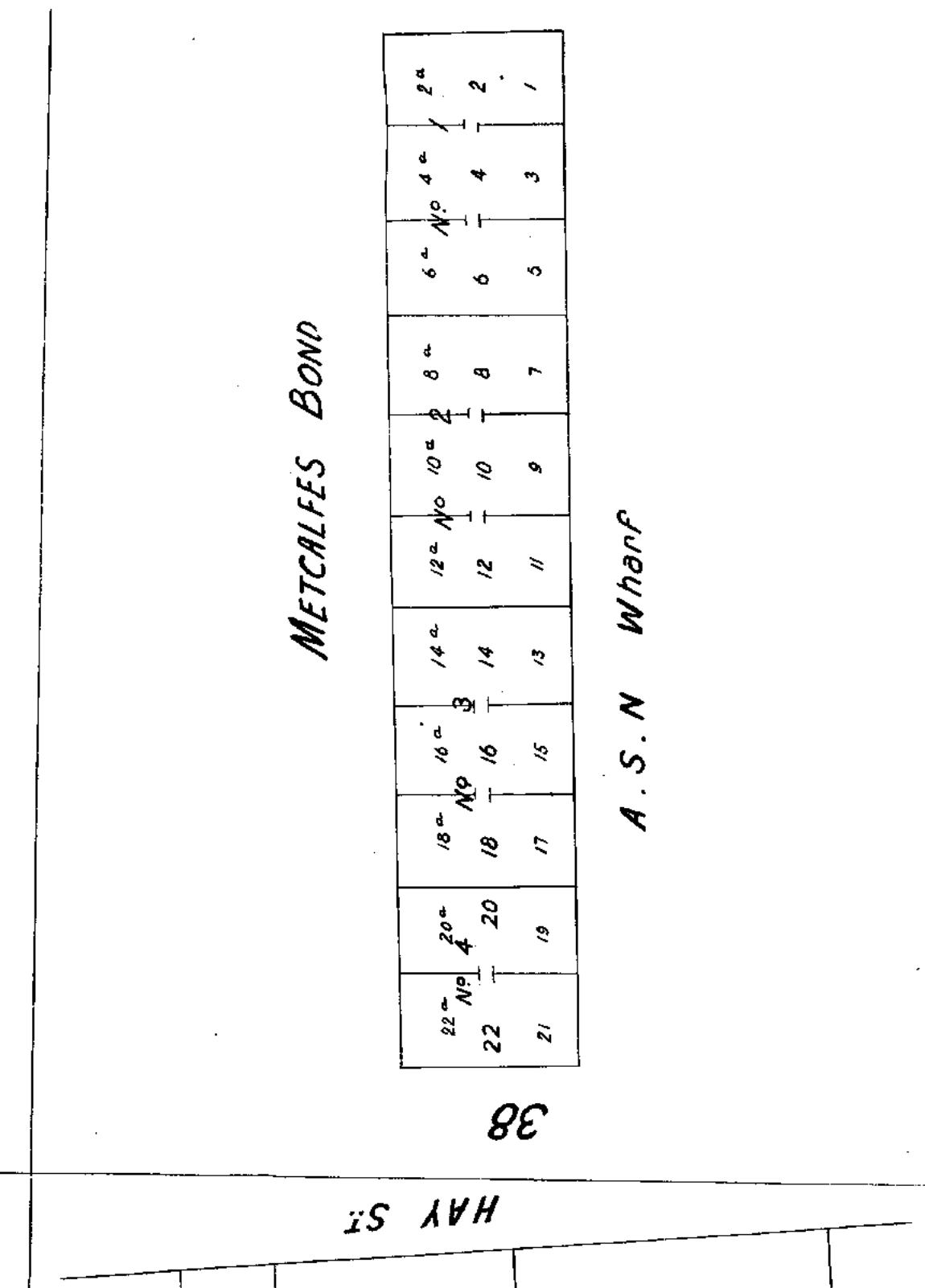


Figure 2.13 Metcalfe Bond, 1887. HP Dove's plan of 1887 showing layout of the Stores with enlargement to three storeys from the original two. (Source: HP Dove, *Plans of Sydney Executed for Insurance Companies*, Sydney, 1882)

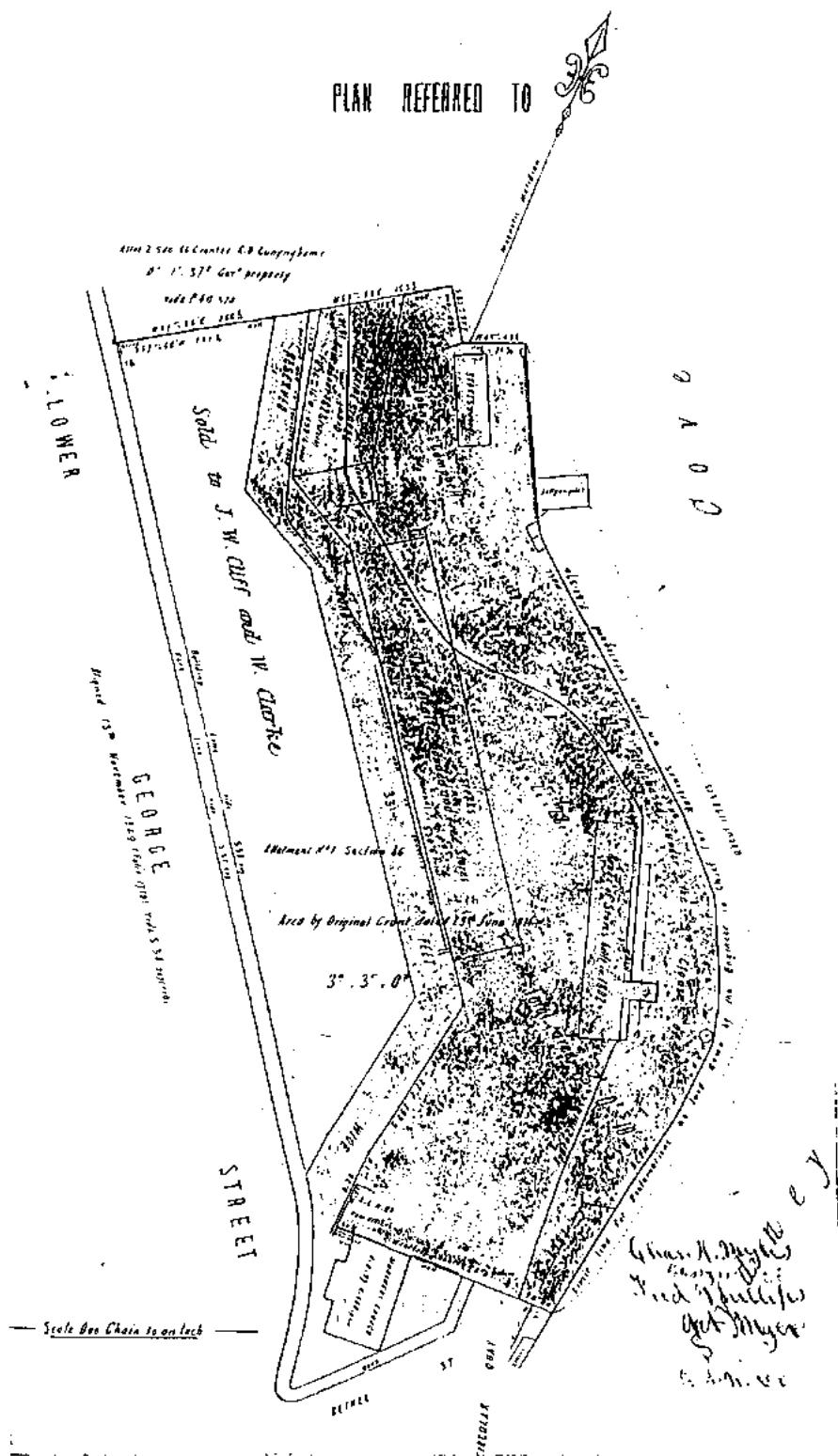


Figure 2.14 This plan appears in a number of different versions for the sale of the wharf by the ASN to the government. This copy accompanied the legal conveyance. (Source: LTOD, No.891, Book 373)

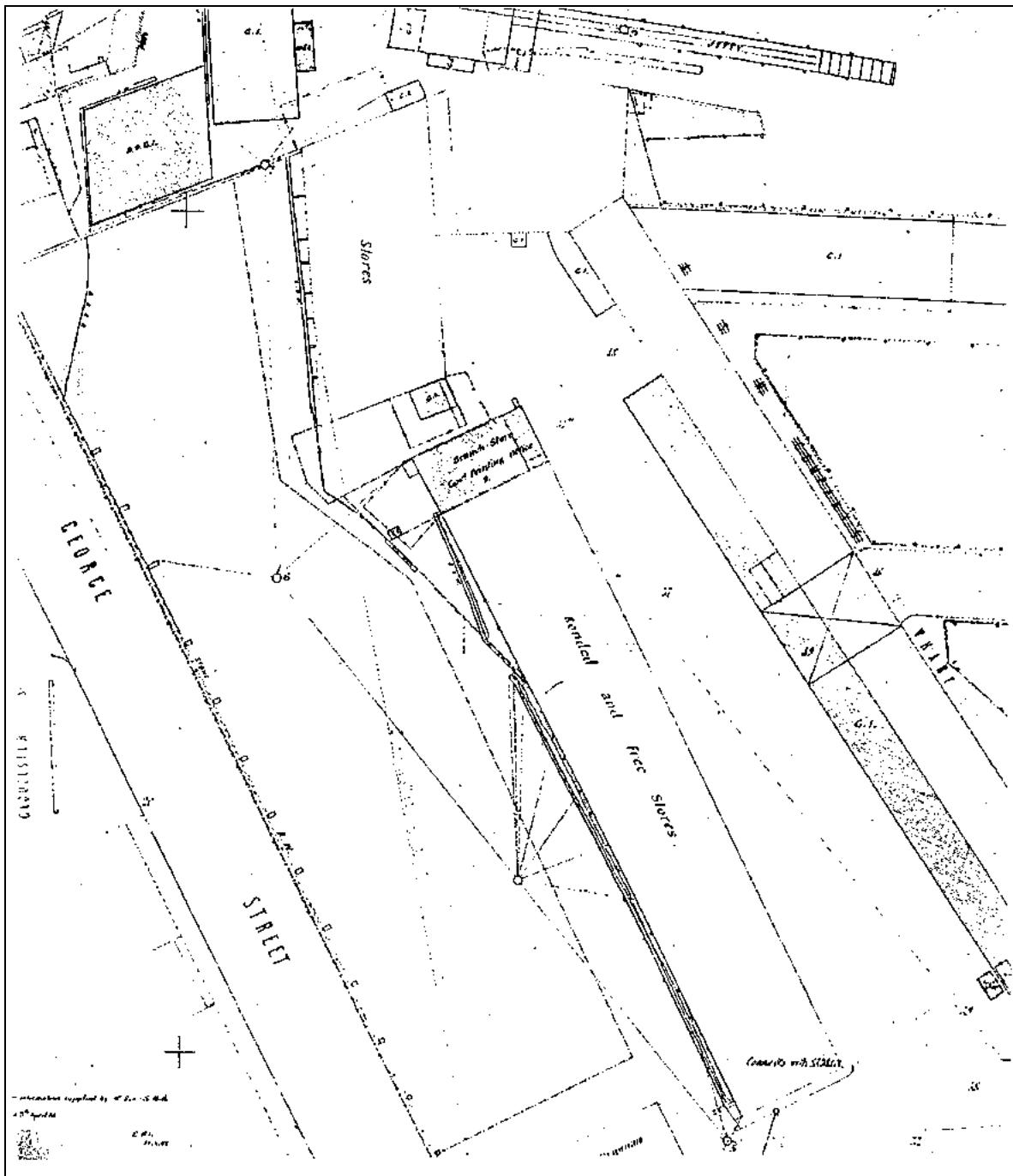


Figure 2.15 Metcalfe Stores, 1882. This survey by Charles Bullock of 1882, showed details of the stores. Additional details were added to the plan until 1895 so it more accurately shows the details at 1895. (Source: S.46.1544.Sh4)

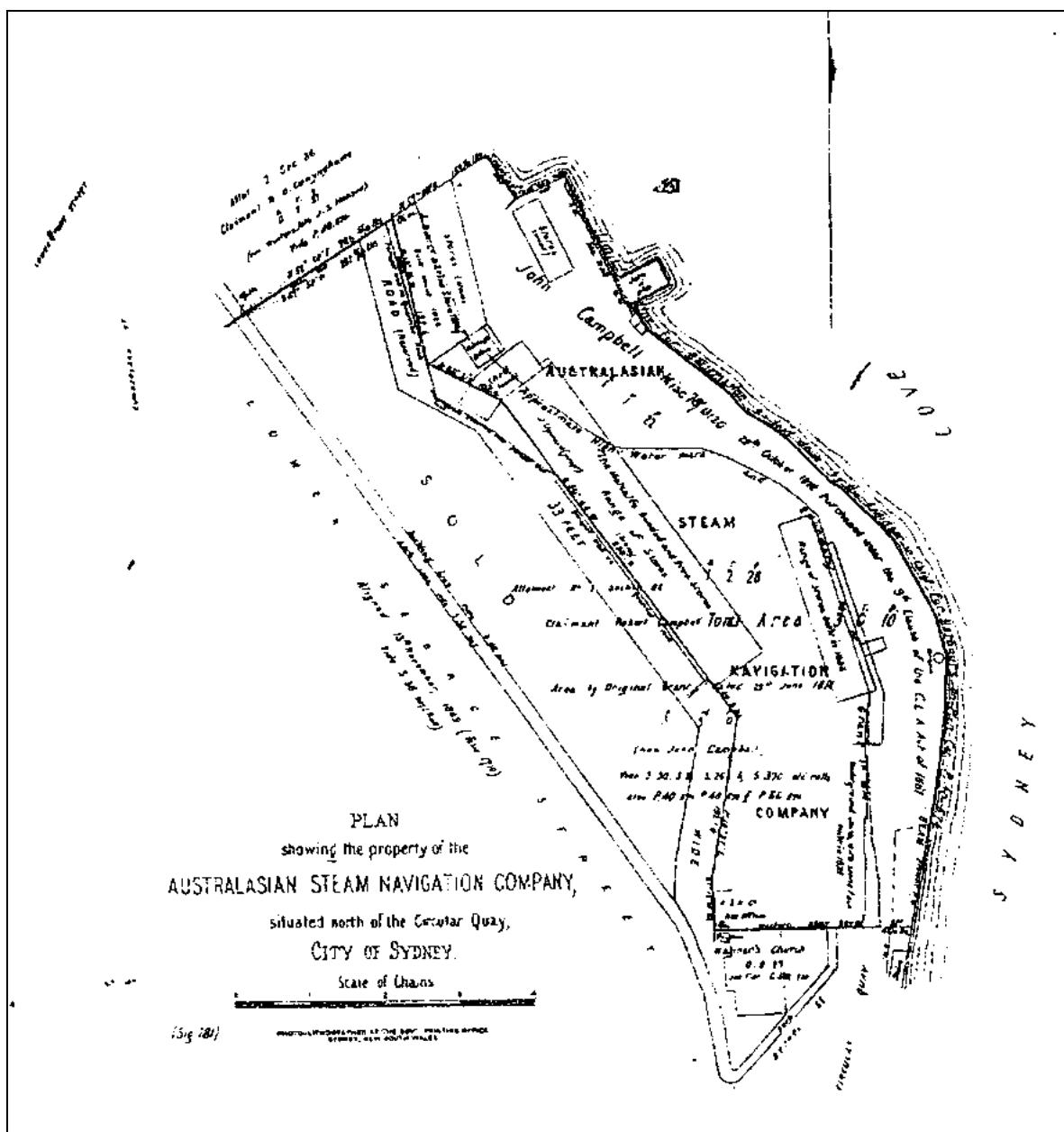


Figure 2.16 Plan 1887, this plan accompanied the documentation supplied to the New South Wales Legislative Assembly when it sought further information about the details of the government's purchase of the former Campbell's Wharf. (Source: V&PLANSW, 1887-88, Vol. 3, p 1018)

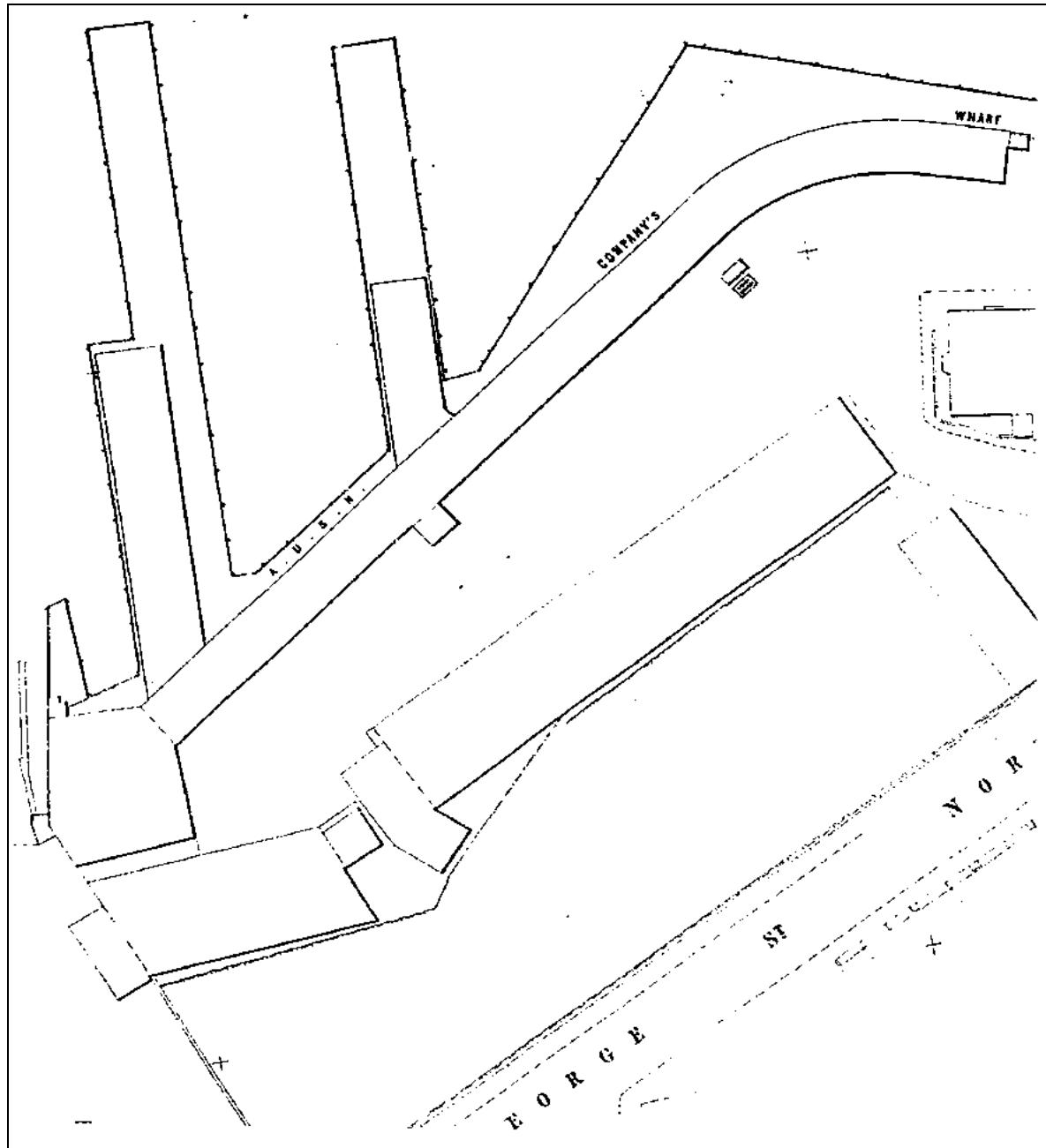


Figure 2.17 Metcalfe Stores, 1889. The Metropolitan Detail Survey of 1889 showed the stores. (Source: ML Map, M.Ser 4/811.17/1, City of Sydney, Sheet Q4, 1889)

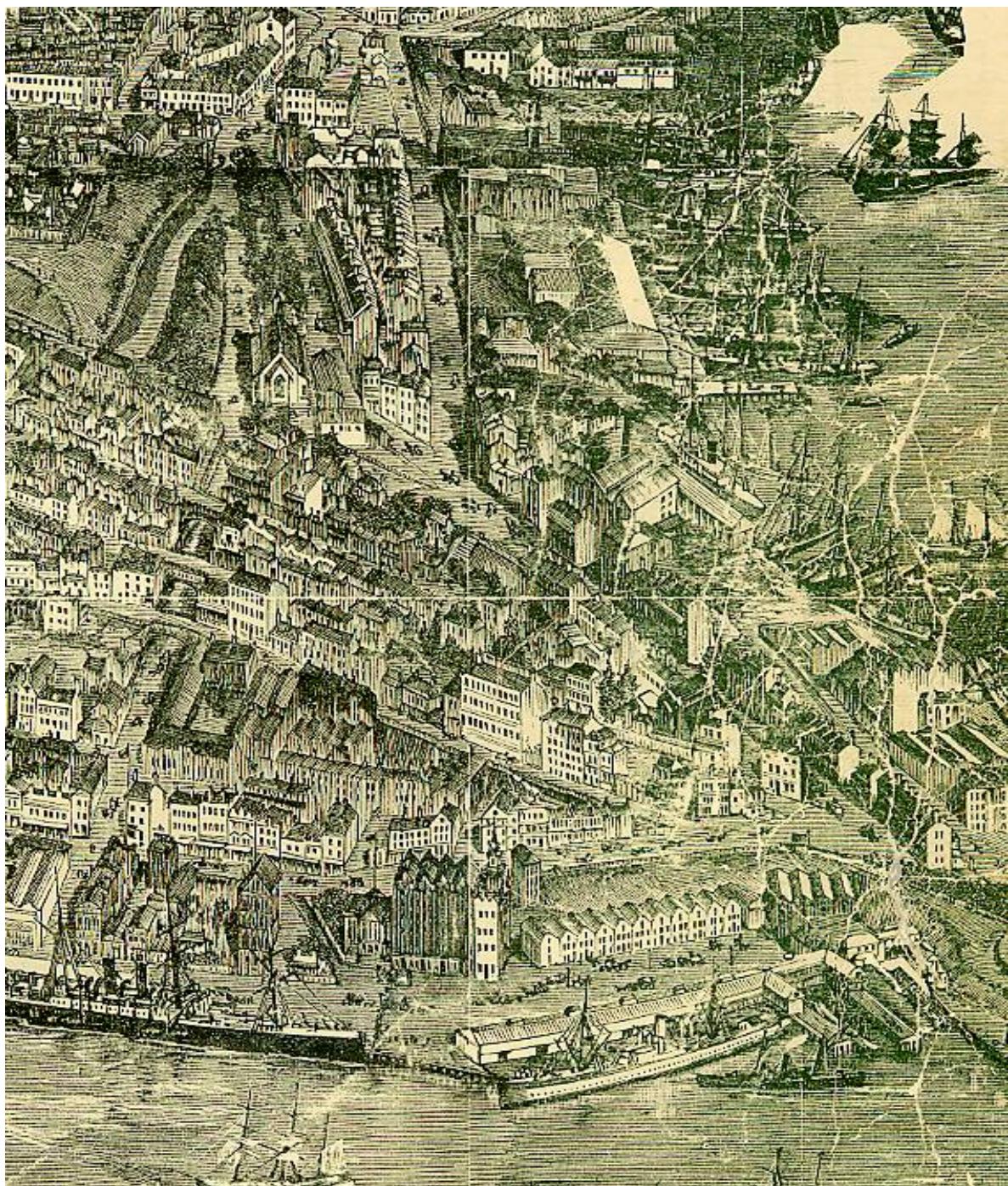


Figure 2.18 The City of Sydney, 1890, reproduced from *Sydney Takes Shape*, Kelly, Max and Crocker, Ruth, Macleay Museum, 1978. This image was produced for a newspaper supplement and it is difficult to determine its accuracy.

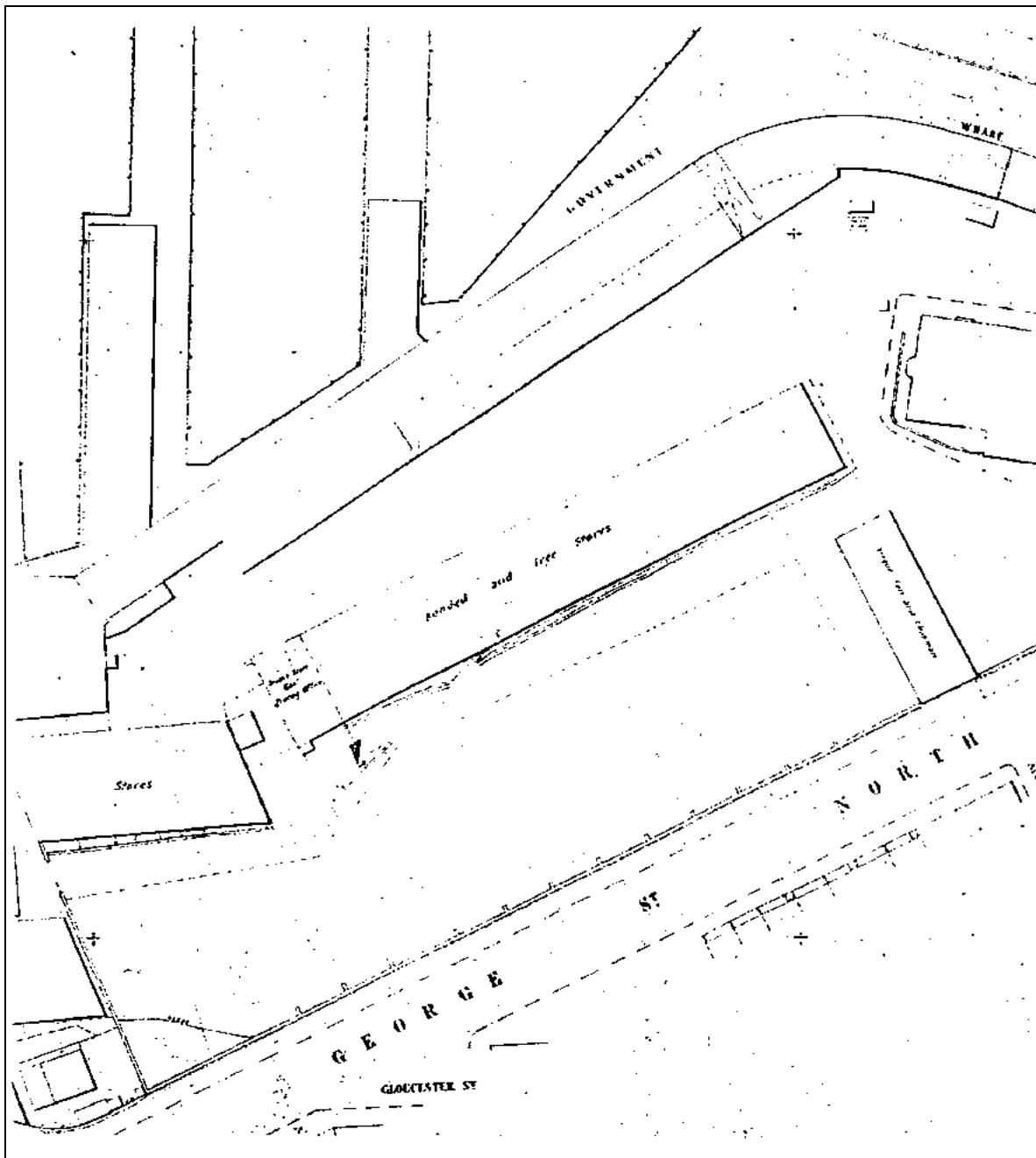


Figure 2.19 Metcalfe Stores, 1895. The Metropolitan Detail Survey of 1895 showed the stores with further details of their use.
(Source: ML Map M.Ser 4/811.17/1, City of Sydney, Sheet Q4)

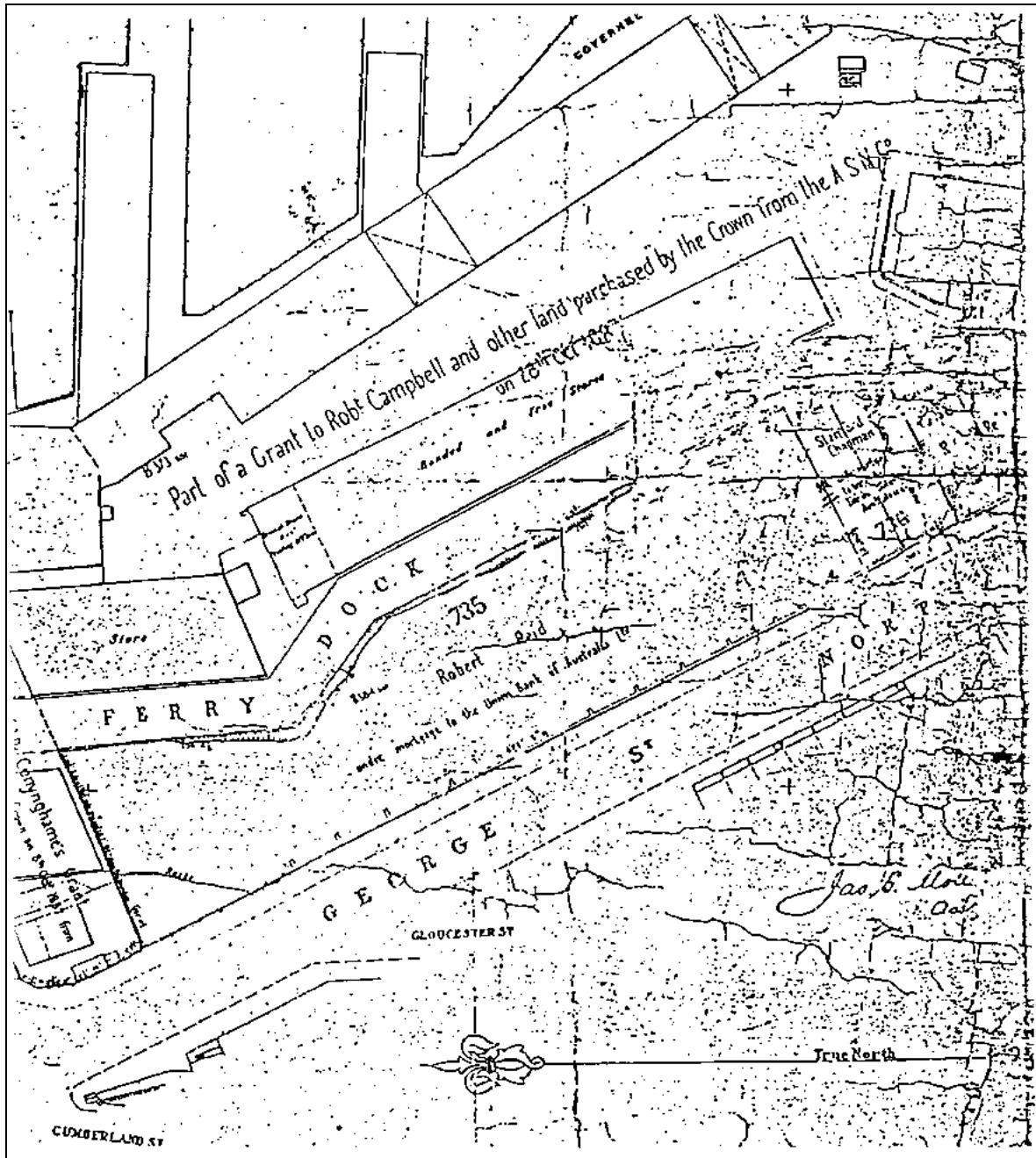


Figure 2.20 Darling Harbour Resumption, 1901. The Resumption Plan appears to have been based upon the 1895 Detail Survey, so no additional details can be gleaned from it. (Source: Darling Harbour Resumption Plan, 1901 Plan v, SCA)



Figure 2.21 Campbell's Cove, 1904. The Sydney Harbour Trust pattern-design wharf sheds are complete and the wharf deck is still under repair. Norddutcher Lloyd wharf at far left. (Source: Photograph by Melvin Vaniman DL Pg 23 Mitchell Library).



Figure 2.22 Campbell's Stores c1860. Note the staircases leading up to the pedestrian door of the second level of each store and that the southernmost bay is still under construction. This bay was demolished to make way for the infrastructure of the Overseas Passenger Terminal.



Figure 2.23 Campbell's Store c1970. The stores at this stage were home to Carmels Wine and perhaps a single restaurant. They had suffered from severe rising damp and soft degrade and the staircases were missing off most second levels.



Figure 2.24 The typical arrangement of the pads for the beams. The present pad, which holds the beam to support the floor joists for Level 3, has been packed to raise it to the correct height. The earlier pad shown immediately below, has the three vertical cuts indicating the form and position of the roof truss.

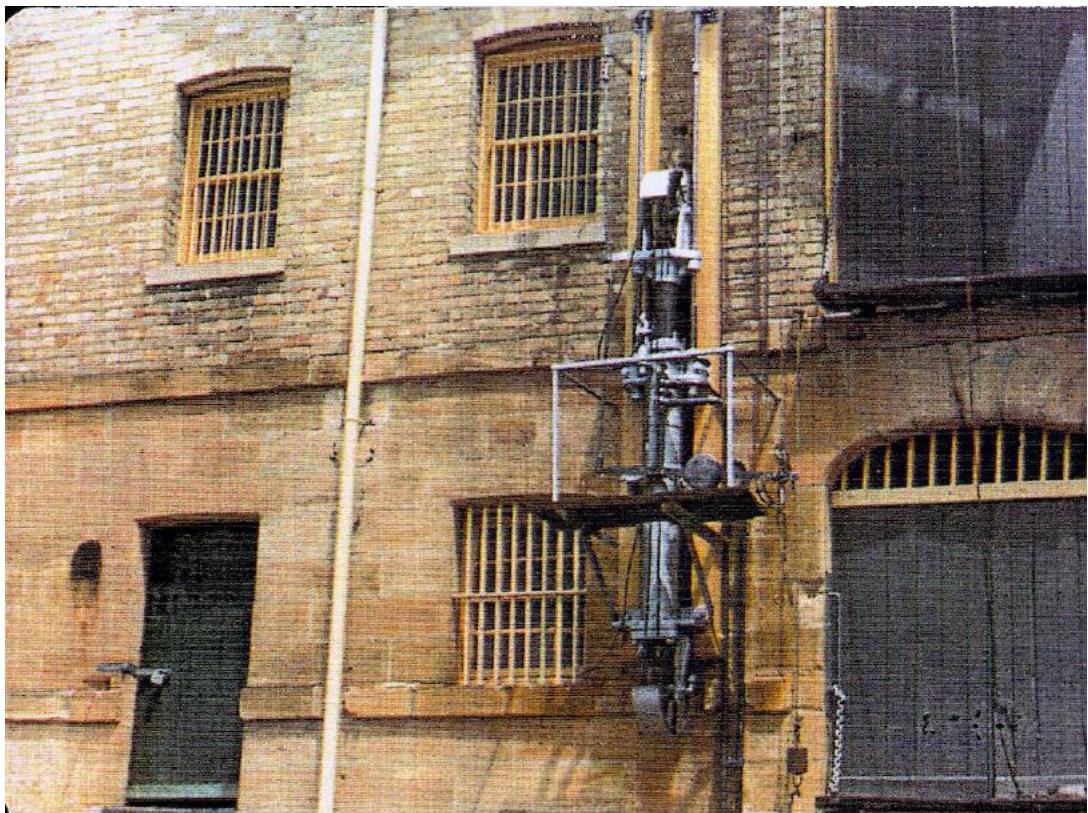


Figure 2.25 The hydraulic hoist outside Bay 1. Note the hoist is complete in almost all details, with the hoist cable, the activating lever and the activating lever cable intact. A similar hoist was outside Bay 8.



Figure 2.26 The building in June 1977 showing the decayed outer 100mm of stone removed. (Source: CCM105, June 1977)

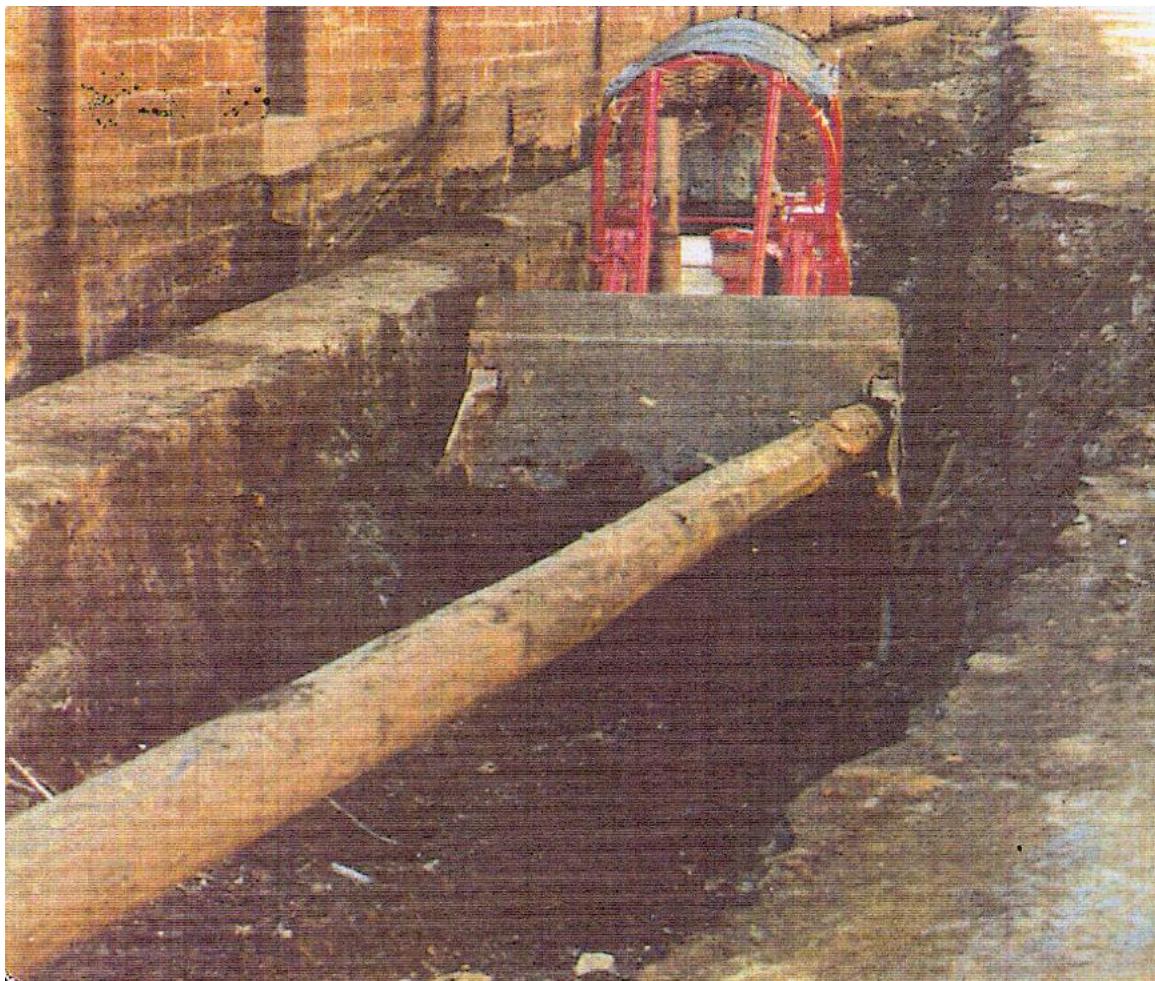


Figure 2.27 The concrete retaining wall about 800mm from the west wall of the building, in 1977.

2.7 Endnotes

- ¹ Both the Australian Heritage Commission (national) and the NSW Heritage Council (state) have identified themes for research relating to places of heritage significance. www.heritage.nsw.gov.au
- ² NSW Heritage Office, *Heritage Information Series, Historical Research for Heritage*, Baskerville, Bruce, (2000) p. 2.
- ³ Attenbrow Val. *Sydney's Aboriginal Past* Introduction p 3. 2003 University of NSW Press
- ⁴ Attenbrow 2003 Table 2.1 pp 9-13
- ⁵ *Historic Records NSW* 15 May 1788 Governor Phillip to Lord Sydney pp 121-36 Government Printer Sydney.
- ⁶ Godden Mackay Logan. *The Rocks – Heritage Management Plan* pp 7 Prepared for Sydney Harbour Foreshore Authority June 2001
- ⁷ Bennelong's Estimate to Phillip 13 Feb 1790 in Historical Records, letter from Governor Phillip to Lord Sydney pp 304-10
- ⁸ Hinkson Melinda. *Aboriginal Sydney* pp xix 2001 Aboriginal Studies Press Canberra
- ⁹ Most of this section is based upon Steven, M 1966 'Robert Campbell (1769-1846)', *Australian Dictionary of Biography*, Melbourne University Press, Melbourne, pp 202-6; and Steven, M 1965 *Merchant Campbell 1769-1846: A Study of Colonial Trade*, Oxford University Press, Melbourne..
- ¹⁰ Steven, M 1965 *Merchant Campbell 1769-1846: A Study of Colonial Trade*, Oxford University Press, Melbourne, p 255-6.
- ¹¹ Grants Register, TG2 (vol 6), p 15, Land Titles Office (LTO).
- ¹² Prince, Metcalfe Stores, p 21.
- ¹³ Prince, Metcalfe Stores, p 21.
- ¹⁴ Kerr J & H Falkus 1982, *From Sydney Cove to Duntroon: A Family Album of Early Life in Australia*, Hutchinson, Richmond, Vic, pp 8-9.
- ¹⁵ Archives Office, Map SZ 469.

- ¹⁶ Removal of Campbell's Wall (Correspondence etc), *V&PLANSW*, 1879–80, V, p 1196.
- ¹⁷ Removal of Campbell's Wall (Correspondence etc), *V&PLANSW*, 1879–80, V, p 1200.
- ¹⁸ Surveyor-General, Sketch Books, No. 2. f9.
- ¹⁹ 'Removal of Campbell's Wall (Correspondence etc)', *V&PLANSW*, 1879–80, V, p 1226–7.
- ²⁰ Col Sec Special Bundles, Magistrates Returns, 1831–42, Archives Office Of NSW (AONSW), 4/7267.
- ²¹ Col Sec Special Bundles, Magistrates Returns, 1831–42, AONSW 4/7267.
- ²² *Australian*, 6 May 1836.
- ²³ *Sydney Gazette (SG)*, 4 June 1836, p 1.
- ²⁴ SG, 21 June 1836, p 3.
- ²⁵ Campbell's Wharf (Plans, etc, of Proposed Alterations by the ASN Company), *V&PLANSW*, 1876–7, II, p 783.
- ²⁶ JS Prout & John Rae, *Sydney Illustrated*, Part II, Sydney 1842, opposite p 11; p 13.
- ²⁷ Land Titles Office, Deed Registers (LTOD), No. 289 Bk 4.
- ²⁸ Recited in LTOD, No. 738 Bk 12.
- ²⁹ Some thirty years ago, architecture student C Prince prepared an architectural thesis about the history of these buildings. This study has formed the basis for all subsequent work on Campbell's Stores. It was a meticulous piece of research and compared well with many contemporary assessments of buildings. However, there is now a much wider range of sources available and our better understanding of the history of commercial development in Sydney has evolved considerably since then. Prince estimates that the stores were commenced about 1838–43 largely upon the evidence of Conrad Martens' sketch of 1835–51 (see Figure 2.4). This is not the case as an examination of the Sydney City Council Rate Assessment Books shows. Prince has documented the various building campaigns which saw the erection of these stores. However, some of the photographic and artistic images he has used do not possess sufficient verification of their date. The stores remained as two storeys for some years. Between about 1882 and 1887, as part of the upgrading of their port facilities by the Australasian Steam Navigation Company, a third storey was added to the buildings, see below for evidence. Prince estimated that this work was carried out by the Public Works Department after the government purchased the wharf but this is not the case, as documentary evidence shows. Prince surmised that the first five bays of the Metcalfe Bond stores were built in the period between 1838–43. This is not supported by any of the evidence.²⁹ The first rate assessment of the City of Sydney taken in 1845 showed the following structures on Campbell's Wharf. It identified a house, stores, warehouse and wharf valued at £1,000. At the 'north end of Campbell & Co wharf' were three stores plus an office and store, all of three storeys, with slate roofs, valued at £150 each, two of which were vacant and two occupied by Smith and Campbell. Additionally, there was a cottage for the overseer George Atherden, and an empty timber woolshed.²⁹ The stores identified here appear to be the older stone stores on the waterfront built in the 1820s. Evidence for the building of these stores appear later, after certain legal transactions had taken place.
- ³⁰ LTOD, No. 738 Bk 12.
- ³¹ LTOD, No. 559 Bk 14.
- ³² LTOD, No. 47 Bk 13.
- ³³ See, for example LTOD, No. 817 Bk 22.
- ³⁴ LTOD, No. 896 Bk 14; No. 897 Bk 14.
- ³⁵ LTOD, No. 540 Bk 28.
- ³⁶ Bourke RAB, 1851–2, Nos. 8821–8825.
- ³⁷ SCA Photo SC 7.
- ³⁸ Wells WH, Plan of Sydney, ML Map M4/811.17gbbd/1850/1; Mitchell, TL, Trigonometrical Survey of Sydney, ML Map M4/811.15/1853/1; Sands & Kenny, Map of Sydney and environs, ML Map M3/811.17/1858/1.
- ³⁹ EA Beever, 'The Pre-gold Boom in Australia 1843–51', *Australian Economic History Review*, XIX, 1, March 1979, pp 1–25.
- ⁴⁰ Prince, Metcalfe Stores, p 39.
- ⁴¹ Bourke RAB, 1858, Nos. 26–29.
- ⁴² Prince, Metcalfe Stores, p 39.
- ⁴³ Jevons, WS Photoprints of WS Jevons' photograph album, 1858, PXB 73.
- ⁴⁴ Moresby, MF Album, 1856–60, ML PXA 9. This same image is reproduced with date 1857 in Groom, B & W Wickman, *Sydney — The 1850s: The Lost Collections*, University of Sydney, Sydney, 1982, p 29.
- ⁴⁵ Bourke RAB, 1861, Nos. 781/954 — 793/966.
- ⁴⁶ MWS & DB, 1865 Trigonometrical Survey of Sydney, Sheet B2.
- ⁴⁷ CET Newman, 'R Campbell (1804–1859)', ADB, v 1, p 206.
- ⁴⁸ Sands, *Directory*, 1858–9, p 28.
- ⁴⁹ Sands, *Directory*, 1861, p 29.
- ⁵⁰ Sands, *Directory*, 1863, p 32.

- ⁵¹ AO Photo 1077.
- ⁵² *Australian Town and Country Journal*, 20 Nov 1875, p 820.
- ⁵³ *Australian Town and Country Journal*, 20 Nov 1875, p 820.
- ⁵⁴ *Illustrated Sydney News*, 10 Nov 1877, pp 4, 6.
- ⁵⁵ Campbell's Wharf (Plans, etc, of Proposed Alterations by the ASN Company), *V&PLANSW*, 1876–7, II, p 785.
- ⁵⁶ Unfortunately, copies of this plan are not available at either the LTO or DLWC Plans Room.
- ⁵⁷ Captain FH Trouton, *Handbook and History of the Australasian Steam Navigation Company*, JL Holmes, Sydney, 1884, p 18.
- ⁵⁸ LTOD, No. 265 Bk 168.
- ⁵⁹ Campbell's Wharf (Plans, etc, of Proposed Alterations by the ASN Company), *V&PLANSW*, 1876–7, II, p 787–8.
- ⁶⁰ LTOD, No. 266 Bk 168.
- ⁶¹ LTOD, No. 267 Bk 168.
- ⁶² LTOD, No. 192 Bk 169.
- ⁶³ *Illustrated Sydney News*, 10 Nov 1877, pp 4, 6.
- ⁶⁴ This appears to be the road to the Horse Ferry Dock. Sketch is supplied but not dated. It is possibly c1878. 'Removal of Campbell's Wall (Correspondence etc)', *V&PLANSW*, 1879–80, V, p 1226–7.
- ⁶⁵ Sands, *Directory*, 1878, p 132.
- ⁶⁶ CT 387 f. 131.
- ⁶⁷ CT 387 f. 131.
- ⁶⁸ Selfe, N 1902, 'A Century of Sydney Cove and the Genesis of Circular Quay, *JRAHS*, p 62.
- ⁶⁹ CT 387 f. 131.
- ⁷⁰ Selfe, N 1902 loc cit.
- ⁷¹ Sydney Subdivision Plans — City of Sydney, Area 811.1719, No. 1.
- ⁷² Trouton, Captain FH 1884, *Handbook and History of the Australasian Steam Navigation Company*, JL Holmes, Sydney, p 18.
- ⁷³ Sands, *Directory*, 1882, p 128.
- ⁷⁴ SCA Photo CC 7.
- ⁷⁵ S.46. Sh 4.
- ⁷⁶ Trouton, Captain FH 1884, *Handbook and History of the Australasian Steam Navigation Company*, JL Holmes, Sydney, , p 19; Selfe, N 1902, 'A Century of Sydney Cove and the Genesis of Circular Quay, *JRAHS*, p 62.
- ⁷⁷ CT 387 f. 131.
- ⁷⁸ Dove, H Percy 1882, Plans of Sydney Executed for Insurance Companies, Sydney, p 16.
- ⁷⁹ Dove, H Percy 1887, *Plans of Sydney – Supplement*, Sydney, Sec 38.
- ⁸⁰ Prince, Metcalfe Stores, p 62.
- ⁸¹ McKellar, NL 1977, *From Derby Round to Burketown: The AUSNStory*, University of Queensland Press, St Lucia, p 87.
- ⁸² McKellar, NL 1977, op cit, p 68.
- ⁸³ Curtis, E, *An History of the Australasian Steam Navigation Company and the Australasian United Steam Navigation Company Ltd*, pp 15–17.
- ⁸⁴ The Australasian Steam Navigation Company's Wharf, Circular Quay (Correspondence Relating to the Purchase of), *V & P L A N S W*, 1887–8, III, pp 1013–4
- ⁸⁵ The Australasian Steam Navigation Company's Wharf, Circular Quay (Correspondence Relating to the Purchase of), *V & P L A N S W*, 1887–8, III, p 1014–5.
- ⁸⁶ The Australasian Steam Navigation Company's Wharf, Circular Quay (Correspondence Relating to the Purchase of), *V & P L A N S W*, 1887–8, III, p 1016.
- ⁸⁷ The Australasian Steam Navigation Company's Wharf, Circular Quay (Correspondence Relating to the Purchase of), *V & P L A N S W*, 1887–8, III, Plan at p 1019.
- ⁸⁸ LTOD, No. 192 Bk 169; CT 387 f. 131.
- ⁸⁹ Prince, Metcalfe Stores, p 58.
- ⁹⁰ Prince, Metcalfe Stores, p 62.
- ⁹¹ NSW — Dept of Lands, Metropolitan Detail; Survey, M Ser 4/811.17/1, City of Sydney, Sheet Q4, 1889.
- ⁹² NSW — Dept of Lands, Metropolitan Detail; Survey, M Ser 4/811.17/1, City of Sydney, Sheet Q4, 1895.
- ⁹³ NSW — Public Works Department, *Annual Reports*, 1890, p 8.
- ⁹⁴ Sands, *Directory*, 1901, p 1607.
- ⁹⁵ AO Map 519.
- ⁹⁶ Darling Harbour Resumption Plans, 1901, SCA.
- ⁹⁷ Sydney Harbour Trust, Register of Properties vested in the Sydney Harbour Trust, 1901–13, AONSW 6/14984, p 13.

- ⁹⁸ Sydney Harbour Trust, Register of Properties vested in the Sydney Harbour Trust, 1901–13, AONSW 6/14984, p 13.
- ⁹⁹ Sydney Harbour Trust, *Annual Report*, 1914, p 16.
- ¹⁰⁰ Sydney Harbour Trust, *Annual Report*, 1915, p 17.
- ¹⁰¹ Prince, Metcalfe Stores, p 66, 70.
- ¹⁰² Prince, Metcalfe Stores, p 70.
- ¹⁰³ Prince, Metcalfe Stores, p 70.
- ¹⁰⁴ Based on material prepared by Fred Yarad of Godden Mackay Pty Ltd.
- ¹⁰⁵ 'Removal of Campbell's Wall – (Correspondence etc)', *V&PLANSW*, 1879–80, V, p 1196–7.
- ¹⁰⁶ Col Sec Special Bundles, Magistrates Returns, 1831–42, AONSW 4/7267.
- ¹⁰⁷ Col Sec Special Bundles, Magistrates Returns, 1831–42, AONSW 4/7267.
- ¹⁰⁸ Col Sec Special Bundles, George Street North extensions, 1836–41, AONSW 4/2529.5.
- ¹⁰⁹ Campbell's Wharf (Plans, etc, of Proposed Alterations by the ASN Company), *V&PLANSW*, 1876–7, II, p 783.
- ¹¹⁰ LTOD, No. 289 Bk 4.
- ¹¹¹ Bourke RAB, 1845, Nos. 2–8, SCC Archives.
- ¹¹² LTOD, No. 440 Bk 11.
- ¹¹³ LTOD, No. 738 Bk 12.
- ¹¹⁴ LTOD, No. 47 Bk 13.
- ¹¹⁵ LTOD, No. 559 Bk 14.
- ¹¹⁶ LTOD, No. 896 Bk 14.
- ¹¹⁷ LTOD, No. 897 Bk 14.
- ¹¹⁸ LTOD, No. 817 Bk 22.
- ¹¹⁹ LTOD, No. 540 Bk 28.
- ¹²⁰ LTOD, No. 196 Bk 30.
- ¹²¹ LTOD, No. 99 Bk 44.
- ¹²² Bourke RAB, 1858, Nos. 26–29.
- ¹²³ Sands, *Directory*, 1858–9, p 28.
- ¹²⁴ Bourke RAB, 1861, Nos. 781/954 – 793/966.
- ¹²⁵ Sands, *Directory*, 1861, p 29.
- ¹²⁶ Sands, *Directory*, 1863, p 32.
- ¹²⁷ MWS & DB, 1865 Trigonometrical Survey of Sydney, Sheet B2.
- ¹²⁸ AO Photo 1077.
- ¹²⁹ LTOD, No. 891 Bk 145.
- ¹³⁰ Australian Town and Country Journal, 20 Nov 1875, p 820.
- ¹³¹ Trouton, Captain FH 1884, op cit, p18.
- ¹³² Selfe, N 1902, loc cit.
- ¹³³ LTOD, No. 265 Bk 168.
- ¹³⁴ Campbell's Wharf (Plans, etc, of Proposed Alterations by the ASN Company), *V&PLANSW*, 1876–7, II, p 787–8.
- ¹³⁵ LTOD, No. 266 Bk 168.
- ¹³⁶ LTOD, No. 267 Bk 168.
- ¹³⁷ LTOD, No. 192 Bk 169.
- ¹³⁸ Campbell's Wharf (Plans, etc, of Proposed Alterations by the ASN Company), *V&PLANSW*, 1876–7, II, p 788–9.
- ¹³⁹ Campbell's Wharf (Plans, etc, of Proposed Alterations by the ASN Company), *V&PLANSW*, 1876–7, II, p 789.
- ¹⁴⁰ *Illustrated Sydney News*, 10 Nov 1877, pp 4, 6.
- ¹⁴¹ Sands, *Directory*, 1878, p 132.
- ¹⁴² CT 387 f. 131.
- ¹⁴³ CT 387 f. 131.
- ¹⁴⁴ CT 387 f. 131.
- ¹⁴⁵ Sands, *Directory*, 1882, p 128.
- ¹⁴⁶ CT 387 f. 131.
- ¹⁴⁷ Dove, H Percy 1887, loc cit.
- ¹⁴⁸ Prince, Metcalfe Stores, p 62.
- ¹⁴⁹ The Australasian Steam Navigation Company's Wharf, Circular Quay (Correspondence Relating to the Purchase of), *V & P L A N S W*, 1887–8, III, pp 1013–4.

- ¹⁵⁰ The Australasian Steam Navigation Company's Wharf, Circular Quay (Correspondence Relating to the Purchase of), *V & P L A N S W*, 1887–8, III, p 1014–5.
- ¹⁵¹ The Australasian Steam Navigation Company's Wharf, Circular Quay (Correspondence Relating to the Purchase of), *V & P L A N S W*, 1887–8, III, p 1016.
- ¹⁵² CT 387 f. 131.
- ¹⁵³ LTOD, No. 192 Bk 169; CT 387 f. 131.
- ¹⁵⁴ NSW — Dept of Lands, Metropolitan Detail Survey, M Ser 4/811.17/1, City of Sydney, Sheet Q4, 1889.
- ¹⁵⁵ Darling Harbour Resumption Plans, 1901, SCA.
- ¹⁵⁶ Sydney Harbour Trust, Register of Properties vested in the Sydney Harbour Trust, 1901–13, AONSW 6/14984, p 13.
- ¹⁵⁷ SHT plan St P 110 XL in SCA Historic Plan collection.
- ¹⁵⁸ Prince, Metcalfe Stores, p 70.

3.0 Physical Evidence

The aim of this Section is to describe Campbell's Stores in detail, in order to facilitate the understanding of the place. The physical fabric of Campbell's Stores is evidence of the extent of alterations that have been carried out during the building's lifetime. This Section builds on the evidence outlined in the previous section to document the physical changes that have taken place over time in order to understand why, when and how these alterations were made.

An analysis of the history and the physical fabric of Campbell's Stores suggests that it can be grouped in the following manner:

- Original fabric from the first three phases of construction, up to 1885, including the hoists and winches.
- Bay 11 constructed to house the Branch Stores Government Printer c1895–1915.
- Altered fabric from these periods modified as a result of the construction of Hickson Road, for example new door openings made on Level 2.
- Reconstruction of fabric in 1970s works, for example sandstone restoration of face of eastern ground-floor wall.
- Fabric introduced to provide for new uses in 1970s works, for example service tunnel at rear.
- Elements introduced primarily for decorative reasons, for example sailboat masts in front of the Waterfront Restaurant.

3.1 Street and Streetscape Description

The current setting of Campbell's Stores does little to evoke the strong historic associations between the building and the past maritime activity of this area. The Bicentenary works of 1988, as well as the nearby Park Hyatt Hotel construction and the adaptive reuse of the northern section of the Overseas Passenger Terminal, reinforce the role of this area as a recreational landscape. However, the tourist sailing ships that berth at the new wharf in Campbell's Cove do provide some connection to this earlier maritime history.

Campbell's Stores and the other heritage buildings within its setting provide evidence of the periods of maritime development around Campbell's Cove. The stores themselves represent the early- to mid-nineteenth-century period, associated with the Campbell family. The ASN Co building represents the late-nineteenth-century period and provides a backdrop to Campbell's Stores, when viewed from the harbour. It is also a reminder of the period of ASN's ownership of Campbell's Stores. The early-twentieth-century Metcalf Stores also provides a backdrop to Campbell's Stores, when viewed from the harbour. The Overseas Passenger Terminal represents the mid-twentieth-century changes to Circular Quay, when human cargo and recreation began to dominate this cultural landscape.

The awning over the outdoor seating area for the restaurants of Campbell's Stores is an intrusive and dominant horizontal element, which interferes with an understanding of the vertical emphasis of the building and its division into vertical bays. The ship's masts, located in front of the Waterfront Restaurant, confuse an authentic appreciation of the building.

The west facade of Campbell's Stores fronts Hickson Road. Hickson Road was originally a wide rear laneway and the contemporary road was constructed during the twentieth century. A new concrete retaining wall was built some 600mm to the west of the building (Figure 2.27). This retaining wall held back the fill on which Hickson Road was laid. Photographic evidence¹ indicates that there was a bridge or slab spanning the space between the retaining wall and the building. This prevented fill from falling into the space and coming in contact with the wall. The footpath, which was constructed on this bridging material, however, did contact and mark the wall and is evident in the photographs of the facade.

3.2 Building Description

Campbell's Stores consists of 11 bays, orientated east-west and the building as a whole is orientated north-south. Each rectangular bay is gable-fronted. The simple roofs are slate sheathed with lead capping and copper-lined trough gutters, with a box gutter running the length of the dividing wall between bays. The original two storeys are constructed from sandstone ashlar, both externally and internally, and the third floor addition is face brickwork.

Bays 1–10 are almost identical, measuring 8.1m wide and 15.8m long. Each bay has three floors which are referred to as Level 1 (the ground floor), Level 2 (the middle floor) and Level 3 (the top floor). The ground floor in all bays is presently a concrete slab, while Level 2 and Level 3 floors are timber boards on timber joists. Each level has a cargo door opening on the eastern side of each bay, which allowed for items to be winched up to the openings on Levels 2 and 3 and moved into the bay. The cargo doors are centred within the façade of each bay and there is a window opening on either side of the cargo doors.

The northernmost bay (Bay 11) was the last built in c1890 and is entirely constructed from brickwork. It abuts the homogenous series of 10 bays which are immediately to the south. Bay 11 has a goods apron running up the centre of the bay on the eastern elevation and the floors have been lowered, along with the windows, while a fourth floor has been introduced beneath the roof (Figure 3.24).

Building Development Phase 1: 1851–1852

Construction of the first five two-storey bays for Robert Campbell Junior (Bays 6–10 in the current building).

Building Development Phase 2: 1858–1861

Construction of a further six two-storey bays, possibly in two campaigns for John Campbell (Bays 1–5 in the current building, with one bay to the south of Bay 1 removed in 1958).

Building Development Phase 3: c1885

This phase begins with construction of the brick third floor to Campbell's Stores c1885 by the Australasian Steam Navigation Company (ASN).

Building Development Phase 4: c1895–1915

After the NSW Government purchased the buildings in 1887 the Sydney Harbour Trust took control of the stores in 1901. The Stores Branch of the Government Printing Office was constructed as a separate building, possibly in c1895 or 1915. Alterations were made as a result of the Hickson

Road construction in 1915, including entries at Level 2 from Hickson Road, and cutting the corner of Bay 10 (Figure 5.1d).

Building Development Phase 5: Post-1958

One bay at the southern end was removed for the construction of the Overseas Passenger Terminal in 1958. Redevelopment undertaken by the Sydney Cove Redevelopment Authority in the 1970s included adaptive works for restaurant usage (eg service tunnel) and sandstone replacement. Change in floor levels in the former Government Printing Office/Commonwealth Film Censor building (Bay 11) and linkage via new openings to Campbell's Stores were undertaken for the fit-out of the Italian Village restaurant in 1986.

3.3 Identification of Fabric Development—External

The first five bays to be constructed were built in 1851/1852 and are the present Bays 6–10. A further three bays, which are the present Bays 3–5, were completed around 1858 and the final three bays were finished by 1860. Of these last three bays, the southernmost and final one to be completed was demolished in 1958 to make way for the first Overseas Passenger Terminal at Circular Quay. The building is stepped up towards the south, with a rise of about 0.3m between Bays 1 and 2 and Bays 4 and 5, the change in level being most evident at the string course at gable level and the second-level sill course. When originally completed, the building was only two-storeys high.

The stone on both east and west facades of all bays is ashlar with tooled margins and lightly picked faces. The gables rose some 500mm above the level of the lead ridgeline of the slate sheathed roof and had a prominent sandstone coping. The gable had a round circular vent below the apex of the gable.

When constructed, each gable-ended section or bay had a centrally placed double-width ledged and braced pair of doors on both Level 1 and Level 2 of the east facade, for loading of goods. All doors on both levels were hinged with wishbone strap and gudgeon hinges which opened inwards. The second-level doors were fitted with a heavy goods apron attached to the doorway reveals by heavy chain and supported by wishbone strap and gudgeon hinges. Each bay also had a cat-head hoist with a single sheave for raising goods to Level 2. There is no evidence that any machine was employed prior to 1890 to raise goods and hoisting was most likely done by hand or with horses.

The door openings on both levels were arched and fitted with a horizontal timber lintel. The area between the jamb and the arch was fitted with short, vertical wrought-iron bars of about 30mm square section.

On Level 1 (east elevation), the doors were flanked on either side with a window opening 1.2m wide and 1.6m high and about 0.9m above ground level. Eight vertical 30mm, square-section, wrought-iron bars were fitted into the sill and lintel, with a horizontal spacing bar attached to the reveals about two thirds of the bar length above the sill. On Level 2, the central doors were flanked on the south by a window and on the north by a ledged and braced pedestrian door. The door and window were the same width as the openings on level 1, while the window itself was shorter than the ones below. The window opening was fitted with bars in the same manner as the windows on Level 1.

The pedestrian door on Level 2 was accessed by a staircase which ran from ground level up from the north at a sharp angle and terminated in the small cantilevered platform immediately outside the door. This staircase provided the only access to Level 2 and nowhere is there evidence that

internal staircases had been constructed to lead between the two lower floors. Thus, when completed, the stores consisted of 22 secure spaces.

All the original fenestration appears to have been wooden shutters, which could be opened to allow light and ventilation. All shutters on the window openings are also assumed to have been ledged and braced in the manner of that of the northern side of the door in Bay 1, Level 1.

On the west or rear elevation there appears to have been a single, centrally-placed window on both levels, similar to the size of the ground-floor windows on the east elevation. No doors opened into the rear laneway, probably for security reasons.

When completed around the 1860s, Campbell's Store was an exceptionally handsome industrial building consisting of 11 gable-roofed, two-storey bays constructed in Sydney sandstone. On the east or front facade each bay had a centrally-located, arch-headed, double-door loading bay on each level, with two flanking openings. The pedestrian doorway on Level 2 was accessed by a steep staircase. The pattern created by the row of external staircases gave visual relief to the otherwise regular facade. Apart from the continuous string course at the junction between gable and wall, a sill course at Level 2, and slightly protruding Level 1 window sills, the east elevation was unadorned. The buildings had a certain dignity and were a feature of the waterfront through their proportion and design, rather than through any excessive articulation of the stonework.

On the west facade the building was devoid of all articulation except for the slightly projecting sills of the centrally-placed window on both levels.

Some time between 1882 and 1887, the third level was added to the stores. Unlike the lower two levels, this addition was in dry-pressed, well-consolidated brick, laid in English bond. The longitudinal walls between bays were continued to a point some 500m above the valley line to form a true fire wall. Here they were capped with a stone coping.

The stone gable end was completely removed and replaced with brick. The coping on the gable ends was dispensed with and the roof was allowed to project forward above the gable and was finished with a barge board. The external staircases remained intact on the east facade and the gable string course on this facade was removed beneath the Level 3 loading doors to permit the installation of goods aprons.

The lead-lined gutters which ran either side of the dividing fire walls terminated at rainwater heads, and downpipes of unknown cross section ran down both facades.

Windows at Level 3 flanked the central goods doors and were the same width as those below, but slightly shorter. When complete, all windows had arch bars with a two soldier course arch above and had bars fitted. All windows had sandstone sills.

The central pair of Level 3 goods doors are ledged and braced and run on top and bottom rails and rollers. The lintel on this floor is a cast-iron webbed section with an arch top and a flat bottom. The arch (which is flatter than on Level 2) was surmounted by a single row of bricks on end.

No vent was provided in the gable ends as the gap created between brickwork and slate by the purlin was sufficient for ventilation. The windows on Level 3 in Bays 1–8 are 2 x 6 pane, double-hung sash windows, while those in Bays 9 and 10 are inward opening casement windows.

The Percy Dove Plan Revision, published in 1887 (Figure 2.13), shows there were openings in the Level 3 longitudinal walls between existing Bays 2/3 and 3/4 as well as between Bays 5/6, 6/7, 8/9,

and 9/10. Access to Level 3 was via an internal staircase from Level 2 to Level 3. It is possible that all bays were fitted with these stairways; however, the evidence is inconclusive. The high degree of security previously afforded by the earlier configuration was lost or at least diminished by the introduction of the Level 3 interconnecting doorways. The 1975 drawings of Devine Erby Mazlin show small secure lobbies surrounding the staircase on Level 2 in some bays. These lobbies would have allowed access from the external stair directly to Level 3 without entering the secure section on Level 2.

Window openings on the upper floor are intact, as are the window frames. In many cases the glass, which exhibits the characteristics of blown and cut cylinder glass of the 1840s–90s, appears original.

Two hydraulic hoists were introduced to the east facade of the building between the construction of the third floor and the turn of the century. These hoists could have been powered by a local steam system, possibly by the Tangye Brothers gas engine that was located within the building until the early 1990s, when it was removed, repainted and placed in storage by the SCA. An engine like this could easily have powered a hydraulic accumulator for the operation of two simple hoists. It is also possible that they were connected to the reticulated network of the Sydney Hydraulic Power Company (from 1891), or the hydraulic system of Walsh Bay Wharves may have been connected to the stores when both operations were controlled by the Sydney Harbour Trust.

The hoists are of the inverted, triple-sheathed, short type and were capable of hoisting loads in excess of one tonne. The twin cable sheaths and maintenance platforms have been removed from above the Level 3 doors. The valving lever activating systems and piping have been removed. The use of double sheaves on the top platform suggests that these hoists operated with a sheaved lifting hook, for mechanical advantage.

Until the 1970s, Campbell's Stores were at the same level as the street that then passed in front, and vehicles could park directly in front of the Stores. The creation of a forecourt in front of Campbell's Stores at a lower level than the pedestrian promenade has changed this direct relationship, resulting in a degree of spatial separation with the waterfront. The construction of four ships' masts, as part of the presentation of the Waterfront Restaurant, has also impacted on the relationship between the building and the water's edge.

The northern courtyard space, adjacent to Bay 11 (the former Government Printing Office store) was once the site of a cottage and more recently an open area for storage. This space is now part of the Italian Village restaurant lease and includes a glass pavilion structure and paving.

3.4 Identification of Fabric Development—Internal

All internal stone was laid in courses, picked but unmargined. As the building was subject to waterblast² and perhaps sandblast³ on all internal surfaces, followed by repointing, the original type of mortar is unknown, without intrusive investigation.

The floor in Level 1 is believed to have been stone flagging while that on the first level was 125mm wide, 35mm thick, butt-jointed hardwood boarding supported on beams measuring 290mm square, and joists 220mm by 90mm, at 450mm intervals. The posts were morticed into the beams and were not fitted with capitals. There were no posts on the second level, giving a large clear space to the whole of this floor. All posts and beams were broad-axed and hand-sawn, while all joists and floorboards were hand-sawn (possibly pit sawn). All nails in the original boards were flat, stamped, wrought-iron, typical of the period. The longitudinal walls between bays were all without penetration

and there were no internal staircases. Hence there was no movement possible between the bays or between levels within the bays. When goods were to be transferred from one floor to another in the same bay, they were either hoisted through the front loading doors or carried by hand up the external staircase.

The Level 3 floor was formed by introducing posts at Level 2, which were supported on the beam immediately above the Level 1 posts. These Level 2 posts were morticed at their lower end into beams and were supported at their ends on timber pads which were 1200mm long and set into the longitudinal walls. These pads are some 40mm above the original timber pads which supported the roof truss. The precise reason for this increase in height is unknown and it appears to have been made on a drawing board, as each timber pad had to be packed by some 40mm to be at the required level.

The original pads, which supported the ends of the roof trusses, are still in place in many cases. These hand-sawn pads were laid on the stonework as the wall was being constructed and were marked with three vertical saw cuts. The central cut was the mid-point of the truss position and was used in precisely positioning the truss. The two flanking cuts represented the edges of the truss. This enabled the trusses, which were not located directly above the beams, to be precisely positioned during construction.

The new pads, which support the beam that in turn supports the Level 3 floor joists, have no such vertical cuts, are positioned directly above the lower beams and are circular-sawn.

It is believed that the original roof trusses were simply raised to Level 3 and re-used. At least two of the trusses have Roman numerals at the extremities of their members to assist in fixing as the pieces were rising to the assembly level. This practice had largely died out by the 1880s. It is not known if any of the timbers, such as the purlins, were re-used but it seems unlikely as the trusses were in different locations to the earlier ones and the new roof, although probably at the same angle, projected beyond the gable end walls by some 400mm.

All the roof trusses sit on heavy, slightly corbelled, chamfered stone beds which are supported on the eighth course of bricks below the gutter course.

The east truss has a kingpost incorporated, which was included not as part of the truss but to hold the inner end of the cat-head beam which is through morticed into it. Most of the kingposts are original while many of the cat-head beams appear to have been reconstructed as the outer or east end was exposed to weather for some 100 years.

A significant amount of original fabric remains in the form of a sandstone wall and timber post and beam framing structure. Less evidence remains of non-structural and minor original fabric, and particularly fabric that reflects the users and uses of the building between 1901 and 1970. A large proportion of the non-structural internal fabric falls into the last two categories identified above and either provides service facilities for the restaurant uses or is decorative in nature.

3.5 Contemporary Alterations and Additions

3.5.1 Alterations Prior to 1970

Campbell's Stores have undergone at least two adaptations. The first was sometime before 1970 and little is known of it. Carmel Wines had a cellar and bottling unit on the ground floor of one bay and it is believed that at least one other restaurant was operating from the stores.

A photograph taken prior to the leasing of any of the stores⁴, but probably around 1970, shows a sign painted on the east elevation of Bay 5 pronouncing 'Metcalfe Bonded and Free Stores'. The building looked in good order, although somewhat shabby. The cat-head beams were missing from Bays 2, 5, 7 and 10, while the hydraulic hoists were intact over Bays 1 and 8.

A wide set of stairs constructed on posts led to the centre door in Bay 5, Level 2, while the traditional smaller staircases were still intact on Bays 1, 9 and 10. The bargeboards appear to be in poor condition and the terminal sections of the boards located immediately in front of the rainwater heads were missing in almost every case.

The sandstone on the east facade had been rendered to a height of about 1200mm for almost the whole length of the wall. This indicates that an attempt was being made to stem the problem of rising damp and the consequent salt degradation. The photograph also indicates that gardens or, at least, incipient gardens, existed intermittently along the length of the building and this may have exacerbated the rising damp. In other places, such as in front of doors, bitumen appears to have been laid right up to the front wall. It is evident from an earlier photograph that there were timber ramps leading from the bitumen road, which appears to be at a somewhat lower level to the doorway in each bay.

The archways over the Level 1 goods door were also in poor condition in several bays. This may have been due to the ingress of water through the stones beneath the doorway on Level 2. This Level 2 doorway was fitted with a goods apron and the threshold was often damaged by goods handling. Further, the threshold was seldom provided with adequate flashing.

The spaces inside the original two levels of Campbell's Stores were not connected either vertically or horizontally, and thus formed twenty-two compartments (two levels by 11 bays). When the third level was constructed, c1885, narrow stairs were added between Levels 2 and 3 and openings were formed in some of the walls between the bays on level 3. Level 3 comprised three separate bay compartments and one of two bays (what is now Bay 1 and the bay demolished in 1958). One lifting device (hydraulic hoist, or roof-mounted winch) served each of these compartments, with Bays 8, 9 and 10 was served by two devices.

A 1972 SCA plan shows that openings were made between some of the bays on Level 2, before the Authority's restoration program commenced in 1973. Prior to 1973 there were no openings between the bays on Level 1.

3.5.2 Alterations During 1970s

In general, the work done during the 1970s has remained unchanged. Documentary evidence is insufficient to precisely determine the date on which alterations were made. Information for this section was primarily derived from plans and photos of the 1970s Development Applications (DAs), an analysis of the fabric undertaken in September/October 1996 and from further inspections undertaken in April 2003 and June 2010.

A major conservation program was undertaken by the then Sydney Cove Redevelopment Authority between 1974–78, based on documentation prepared by Devine Erby Mazlin Architects.

During the 1970s restoration work, the salt-damaged sandstone of the east facade was removed to a depth of about 100mm using mechanical picks (Figure 2.26). The attack appears to be less obvious at the southern end of the building, although several of the arches over the Level 1 doors were replaced in the same contract.

It is not known if a damp-proof course (DPC) was introduced during this work, although it was recommended in the Devine Erby Mazlin drawings of Stage 3 works dated 2 August, 1976. No DPC was evident in the inspections carried out in 1996, 2004 or 2010.

The work, as executed, is evident on cursory inspection as a reconstruction, not only by the colour of the stone, but also by the tooling and picking, which is quite different from the original work. However, the repair is well executed and conforms to Burra Charter principles, in that it is not conjectural reconstruction but, rather, was based on the known original work.⁵

There is little evidence of the original external staircases that led from the ground to the Level 2 pedestrian doorways. The early photographs indicate that the platform at the top was cantilevered from the building but there is no evidence remaining that this was so. A photograph⁶ shows the stubs of the cantilever beams still in position on Bay 6, however the opening from which they protruded was subsequently infilled, albeit inappropriately, with stone during restoration.

There is a series of four brackets which consist of a kingpost and a horizontal member held in place by an inverted steel bracket in Bays 4, 6, 8 and 10, on which signs were once suspended. These signs have been removed but the hooks that held them in place are still evident. The kingposts were fastened by three bolts to the fabric of the wall about 4m from the ground. Two of these brackets now support floodlights. In other cases, floodlights had been attached to the stone facade or the goods apron on the top floor of the building.

3.5.3 The Forecourt

With the opening of the Waterfront Restaurant, three ships' masts, or replica masts, were erected outside Bays 1, 2 and 3. The timber masts, of the square rigger style, are complete with lines, lashings, belaying brackets and stays and are supported by guy wires attached to steel eyes. These eyes are fastened to the wall of the stores and to the retaining wall at the edge of the forecourt. In some cases the eyes were inserted into the mortar between the stones and in other cases, holes were drilled into the stone front of the building around the 3m height.

The masts, although a maritime feature, significantly obscure views to and interpretation of Campbell's Stores and the methods used to stabilise them have damaged the fabric of the building (Figure 3.9).

The forecourt, which runs the full length of the east facade of the stores, is now paved with sandstone flagging. The flagging slopes from east to west to allow drainage. The forecourt is used for outdoor dining. Over time, this area has been subject to accumulated awnings, drop-down screens, signage and waiter stations. During wet weather, water is directed onto the façade of Campbell's Stores by the awning cover to the outdoor dining area, which is causing the sandstone façade to deteriorate.

The forecourt varies in width from about 8m at the north end to about 16m at the south end and is set about 1m below the ground level of the promenade. The level of the promenade was determined by the level of the footpath outside the Overseas Passenger Terminal and is a continuation of it. The promenade was constructed as part of the major upgrading of the Sydney Cove precinct undertaken by the NSW Public Works Department prior to the Bicentenary celebrations in 1988.

The forecourt is marked on its east side by an ashlar stone retaining wall which projects about 150mm above the promenade surface and is capped with a coping stone. The retaining wall is

penetrated by stairs in three locations to allow access to the outdoor dining area and the restaurants behind.

The forecourt immediately in front of the stores is flagged with stone to form a spoon drain. The flagstones were introduced in 1977 and are probably recycled. It appears they were laid directly on the consolidated fill and it is not known if any sort of a membrane was introduced beneath the flagstones. They now exhibit some impact of salt build-up. The repointing, which has been done since they were laid, is in a Portland cement rich mix which will prevent breathing between the stones and will exacerbate the salt problem and accelerate their deterioration.

3.5.4 The Pavilion

The pavilion, located north of the stores, is a square structure measuring about 12m by 12m, which is steel framed and has a corrugated-metal roof (Figure 3.12). All four sides are glazed from floor to ceiling and, on the north and east facades, six of the eight glazed panels slide to the side to allow the dining experience to be almost alfresco. The building has a scalloped decorative lattice panel suspended in front of the glazing on two sides, which acts as a shading device. The pavilion is located adjacent to the newest, and certainly the most altered, of all of the bays of the warehouse.

The pavilion, which was constructed c1990, replaced an earlier timber pergola structure. As far as is known, no other structures have been erected on this part of the site since the cottage shown on early plans was removed.

3.5.5 The Western Facade

By August 1978, the former Level 2 windows had been opened to form a doorway in Bay 9. New doors were opened through the wall to the north of the original window in Bay 4 and to the south of the original window in Bay 5. A further two new windows were opened in Bays 4 and 5, which had an unfortunate impact on the uniformity of the western elevation.

When these windows were inserted, the sills and lintels, and possibly the reveals, were constructed in concrete.⁷ These were subsequently replaced with stone to produce a more acceptable appearance.⁸ Inspection of this work in May 1996 showed that the repointing, in certain areas, was done with a lime-rich mortar containing crushed shells. As the shell was not calcined it appears that it was added to the lime mix to produce an 'authentic' look to the pointing and to possibly match the original. However, a closer inspection of several areas containing what appears to be original mortar, where little intervention has taken place, did not indicate the use of a shell lime.

It is worth noting that photographic evidence⁹ shows that the internal walls were subject to waterblast. This would have removed mortar pointing and subsequent repointing has obscured the original.

3.5.6 Bay 11

Bay 11 was built as a separate building for the Government Printing Office (Figure 3.31). It was used by the Branch Stores section of the Government Printing Office and, more recently, the Commonwealth Film Censor. Originally its floors were at quite different levels to those in Bay 10 and it was constructed entirely of brick in a totally different style.

The building was of three storeys with the east facade having a half-gable roof and the west facade a full gable constructed at an angle. It is not known whether the building was constructed prior to 1915 or after 1915, when this part of Hickson Road was constructed, but the latter seems most

likely. The east facade had a central goods bay which ran from top to bottom of the building and was fitted with a cat-head beam. The two upper sets of goods doors were on rollers and each doorway had a goods apron on chains. Flanking each doorway were narrow, double-hung sash windows with concrete sills. Each window was fitted with vertical bars.

The west facade was symmetrical in articulated brickwork. The central of the three bays had a double set of double-hung sash windows while the flanking bays had single windows at each level. At some stage the bottom southern window on the west facade was enlarged to form a door while a further door was introduced to Level 1 on the east facade. Both alterations disrupted the symmetry of the building and neither were carried out with much sympathy.

The most notable feature of the building was a massive vent built against the southern wall. The vent appeared to commence at Level 1 and continued some 8m above the roof level. It appeared to be 2m wide and 10m long and to have been in reinforced concrete. It was capped with a concrete skillion roof which drained to the north.

Internally, the building was shown, through drawings completed in 1973, to have a set of five steel-doored concrete vaults constructed against the south wall, measuring about 2m by 3.5m on the ground floor. A further three vaults were located on Level 2, while Level 3 appears to be clear space.

During the 1986 refurbishment of Bay 11, the interior of the building was gutted, the floors were lowered to correspond as closely as possible to those in the adjacent bay 10 and a fourth level was created. All windows on the east and north facades were enlarged and lowered to conform to the new floor level. The goods bay on the east elevation, however, retains its goods aprons indicating previous floor levels and the industrial nature of the building.

3.5.7 Internal Fitout

The stores were constructed originally as a series of discrete spaces with no connecting penetrations, except on level 3, to allow movement between bays. In order to permit the operation of restaurants which now take up the entire building, many new penetrations, through both longitudinal walls and the floors, were made. Some of these penetrations were made during the 1970s as part of the general work.¹⁰ In some spaces, atriums have been introduced by the removal of sections of the level 2 and level 3 floors, while in others the intervention in the original fabric and the introduction of new fabric is so great that the original function and configuration is almost completely obscured. The first and second levels of all bays have five posts which support the floor above. The posts divide each of the levels into six sub-bays which, for the purpose of this report, will be called A–F from front to rear. The Level 1 floors appear in all cases to be concrete slabs, most of which were laid in 1976. The slabs were laid to the walls and posts with polymer inserts preventing direct contact.¹¹

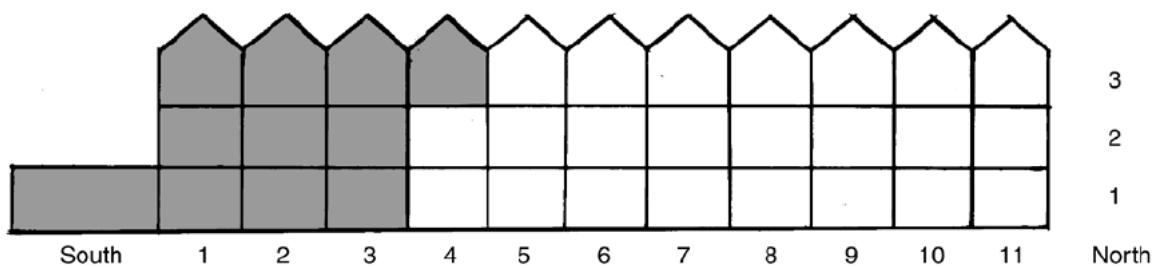
Since the 1970s adaptation works a large amount of new fabric has been introduced. Partitions have been constructed for kitchens, amenities, fire stairs and offices. False ceilings and mezzanines for restaurant eating areas have also been inserted. Sections of flooring were removed in a number of areas (in the early 1970s) to create atrium spaces between levels. Numerous door openings have also been made between the spaces. Figure 3.3 is a graphical representation of the impact of these changes on the spaces within Campbell's Stores.

3.6 The Existing Situation

The following description is based on the site inspection carried out as part of the CMP revision in April 2004 and the description remains current in 2011. Sub-bays, based on column locations, are described as A to F, with sub-bay A being located at the eastern end of each bay.

3.6.1 The Waterfront Restaurant

Bays 1, 2 and 3 (all levels), Bay 4, Level 3 (Part Office); South Extension to Bay 1, Level 1



The Waterfront Restaurant occupies Bays 1, 2 and 3 on all three levels, plus a southern extension of Bay 1, Level 1, which holds the kitchen, as well as Bay 4 on Level 3. All doors in this section of the east elevation are the original ledged and braced doors. They have been sheathed in flat galvanised sheet steel on their inner surface to strengthen them and render them draught proof. Perspex has been added to the sheathing in Bays 2 and 3 to protect the paint on the steel. All posts and beams in this section on Level 1 date from the 1860s and are broad-axed, and all joists and floorboards have been hand sawn. All Level 2 posts, beams, joists and boards date from the 1880s and have been circular sawn.

South Extension

The south extension was constructed in the 1970s to permit kitchen and food preparation areas to be installed outside the envelope of the stores building. It is located beneath the elevated pathway which abuts the retaining wall at the southern end of the stores and gives direct access to the delivery dock beneath the elevated roadway leading to the Overseas Passenger Terminal. This extension is built on part of the site of the original end bay of Campbell's Stores.

Bay 1 Level 1

Access from the kitchen to Bay 1, Level 1, is through two doorways opened in the south wall, c1973. Two doorways, c1973, provide access to Bay 2, Level 1. The doors are fitted with a double lintel composed of two 8 inch by 4 inch (200mm x 100mm) hardwood planks side by side and is typical of all doors opened in the 1970s. The sandstone blocks which form the reveals have been plumbed and the gaps repointed. The concrete slab floor is fitted with ceramic tiles. In general, all of this level in this bay is now used for services, offices and store areas apart from the entry space defined by a curved partition. The partitions have been attached to ceiling joists, beams and posts. A short passage leads between the cellar and office to the rear door (c1973¹²), leading to the western service tunnel. The entrance to Bay 1 is through the original ledged and braced doors. Both flanking window openings are fitted with fixed perspex. The north window has the ledged and braced shutter still in place.

Bay 2 Level 1

This bay has had its floor raised 450mm to the level of Bay 1 and has polished timber floors with carpeted sections. Two doorways, in similar position and construction to those in the southern wall, lead through the northern wall to Bay 3. There is no entry to Bay 2 from the forecourt and the door is fixed in a closed position. The original doors are in place, there is a new shutter on the northern window and fixed perspex to the southern window.

A stairway, with landing, has been inserted in sub-bay E and is bolted to posts 4 and 5.

Bay 3 Level 1

Bay 3, Level 1 is a bar area with the bar running the eastern third of the length of the northern wall. There are no openings in this wall. Access to this space is from the two doors that give entry to Bay 2 and from the front doors. The area steps down to the eastern entry while the rest has been raised to the level of Bays 1 and 2.

The door is original. The flanking openings have been fitted with new shutters and are complete.

A new staff staircase has been constructed behind partitioning in the southwest corner, and male and female toilets have been built in the northeast corner. The partitions are timber framed and are faced externally with sandstone and timber veneers.

Bay 1 Level 2

Bay 1, Level 2 is accessed via one doorway (c1973) in its north wall from Bay 2, Level 2. Another c1973 door in this wall has been blocked since 1996. This floor is 450mm higher than that of Bay 2, Level 2. The western door gives access to an L-shaped kitchen which occupies half the area of the bay. The other half is a dining room which has freestanding tables and chairs which flank the eastern doorway.

The goods door, window shutter and pedestrian door are missing. The window has been infilled with perspex, the pedestrian door with a floor to ceiling, double-hung aluminium window, while the goods door opening is fitted with fixed and sliding-pane aluminium-framed windows.

Bay 2 Level 2

Bay 2, Level 2 is accessed by stairs in sub-bay D and from Bays 1 and 3, Level 2. There are two doorways in the northern wall to Bay 3 (Figure 3.15).

The goods door, pedestrian door and shutter are missing from the openings on the east facade. Their treatment is similar to that of Bay 1.

Bay 3 Level 2

Entry to this bay is via two doorways from Bay 2 (Figure 3.19) and one from Bay 4 (Figure 3.17). It should be noted that on the Devine Erby Mazlin drawings of 1973 the openings in the longitudinal walls are shown to be in sub-bay C and are shown as new fire rated inter-tenancy doors. However, it is not known if the openings existed before 1973 or not. The existing openings in the southern wall of Bay 3, Level 2 are therefore assumed to have been opened c1973.

Bays 1–3 Level 3

These three bays are interconnected by doorways in the longitudinal walls. The original openings are characterised by having a two course relieving arch above the opening. A timber door head or lintel has been inserted about 0.3m into the wall immediately below the spring point of the arch. The space between the arch and door head has been filled with brick. All of these original interconnecting doorways are located in sub-bay B and all have well-formed, precise reveals.

New openings in Bay 1 northern wall (sub-bay E) and Bay 2 northern wall (sub-bay D and sub-bay E) are differently constructed. The openings in the wall between Bays 1 and 2 have the same basic construction as the original with twin brick arches above a false timber lintel. However, in the new openings, the brickwork is much more crudely constructed and the bricks above the lintel have separated from the arch (probably due to the shrinking of the lintel). The pointing is of a different colour and consistency, and there is also interference with the reveals.

Bay 2, Level 3 has a stairway connecting to Level 2 with partitioned areas taking up much of sub-bays D and E. A new pedestrian door leads through the northern wall to this area. The original opening connecting Bay 2 and Bay 3 is shown in Figure 3.18.

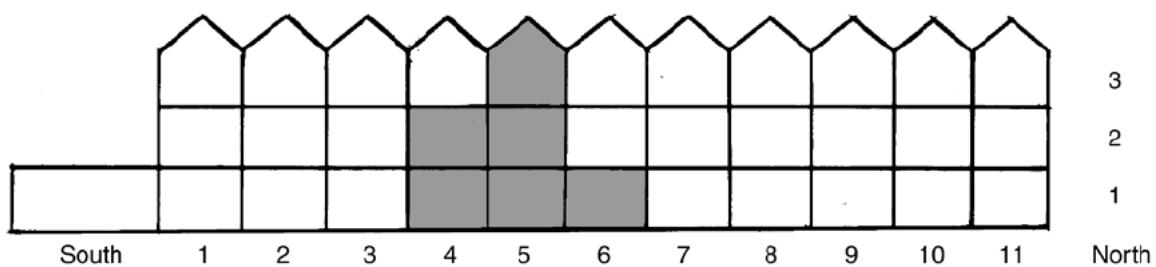
Bay 3, Level 3: the western end of this bay is partitioned and contains a store, public toilets, bar area and staff stairway connecting to Level 2. The framing of the partition is attached to the fabric of the Bay 4, Level 3 building (Figure 3.16).

Bay 4 Level 3

Bay 4, Level 3 is divided by a partition into two sections, the eastern one of which is a private dining room with the western section used as offices. Access is from Bay 3 via one of the original openings to a small lobby which leads to a further four doorways. A single doorway, smaller than other doorways, gives access from Bay 4 to Bay 5, where there are further offices (see below).

3.6.2 Wolfie's Restaurant

Bays 4, 5 and 6 Level 1: Bays 4 and 5 Level 2; Bay 5 Level 3 (Office)



Bays 4, 5 and 6 Level 1

The original doors remain in place in all three bays with a glass door providing the main entrance in Bay 5. In Bays 4 and 5, internal glass partitions fitted with face doors have been added to form an airlock. All window openings have shutter gudgeons but the shutters have been replaced by fixed perspex sheeting.

This area of the stores has been badly affected by rising damp (Figure 3.21). Internally, plasterboard has been erected to window height in Bay 4, while sacrificial render had been added to

1.6m height along the front and north walls in 1996. In Bay 6 a false plasterboard wall has been attached and runs to a point 540mm below the beams. The false plasterboard wall has been added to conceal the rising damp and there is evidence that the plasterboard has been placed over sacrificial render. The plasterboard would probably slow the damage as the draught of air, which causes the surface evaporation, would be greatly reduced.

Bay 4 has had a kitchen established behind ceiling height partitions in sub-bays C, D, E and F.

Bay 5 has a passageway in sub-bay A leading from Bay 4 to Bay 6, with access to male and female toilets behind a full-height partition. The toilets occupy the whole of sub-bay C and have been attached to beams, posts and walls. Bay 6, Level 1 is a dining room with a partition concealing a preparation room and access from the rear service tunnel beyond.

A dog-legged stairway leads from north of the passageway in Bay 5 to the Level 2 dining area.

Bay 5, Level 2 is dining space with a staff stairway partitioned off in sub-bay F. Two small storerooms have been added opposite the stairs. In the west wall, there are two windows and a single door. All of these are new openings, the door being constructed some 800mm to the south of the original single window.

This bay has the original bars on the east facade window and the gudgeons are still in place. The shutter has been replaced with fixed perspex, as has the pedestrian door. The central goods door is missing and has been replaced by sliding aluminium-framed windows on the top section with fixed glazing below.

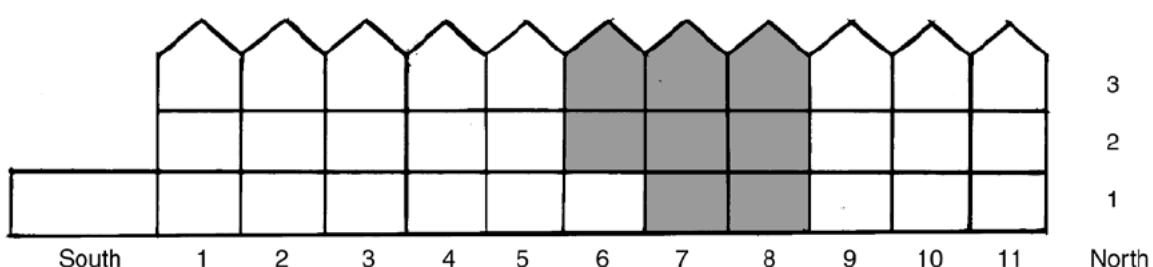
Bay 5 Level 3

Bay 5, Level 3 is given over to offices and an administrative centre. It is provided with a small kitchenette but no other facilities. The floor has been resheathed.

The goods door has been removed and replaced with a fixed lower panel and sliding aluminium upper panel. The apron and chains are still in place.

3.6.3 The Imperial Peking Restaurant

Bays 7 and 8 Level 1; Bays 6, 7 and 8 Levels 2 and 3



Bays 7 and 8 Level 1

The Imperial Peking main entrance is Bay 7, Level 1. There is a staircase attached to posts 3 and 4 that leads to Levels 2 and 3 which are the main dining rooms. In Bay 7, the shutters have been replaced by fixed perspex panels while the original central doors are still in place. A second set of doors inside the original set are part of an airlock which provides weatherproofing to the space. The

west end of the space, half of sub-bay E and the whole of sub-bay F, has been partitioned as an office and store and bar.

Bay 8, Level 1 has a similar entranceway to Bay 7 with an internal airlock in plate glass. There are two doorways opening into Bay 7—one is in sub-bay C and one in sub-bay F. The whole of sub-bays E and F have been partitioned as a food preparation area and store.

A large unglazed window opening has been opened between the bays in sub-bay A. This provides a visual link between the spaces. This new opening has resulted in a loss of original fabric and adversely impacts an appreciation of the original spaces.

All exposed timber in this area and throughout the Imperial Peking has been painted a matte charcoal or matte black to provide uniformity throughout the space and promote the introduced decor.

Salt attack of the walls in this section of the building is severe (Figures 3.23 and 3.24).

Bays 6, 7 and 8 Level 2

Adjacent to the staircase in Bay 7 on both Level 2 and Level 3 is a void which visually links all three levels. This void has been reduced from the size that it was in 1996. The atrium, or void frame, is attached to the No. 2 and No. 3 posts. At the rear of Level 2, Bay 7, there is a bar backed by a storeroom. The partitions for these are attached to the fabric of the building. Doorways in sub-bay C lead to Bay 6 and Bay 8, both of which are dining areas.

In Bay 7 the shutter on the east facade has been replaced with a single casement window and the pedestrian door by a double-hung, aluminium-framed, full-length window. The goods door has been removed and replaced with a fixed lower, and a sliding upper, aluminium-framed window.

The western end of Bay 6, Level 2 is partitioned with a timber-framed hardboard wall with a single access door. A second doorway through the northern wall of this bay gives access to Bay 7. The original shutter on this level has been replaced with a single casement window. The pedestrian door has been replaced with a double-hung, full-height, aluminium framed window. The goods door itself has been removed and replaced with fixed and sliding aluminium framed panes. There is evidence of the former stairway to Level 3 against the northern wall and in the floor of Level 3 above.

Access to Bay 8 is via an opening in the southern wall. The western half of the bay has been given over to male and female public toilets, storerooms and stair. Access to the toilets is from Bay 8 only. Decorative elements in the form of false rafters on both Level 2 and Level 3 have been attached to the walls to provide a distinctive décor (Figure 3.25).

Bays 6, 7 and 8 Level 3

The staircase and atrium are the main elements in Bay 7, which has dining tables in the east half. The original windows are intact, as is the central goods door which is pushed to one side of the opening. The doorway is now covered by a four-pane aluminium-framed sliding window.

There are two entrances from Bay 7 to Bay 6. The easternmost, in sub-bay B, is an original opening with a relieving arch over the timber lintel, while the one in sub-bay D has a lintel only.

Bay 6, Level 3 is a dining room with a stewards station against the western wall behind partitions.

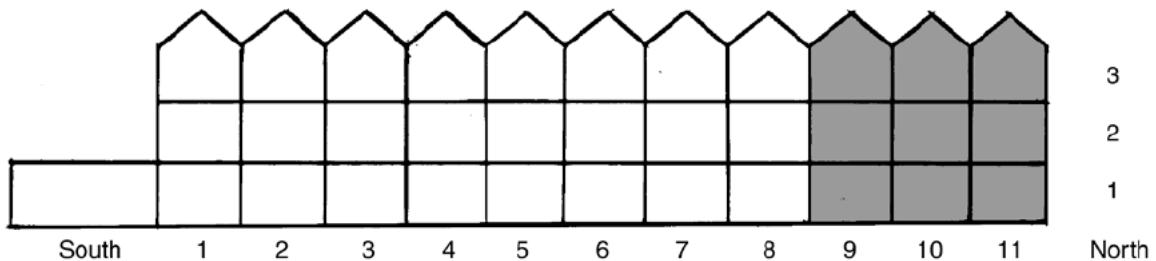
Besides the two openings from Bay 6 to Bay 7 there was an original doorway to Bay 6 that has been bricked in. In the northern wall of Bay 7 there are two entrances to Bay 8. The original eastern one has been bricked in with the original lintels still in place. A new door of the same dimensions with no relieving arch has been opened some 2m to the east. The date of this opening is unknown; however, it would be probable that it was opened at the time of the present tenancy. The second opening, towards the west, leads to a short corridor, the toilets and stores.

The goods door is still present, as are the flanking double-hung windows. The doors have been rolled to the side and fixed perspex attached to the opening.

In Bay 8 the toilets in the western half correspond to those on Level 2 and can be accessed from Bay 7, via the small door, or from Bay 8. There are decorative false rafters, again in the space, painted a grey-green. The central goods door is intact and has been pushed to the side. The space is fitted with fixed perspex panes.

3.6.4 The Italian Village

Bays 9, 10 and 11, all Levels



The design drawings for the conversion of the former Governor's Pleasure restaurant and nightclub into the Italian Village restaurant were prepared in May 1986 by Ercole Palazetti. The work was undertaken shortly afterwards.

The Italian Village tenancy extends over three levels of Bays 9 and 10 of the gable-fronted building and Bay 11 (the most recently constructed building). The introduction of the restaurant caused significant intervention in the fabric of the building and a corresponding loss of significance. There is a very large void, a very large staircase and there are toilets on the three levels in Bay 10.

There has been a deliberate attempt to deny the existence of the authentic fabric of the colonial building (Bays 9 and 10) and to create instead an Italian village atmosphere. The description will concern Bays 9 and 10 of the stores building followed by Bay 11, which will be discussed separately.

Bays 9 and 10 Level 1

Entry is via the original door in Bay 9 and through a specially constructed airlock. The bay is wholly given over to a dining space with false doorways inserted in the fabric of the northern and southern walls. It is not known how much this has interfered with the fabric of the walls. A mezzanine runs partly around the walls of this space, on which certain objects are displayed. A void has been opened through the Level 2 floor to provide means for a staircase which passes through an opening made in the northern wall of this space. A second opening in the northern wall is surmounted by an

arch above the second level. The arch has been constructed from sandstock bricks, which have replaced much of the stone and brickwork in the northern wall of Bay 9 on all three levels.

Bay 10 is partially given over to the staircase to Level 2 and to a bar in sub-bay D. A toilet block occupies the remaining half of sub-bay D and all of sub-bay E. The toilet block is intrusive and is attached to original fabric of the stores. Bay 10, Level 1 has had its floor raised by about 150mm to bring it closer to the level of the ground floor of Bay 11.

Bay 10, Level 2 has a small section at the eastern wall for dining, while the staircase and passageways take up most of the centre of the bay and toilets to the rear or western end. The floor of the area has been raised and tiled in order to bring it to the level of Bay 11.

Bays 9 and 10 Level 2

Bay 9, Level 2 is accessed by doorways in both the eastern and western end of the northern wall. Both openings are contemporaneous with the major works. This space has had its goods door removed and replaced with fixed and sliding aluminium-framed glazing. The window and pedestrian door have both been replaced with fixed panes.

Bays 9 and 10 Level 3

Bay 9, Level 3 has its windows in place and the goods door removed. The door has been replaced with fixed bottom glazing and sliding aluminium-framed panes on the top level. The rear sub-bays have been converted to male and female toilets and storage space. The rest is for circulation and the staircase. There is a single window on the western wall. There is fixed perspex on the pedestrian door on the east facade and fixed perspex on the window. The original goods doors are still in place with fixed glass panes behind.

Bay 10, Level 3 has the toilets against the rear wall with the central section again taken up with the stairwell and store. The front of the bay overlooking the harbour is a dining room. The floor again is tiled and has been raised. The casement windows and bars are still in place while the goods door has been removed and replaced with fixed bottom and sliding top, aluminium-framed glazing.

Bay 11 Levels 1, 2, 3 and 4

The original floor levels of Bay 11, which were altered in the 1986 conversion, can be discerned from the windows and loading aprons on the east elevation (see Figure 3.20).

Today, the first three levels have been converted almost exclusively to kitchens and food preparation areas. Level 1 has an entrance through the southern, eastern and the northern walls for both staff and clients. There is a further door in the western end of the north wall which gives staff access to the pergola dining area from the kitchen. Level 2 has kitchens in the western half and the eastern half is used for dining. Access is via a ramp from Level 2 of Bay 10. A further entry to the kitchen is provided some 7m to the west of the ramp. Level 3 has a similar layout but this level has a continuous floor with Bay 10.

It appears that a fourth level (and altered roof form) was added during the 1986 conversion. A set of stairs was constructed against the western wall, which leads to the staff amenities room on Level 4. Plant and equipment occupy the eastern half of Bay 11 at this attic floor level.

It could not be determined from a very close inspection of the fabric whether Bay 11 was constructed prior to or after the widening of Hickson Road. Half of the west wall of Bay 10 has been

reconstructed at an angle of about 30° to allow for the widening of Hickson Road. The wall of Bay 11 is a continuation of the reconstructed wall of Bay 10 and appears to have been constructed at or about the same time. However, because Bay 11 was burnt out when it was in the ownership of the Government Printing Office and has been radically altered since 1997, the evidence is inconclusive.

It would appear that the reconstructed brick section of the wall at Bay 10 and the wall of Bay 11 were built contemporaneously. The bricks on both bays appear to be almost the same colour and they are laid in an identical bond and the junction is a relatively good fit. It appears that there could have been settling of the north end of Bay 11, which has caused some slight cracking at the junction between the two bays.

At the northwest corner of Bay 11 the bricks meet at an angle of about 120°. Normally, in a building of this type, where bricks of a non-standard shape were required, they were specially made. However, in this case the bricks have been rather crudely cut to the appropriate angle. It could not be determined whether the cutting was done prior to or after the construction of the building, but it appears most likely that an attempt was made to cut the bricks before laying them. Because of the nature of the bricks, they did not cut well and a jagged, almost dog-toothed corner has resulted.

The original bricks that remain on the north facade of Bay 11 are of the same type and are laid in the same bond as those on the west facade.

When the building was constructed, Level 2 and Level 3 of both the east and west facades were symmetrical. An examination of the north facade original drawings, however, indicated that this facade was not symmetrical. When the facade was redrawn to its supposed original length, a symmetrical arrangement could not be found. Even a consideration of the location of the stone support pads for the former floor joists did not indicate a construction date. As none of the original timbers remain, an examination of the roof structure would also be inconclusive.

3.6.5 The Service Tunnel

The service tunnel is self-contained. It consists of a reinforced concrete wall on its western side, a brick wall that is butted up against the building on its eastern side, and a reinforced concrete roof on which the footpath above is supported. It is not known if the brickwork on the eastern side of the tunnel was shielded in any way from the fabric of the west facade of the building. The service tunnel has a series of doors that open into the western end of most bays. These doors were all formerly windows on the ground level of the stores.

3.7 Assessment of Archaeological Potential

The Campbell's Stores site has potential archaeological, scientific and research significance relevant to earlier uses and the development of the site. The archaeological significance may be reduced due to disturbance from later alterations and refurbishment works to the site. The archaeological potential of the site was assessed in The Rocks and Millers Point Archaeological Management Plan prepared by Higginbotham, Kass & Walker in 1991 and in the Campbell's Stores—CMP prepared by GML in 2004. This section of the report was reviewed for the 2011 CMP update.

3.7.1 Below Ground

Potential exists for the survival of evidence of pre-European occupation in the area. An 1887 plan shows the approximate direction of the high water mark for Sydney Harbour running across the area where the northeast bays of the bond stores are currently situated. Reclamation and infilling of the muddy and rocky Sydney Cove shoreline commenced in 1839, continuing for the next decade. Semi-Circular Quay was largely completed by 1854. This work consisted of a semicircular stone sea wall hugging the harbour edge and giving the area its name. Possible evidence of infill materials and earlier harbour foreshore flora may remain intact in subsurface archaeological remains on the Campbell's Stores' site. Such evidence would compare with archaeological material obtained during the 1988 archaeological excavations of the area in front of Cadman's Cottage further south of the site.

In Section 2.0 (Documentary Evidence) it is noted that a cemetery on the site was visible at the rear of the stores as late as 1875. The cemetery was said to have been established in the 1790s prior to Campbell's occupancy of the site. The name of John Jones, a marine who died in 1792, could be seen on a tombstone. Though the exact location of the cemetery area is uncertain it is possible that archaeological evidence of the burial area remains at the rear of Campbell's Stores. The construction of Hickson Road and subsequent excavations for the passageway at the rear of the stores below street level on this road may have disturbed or removed evidence of the cemetery.

Stewart's plan of 1825 (Figure 2.1) shows a set of warehouses standing slightly east of the current Campbell's Stores. It is possible that intact subsurface remains, such as footings relating to these earlier structures, are contained within the area that now incorporates the outside tables and the walkway in the east, towards the harbour.

A small cottage structure is indicated on plans of the site from 1882 until 1901. The area where this structure stood is currently paved and enclosed by a glass pavilion which functions as an eating area directly north of the stores. This building, described as a 'house' on the Dove 1882 plan and a 'cottage' on an 1887 plan, would have been built on fill introduced during the construction of the Circular Quay seawall. It is possible that evidence of the house remains intact below the current surface. Further research is needed to identify this structure, its function and building materials, etc.

3.7.2 Above Ground

Winch, Bay 3 Level 3

In Bay 3, Level 3, there is a winch mounted on a special frame inserted in the roof space between the eastern wall and to the first roof truss (Figures 3.27 and 3.28). The original cat-head portal structure, which is a feature of all of the Level 3 bays, is intact but three additional beams running to the front wall have been added to the portal. These have all been mortised into place. The original cat-head still survives and the winch cable would have led out to the sheave supported at the outer end of the cat-head beam. The bottom chord of the first truss has been strengthened by a section of timber which measures about 225mm by 100mm. This has been attached by a series of nine bolts to the very heavy lower chord of the truss above.

The winch consists of a main wheel which is cast iron with six straight spokes. This wheel has the facility on the northern side for a steel brake band to be applied and, on the southern side, a groove to carry a cable. The diameter of the wheel is about 1200mm. Coupled on the same shaft is a smaller geared wheel which is about 300mm in diameter. This wheel is in constant mesh with a second geared wheel some 600mm in diameter. This latter wheel or driven wheel is located on the

hoist cable drum. The hoist cable drum itself is about 200mm in diameter and is about 800mm long. It is constructed, like the rest of the hoist, from cast iron.

The large wheel bears an embossed inscription bearing the words 'Hudson Brothers Limited Clyde'. This winch is virtually identical to that located in Bay 9. Hudson Brothers were operating at Clyde between 1884 and 1898, after which they were amalgamated with another business; therefore the winches were manufactured within this date range.¹³

The hoist cable runs from the hoist itself through a hole in the wall and, immediately below the cat-head beam, it bears on a piece of timber which measures about 150mm by 150mm and rests on two of the winch beams. Flanking the hoist cable as it passed through the wall are two vertically mounted rollers to assist in the passage of the cable to the cat-head sheave. Because of the length of the cable drum, there had to be some sort of mechanism to align the cable before it reached the sheave, which is now missing. The cable, when the system was operating, then passed through the cat-head sheave.

It is not known how the winch was powered, as there appears to be no place where an electric motor could successfully be mounted and the power may have been transmitted from a motor on the floor below.

Winch, Bay 6 Level 3

A winch has been installed in the roof space on Level 3 in Bay 6 (Figure 3.26), between the first and second roof truss. The winch utilises the original cat-head beam to carry the winch cable and the cable penetrates the east wall through a small hole, featuring a timber bearing block and metal guide rollers on each side.

The driving wheel, which is a C-Section cast-iron wheel about 800mm diameter, is on a shaft about 3m long (Figure 3.26). The shaft is held by three bearings. The main bearing, adjacent to the driving wheel, is at the southern end. The bearing at the north end, immediately inside, was attached to a triple-V clutch wheel. Adjacent to this was a driven-wheel directly coupled to the shaft of the cast-iron cable drum which is about 500mm in diameter. The cable drum shaft passed through bearings at both ends. The bearing at the south end was fixed while that at the northern end supported a cam. Attached to the end of the shaft bearing was a lever. This lever, when pulled down, lifted the large one-metre driving wheel which was fitted with three grooves that corresponded to the three raised sections of the clutch wheel. By pulling the lever the large wheel was brought into mesh with the clutch wheel, which was constantly in motion, and this gave motion to the cable drum.

When the lever was released, a cable and counter weight pulled it back to its original position where it rested upon a timber brake shaft. The item is of extreme simplicity but would have been extremely effective. This winch was powered probably by the Tangye gas engine which was mounted on the floor nearby. The gas engine appeared to be used exclusively to power the winch. It was removed to the lower floor some time during the refurbishment of the stores in about 1977 and was repainted and polished. Subsequently, it was placed in storage by the Sydney Cove Authority; its current location is unknown. The original position of the item was Bay 6, Level 3.

Winch, Bay 9 Level 3

In Bay 9, Level 3, there is a winch mounted between the east wall and the first roof truss (Figure 3.29). The bottom chord of the truss has been strengthened by a section of timber which measures

about 225mm by 100mm. This has been attached by a series of nine bolts to the very heavy lower chord of the truss above. The original cat-head portal structure, which is a feature of all of the level 3 bays, is intact but three further beams running to the front wall have been added to the portal. These have all been mortised into place. The original cat-head, though disintegrating, still survives.

The winch consists of a cast-iron main wheel with six straight spokes. This wheel has a steel brake band on its northern side and a groove to carry a cable on the southern side. The diameter of the wheel is about 1.2m. Coupled on the same shaft is a smaller geared wheel which is about 30mm in diameter. This wheel is in constant mesh with a second geared wheel some 60mm in diameter. This latter driven-wheel is located on the hoist cable drum. The cast-iron cable drum itself is about 0.2m in diameter and is about 80mm long.

The large wheel bears an embossed inscription on one of the iron spokes, bearing the words 'Hudson Brothers Limited Clyde'. This winch is virtually identical to that located in Bay 3.

The hanging handle for operating the winch and the brake mechanism has been partially removed, possibly during the installation of a boxed component of the air-conditioning system mounted on the highest of the three beams running to the front wall. The box is approximately 80mm square and is fed by water piping and electrical cords.

It is not known how the winch was powered, as there appears to be no place where an electric motor was mounted and the power may have been transmitted from a motor on the floor below.

Hand-Operated Crane

The hand-operated crane that was fixed in position on the former dock in front of the building is now held in the Museum of Sydney. It was a fixed jib crane, with wrought-iron rods supporting a cast-iron main jib. Hoisting was by crank-handle through cast-iron gear wheels to a winch drum.

Cat-Heads and Aprons

A number of timber cat-heads remain on the building above the goods opening on Level 3. These are predominantly replacement timbers, which were installed in the 1970s to replace the deteriorated earlier timbers. The metal band holding the sheave-support eye appears to have been reinstalled onto the replacement timbers. One cat-head has a timber sheave block hanging from its eye.

The timber aprons at the upper level loading doors are mostly intact, though the timber components of many are very dilapidated. They are kept permanently in the open (or down) position, leading to water damage to the timber. Most of the loading door openings retain their iron handles on either side. Many retain their top-hung sliding door intact, usually fixed in the open position (as the opening has been infilled with glazing).

3.7.3 Physical Evidence of Key Historical Aspects

1. Historical Aspect: Campbell's Stores are built on the site of the first privately-owned wharf in Australia.

Physical Evidence: Potential archaeological evidence of early wharves and stores may exist in the area to the southeast of Campbell's Stores (just outside this study area).

2. Historical Aspect: Campbell's Stores are located in Sydney Cove, an area that was the hub of commerce and international shipping transport into Sydney until the late nineteenth century.
Physical Evidence: Potential archaeological evidence of early wharves and stores may exist. A hand-operated jib crane believed to have been originally from these wharves, later used on Goat Island, is now located in the Museum of Sydney.
3. Historical Aspect: Campbell's Stores were built for the Campbell family, one of the earliest free immigrant families in Australia. Although it was not built for the founder of the dynasty, Robert Campbell, it was erected for the firm he created, which was then managed by his sons. Campbell's family home was Duntroon, in what is now known as Canberra.
Physical Evidence: Other places associated with Campbell.
4. Historical Aspect: Campbell's land included a grant made by the Governor as a reward for the construction of Campbell's first wharfage.
Physical Evidence: Campbell's Stores are located on this grant of land.
5. Historical Aspect: Campbell's Stores are the surviving element of an evolving complex of wharves and stores established by the Campbell family and maintained in their ownership until 1876.
Physical Evidence: The jib crane noted above is the only other above-ground evidence.
6. Historical Aspect: The construction of the first five bays of the Campbell's Stores in 1851–52 demonstrates the improved and changing commercial fortunes of Sydney and the Campbell family from the late 1840s, after the earlier depression.
Physical Evidence: First and second level sandstone walls for Bays 6–10, timber internal structure and roof trusses (relocated), Level 1 and 2 door and window openings, including metalwork and some timber doors, and truss sandstone supports on Level 2.
7. Historical Aspect: The construction of the additional six bays between 1858 and 1861 demonstrates further economic growth and perhaps the impact of the gold rushes after 1851, resulting in increasing needs in Sydney for commercial storage.
Physical Evidence: As above, for Bays 3, 4 and 5 for 1858 and Bays 1 and 2 for 1861.
8. Historical Aspect: Campbell's Stores are one of only a few groups of warehouses built before 1860 remaining in Sydney, and the only one of its type remaining on the foreshores of Sydney Cove.
Physical Evidence: Other older style warehouses surviving in Sydney include; Moores (relocated at Walsh Bay); the Gallipoli Club; former Hinchcliff Bond, Young Street; and the group of three former warehouses in Bulletin Place.
9. Historical Aspect: Campbell's Stores, together with the ASN Co Stores (1884) and the current Metcalfe Stores, represent three periods of stores development.
Physical Evidence: These three buildings, which are located a short distance from one another, represent the changes in warehouse use and construction that occurred in over half a century.

10. Historical Aspect: The Campbell's Stores were associated with the Australasian Steam Navigation Company, one of the most important commercial shipping and transport companies in Australian history for a period of ten years, from 1877 to 1887.
Physical Evidence: Other associated places such as the ASN Stores.
11. Historical Aspect: The construction of the third level of the Campbell's Stores by the ASN Co in the mid-1880s together with their construction of new stores and offices nearby demonstrates a further period of commercial confidence.
Physical Evidence: The ASN stores were designed by one of Australia's finest architects, William Wardell, who was also the architect for the Grafton Bond Store in Hickson Road.
12. Historical Aspect: The design of the third level, with its interconnecting doors between bays and internal pedestrian connection between the second and third levels, represents a change in the way these types of stores were used. However, it is an adaptation of an existing stores usage and not the same scale of innovation as seen with larger bond and free stores, such as the massive Oswald Bond store in Millers Point.
Physical Evidence: All of original fabric of Level 3 of Bays 1–10, including original openings between bays, evidence of stairs between Levels 2 and 3, door and window openings and timber joinery.
13. Historical Aspect: Technical changes in goods handling, with the early cat-head hoists replaced by electric, gas-engine and hydraulic-powered winches installed in the upper floor after it was added in the late nineteenth century. Evidence of all of these systems remains in the building, which itself is designed around the contemporary techniques of materials handling, with independent floor levels and facade loading doors on each level of each section.
Physical Evidence: Horse power and pulleys, hydraulic hoists, gas engine-driven winch, and electric motor-driven winch.
14. Historical Aspect: Ownership of the Campbell's Stores by the ASN Co demonstrates an expansion of its activities from an original base in Darling Harbour. The financial collapse of the Company in 1887 resulted at least partly from over extending investments in this area. The wharves in Darling Harbour expanded as the principal area of shipping activity in Sydney.
Physical Evidence: Other examples of warehouse expansion in Darling Harbour including other ASN Co Stores.
15. Historical Aspect: The Government's acquisition of Campbell's Stores in 1887 is noteworthy. It is probable that the main interest for the Government was not the buildings themselves but the use of foreshore land for its own maritime activities.
Physical Evidence: The Government built a boat slipway nearby in Campbell's Cove.
16. Historical Aspect: Evidence of the Government acquisition is provided by the establishment of a section of the Government Printing Office adjacent to the northern end of the Campbell's Stores prior to 1895.
Physical Evidence: Government Printer building may have originally occupied a corrugated-iron shed with an addition at its eastern end, see 1895 Bullock Plan (Figure 2.15).
17. Historical Aspect: Most of the surrounding area was resumed by the Government as part of the changes brought about by the bubonic plague scares of 1901. Campbell's Stores were

transferred to the control of the Sydney Harbour Trust who, like all previous owners, leased most or all of it to commercial enterprises. The Harbour Trust appears to have added the mechanical lifting devices to the building early in its ownership of the building, soon after the addition of the third floor level. These lifting devices include two hydraulic hoists, two electric winches and one gas engine-driven winch, and the disposition of lifting devices reflects the arrangement of interconnecting doors on the third level.

Physical Evidence: Hydraulic hoists and winches may have been added soon after the Harbour Trust acquired ownership of the stores, as the disposition of lifting devices indicates an overall plan by the building owner.

18. Historical Aspect: The construction of Hickson Road to the west of Campbell's Stores that necessitated the construction of a retaining wall behind the buildings and removing part of the western wall of Bay 10. Bay 11, which appears to have been built to the Hickson Road alignment, is evidence of the major development activities of the Sydney Harbour Trust.

Physical Evidence: Evidence of the alteration of Bay 10 and possibly Bay 11 remains but the retaining wall was removed in the 1970s refurbishment work to facilitate construction of the service tunnel.

19. Historical Aspect: The commercial store usage of Campbell's Stores continued after the management of the stores was transferred to the Maritime Services Board in the 1930s.

Physical Evidence: No apparent evidence.

20. Historical Aspect: The construction of the Overseas Passenger Terminal and the demolition of the southernmost bay as part of the Overseas Passenger Terminal development marks another shift in the usage of this area.

Physical Evidence: South extension of Bay 1, Level 1 and new brick end wall.

21. Historical Aspect: The establishment of the Sydney Cove Redevelopment Authority (now Sydney Harbour Foreshore Authority) led to the most fundamental change in the usage of Campbell's Stores and the greatest change in fabric since the third level was added c1885. The restoration, which commenced in 1973, was probably one of the first major restoration projects undertaken in Sydney and reflects the philosophical approach to conservation of the time. The restaurant uses reflected the growing recognition and interest in historic buildings and the role of The Rocks as a tourist area. Conservation works included replacement of damaged sandstone. The construction of a service tunnel along the full length of the western side is evidence of its new functional use.

Physical Evidence: The conservation works undertaken c1970s and the continued use of the building as restaurants.

3.8 Technology and Materials Handling

3.8.1 Evidence of Technologies at Campbell's Stores

The existing fabric of the Campbell's Stores contains evidence of a range of materials handling technologies from the early nineteenth century, when the building was first established, through to the additional equipment and machinery added into the building as it became available. The main phases of technology in evidence are:

1. Manual Handling;

2. Hydraulic Hoisting Equipment;
3. Gas Engine-driven Winch; and
4. Motor-driven Winch

Manual Handling

The manual handling phase of the operation of Campbell's Stores covers the majority of the nineteenth century. Each of the bays of the building was fitted with a loading door in the east facade of each of the two levels (and maintained when the third level was added in the late 1880s). Each bay had a cat-head beam projecting from the centre of the gable, over the loading doors and each cat-head beam had a substantial tackle block (pulley) suspended from its outer end (typically from a ring-bolt) (evident in the 1880s photograph, Figure 2.11).

Loading into the lowest level (ground floor) was relatively straightforward, as goods were carried in, sometimes with the aid of trolleys and barrows. For the upper level, the loading doors were opened and the timber apron lowered to the horizontal position. The goods were lifted by manual haulage on the tackle to the upper level and the timber apron allowed the storeman at the upper level to stretch out to the load suspended from the cat-head and to swing it into the loading doors, landing, if necessary, onto the extended apron. Typically, the storeman at the upper level would have a hooked rod about 2m long to assist in the catching of the load and pulling it into the loading doors. Where the weight of load was more than could be lifted by men, the lifting rope was attached to a horse (or horse team) which would walk along the ground, providing the necessary power.

In this phase of the operation of the building, each of the upper and lower levels of each bay was separate from each of the other spaces and loading into and out of each was undertaken through the loading doors on each level (some small items may have been carried up the external staircase to the second level).

Hydraulic Hoisting Equipment

Two hydraulic hoists were fixed to the front facade of the building, at Bays 1 and 8 (see Figures 3.13 and 3.14). The hoists were driven by the hydraulic rams powered by pressurised water, probably supplied by the Sydney Hydraulic Power Company, which operated from the 1890s until 1970.

Looping of the hoist cable over three sheaves on both the fixed and the moving end of the rams provided mechanical advantage for lifting goods up the facade of the building. As the ram expanded, the distance between the moving and fixed sheaves increased, drawing in the free end of the cable, which led up over the upper platform and hung down the facade of the building. The safety cut-off levers and their counter-weighted, pressure limiting devices remain. The adjacent ladders were for servicing the equipment and to gain access to the former timber platforms projecting from the gables above the hoists. Only the projecting beams of the platforms remain.

It is unclear exactly when the hydraulic hoisting equipment was added to Campbell's Stores but it appears to have been done by the Sydney Harbour Trust as part of their renovation of the wharf in the first decade of the twentieth century. It occurred sometime after the third level was added to the building in the late 1880s.

The hydraulic hoisting equipment appears to be standard equipment of the era, relying on the provision of high-pressure water (700psi) to operate hydraulic rams that, in turn, drove lifting cables.

It is also unknown whether the hydraulic equipment at Campbell's Stores utilised the high pressure water reticulated by the Sydney and Suburban Hydraulic Power Company (from a plant established in Darling Harbour in 1891), generated its own or was connected to the independent system installed in the Walsh Bay wharves by the Sydney Harbour Trust from about 1915.

At Campbell's Stores, the hydraulic rams are attached to the facade of the building and have three sheaves attached to both the fixed and moving ends of the ram. The cable loops around the three sheaves then run up to the overhead platform above the uppermost loading door. A pair of pulleys mounted on this platform direct the cable over the centre line of the loading doors. As the ram extends, the space between the sets of triple sheaves increases, requiring additional length of cable, which comes through the raising of the hook end of the cable from ground level to the upper floor level. Automatic cut-offs prevented the ram exceeding its maximum and minimum travel distances.

The operation of the hydraulic rams was usually able to be done from either the ground or from each set of loading doors, although at Campbell's Stores there is only minimal extant evidence of any mechanism at the loading doors and control may have been from ground level only. Apart from the provision of lifting power, the rest of the operations for loading goods into the upper levels were identical to that of manual handling.

Gas Engine-Driven Winch

The gas engine-driven winch represents a mechanical addition to the traditional cat-head hoist, whereby lifting power is supplied by winding the cable onto a drum and the motive power is supplied to the drum via a flat-belt drive from a gas engine. A gas engine was an early form of internal combustion engine, operated utilising coal or other volatile gas. Coal gas had been reticulated throughout inner Sydney from the 1850s.

The operation of the winch is straightforward, with power supplied in the lifting direction only, leaving the braking system to be utilised for lowering loads. The cable was directed out below the cat-head beam and ran over a sheave suspended below the beam from its ring-bolt.

Motor-Driven Winches

The motor-driven winches are also mechanical additions to the traditional cat-head hoist, which differ from the gas engine-driven winch primarily in the source from which turning motion is supplied to the winch drum. Although no direct evidence of the mounting of an electric motor is visible within the roof or on the floor in the vicinity of these winches, the driven wheel is set up to accept a V-belt or rope drive, which suggests (but is not conclusive proof) that an electric motor was the power source. Electric motors of a suitable size and capacity would have been generally available from about the turn of the century and are contemporary with the other hydraulic and gas-combustion forms of power.

3.8.2 Layout of Lifting Equipment

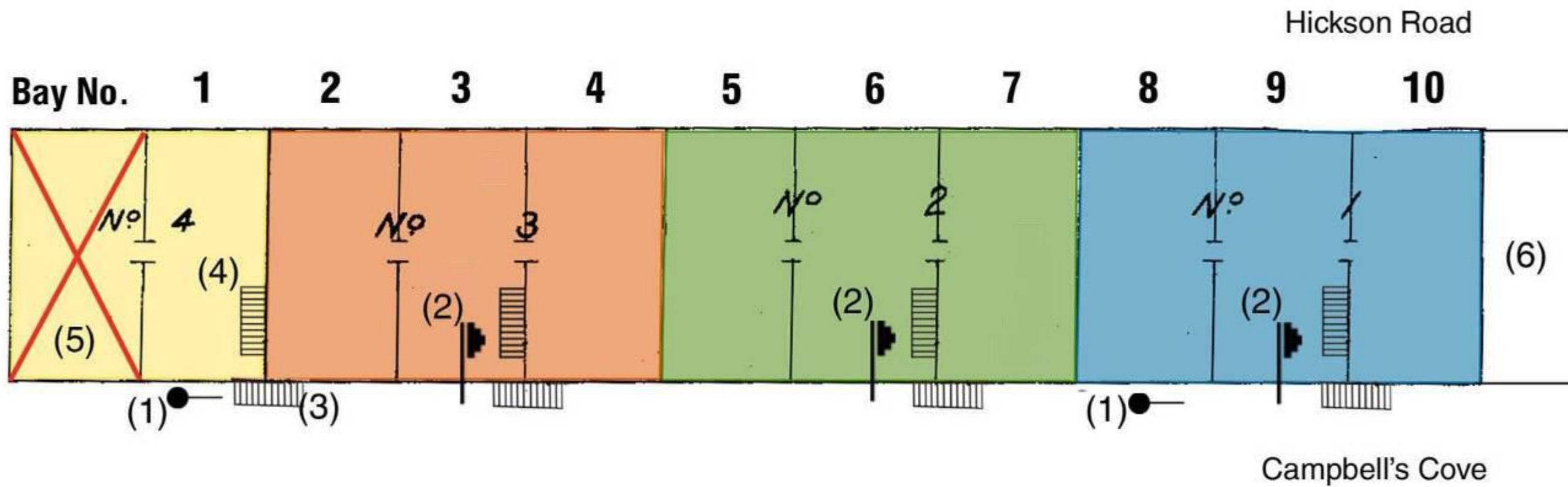
The addition of technological devices for aiding the movement of goods into and out of the Campbell's Stores building occurred in the early years of the twentieth century, although no precise dates have yet been determined. As set out below, whether the various technologies were installed progressively or simultaneously it does not alter the fact that the arrangement and disposition of lifting devices is clearly co-ordinated in terms of the physical layout of the building and its internal

divisions. Lifting devices were added after the third floor had been built and the formerly isolated nature of the bays internal arrangement had been altered.

Hydraulic rams were only fitted to Bay 1 and Bay 8 at Campbell's Stores. The arrangements of internal access within Levels 2 and 3, introduced when the building had the third level added in the 1880s, meant that the upper levels of the original Bay 1 (now removed) and the present bays could both be served from the southernmost hydraulic ram, and the upper levels of Bays 8, 9 and 10 could be served by the northernmost hydraulic ram.

The gas engine-driven winch is located in Bay 6 also gave access into Bays 5 and 7. The winch in Bay 9 served Bays 8, 9, and 10. The motor-driven winches are located in Bays 3 and 9. The Bay 3 winch also serves Bays 2 and 4, while the Bay 9 winch serves Bays 8, 9 and 10. This Bay 9 winch served the same bays as the hydraulic hoist mounted outside Bay 8.

Based upon the available physical evidence, all of Level 3 of the Campbell's Stores building was served by mechanical lifting equipment in the twentieth century. It appears that Bay 11 was not served, though Bay 11 was built separately and contained the Government Printing Office.

**Notes**

- (1) Hydraulic hoists on building exterior
- (2) Engine driven winches in level 3 interior
- (3) External stairs from ground (level 1) to level 2 (originally one for each bay)
- (4) Internal stairs from level 2 to level 3
- (5) Original Bay demolished in 1958
- (6) Government Printer addition c1890 now bay 11

Figure 3.1 Stores operational units (and possibly lease areas) after the construction of Level 3 and based on the Percy Dove plan of 1887 (see Figure 2.13). Each of the units had at least one engine-powered winch or hydraulic hoist, openings between bays and internal stair access from Level 2 to the top floor, Level 3

Sydney Cove Authority Photographic Sources

A collection of SCA photographs documenting the 1970s restoration program was reviewed for the archaeological component of the 2004 CMP. The following photographs are discussed in terms of their ability to provide information on the likely impact of the restoration and renovation program on the archaeological integrity of the Campbell's Stores site. A copy of the photographs follows the descriptions.

CCM 46-49 and CCM 96

Views of internal restoration work in the stores indicate that if sandstone flagging had once existed on the ground floors of the bays this has probably all been replaced by other surfaces such as concrete. It is thus unlikely that any large areas of earlier underfloor accumulations remain intact below these modern surfaces. Photos of the restoration work also indicate that even if the original flooring had been flagging it has been disturbed by recent activities associated with restoration and maintenance of the building. This is especially the case where recent drains and sewerage services have been laid in the basement and ground floor levels in order to facilitate operation of the various restaurants currently occupying the stores. The possibility (albeit slight) of the original sandstone flagging surviving in some areas below current concrete surfaces should be considered.



CCM46 (SHFA)



CCM 47 (SHFA)



CCM 48 (SHFA)



CCM 49 (SHFA)



CCM 96 (SHFA)

CCM 51

Photographic evidence indicates that the sandstone walls were water blasted during the 1970s restoration. This process would have effectively removed evidence of plaster, render or paint traces from the stores' earlier occupation period.



CCM 51 (SHFA)

CCM 151

The northern end of the site appears as a vacant area in the 1970s photographic documentation. The Sydney Harbour Trust resumed four feet from this area sometime after 1915 to build Hickson Road. It is also known that the northern addition to the stores (Bay 11) was subjected to a fire in 1932.

A small cottage structure is indicated on plans of the site from 1882 until 1901. The area where this structure stood is currently paved and enclosed by a glass pavilion which functions as an eating area. This building, described as a 'house' on the Dove 1882 plan and a 'cottage' on an 1887 plan, would have been built on fill introduced during the construction of the Circular Quay seawall. It is possible that evidence of the house remains intact below the current surface. Further research is needed to identify this structure, its function and building materials, etc.

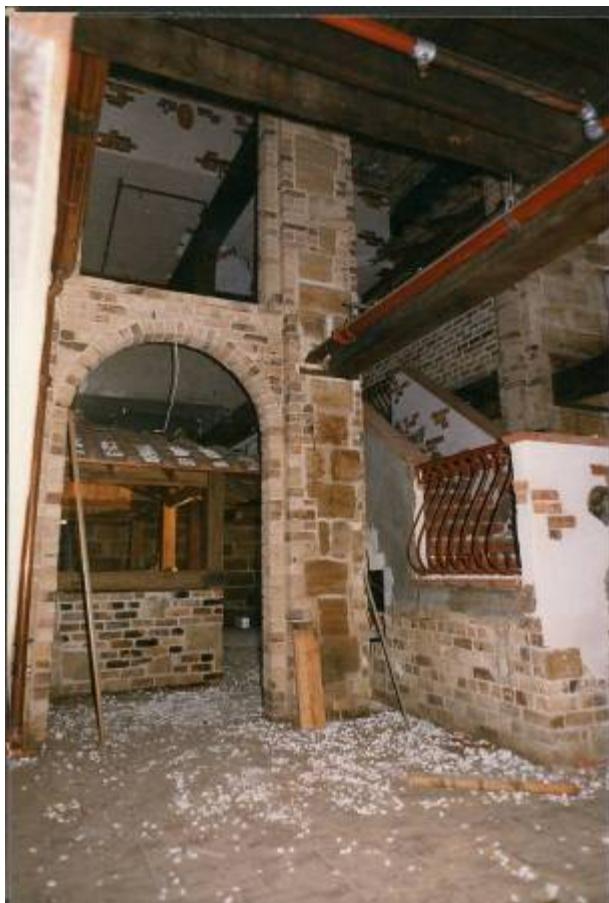


Figure CCM 151 (SHFA)

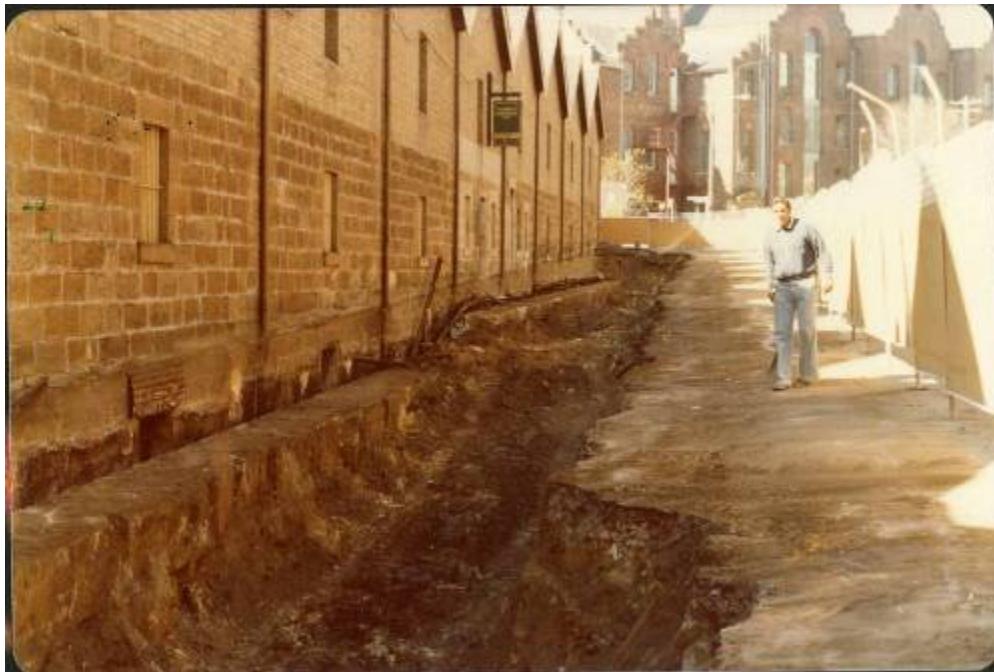


Figure CCM 171 (SHFA)



Figure CCM 172 (SHFA)



Figure CCM 173 (SHFA)

CCM 171–178

It is unlikely that intact archaeological deposits relating to the area's earlier occupation remain along the rear of the stores on Hickson Road. Construction of this road and subsequent excavations for a service tunnel at the rear of the stores below street level would have significantly affected archaeological deposits within this area. Thus, it is unlikely that stratified deposits remain here, having been removed during these excavation works.



CCM 171 (SHFA)



CCM 172 (SHFA)



CCM 173 (SHFA)



CCM 174 (SHFA)



CCM 175 (SHFA)



CCM 176 (SHFA)



CCM 177 (SHFA)



CCM 178 (SHFA)

Warehouse Practice

The two phases of internal arrangement of spaces within Campbell's Stores are evidence of two periods and approaches to dockside goods storage. The early phase had the building comprising separate bays, with no internal access between bays and the upper level of each bay physically separated from the lower level, so that, at its fullest extent of eleven bays (the Government Printing Office addition of 1895 is not relevant in this discussion), the building contained 22 separate spaces, each only able to be accessed from the eastern side. The necessity for this separation is related to the general practices of warehousing and to the particular needs of international port wharfage.

International ports have a unique feature which affects the way in which certain goods are handled, especially imports. This is the excise or duty payable on certain goods, which must be inspected and assessed (in terms of rate and amount), and the goods may not be released from the wharf until these excises are paid. Hence, waterfront warehouses are often divided into 'Bond' Stores, where goods are securely stored until released by the government, and 'Free' stores, where the goods are stored but are able to be removed at will. Campbell's Stores operated for nearly a century as the Metcalfe Bond and Free Stores.

Another feature is that warehouse owners, including the Campbells, may often be importers and exporters in their own right but will build a warehouse in excess of their needs, then lease the additional space out for other warehouse operators. An element of international trade is that, in addition to the staples of trade, there are many small and specialist importers whose volume of trade is not sufficient to warrant their investment in permanent warehouse space but which may require periodic or temporary space; hence, there is an associated trade in the leasing of warehouse space. All these circumstances have a bearing upon the original arrangement of Campbell's Stores.

The later arrangement of the building, after the addition of the third floor, maintains the original divided character of the stores but the internal spaces of the upper floor are organised into four compartments. This arrangement may have allowed a single lessee to utilise all three rooms on this level, without recourse to either of the lower levels.

One speculation regarding the variety of lifting technology installed in the different bays is that various internal sub-leases may have meant that, in certain cases, the lessee of the topmost floor would not be the same as the lessee of the lower floors. In this case, the hydraulic lifting gear, which was operated from outside the building from any floor level, would have been more appropriate than the roof-mounted winches, which needed to be operated from the top floor level.

Figure 3.2 shows the relationship between goods handling and the lease areas after the third level was constructed.

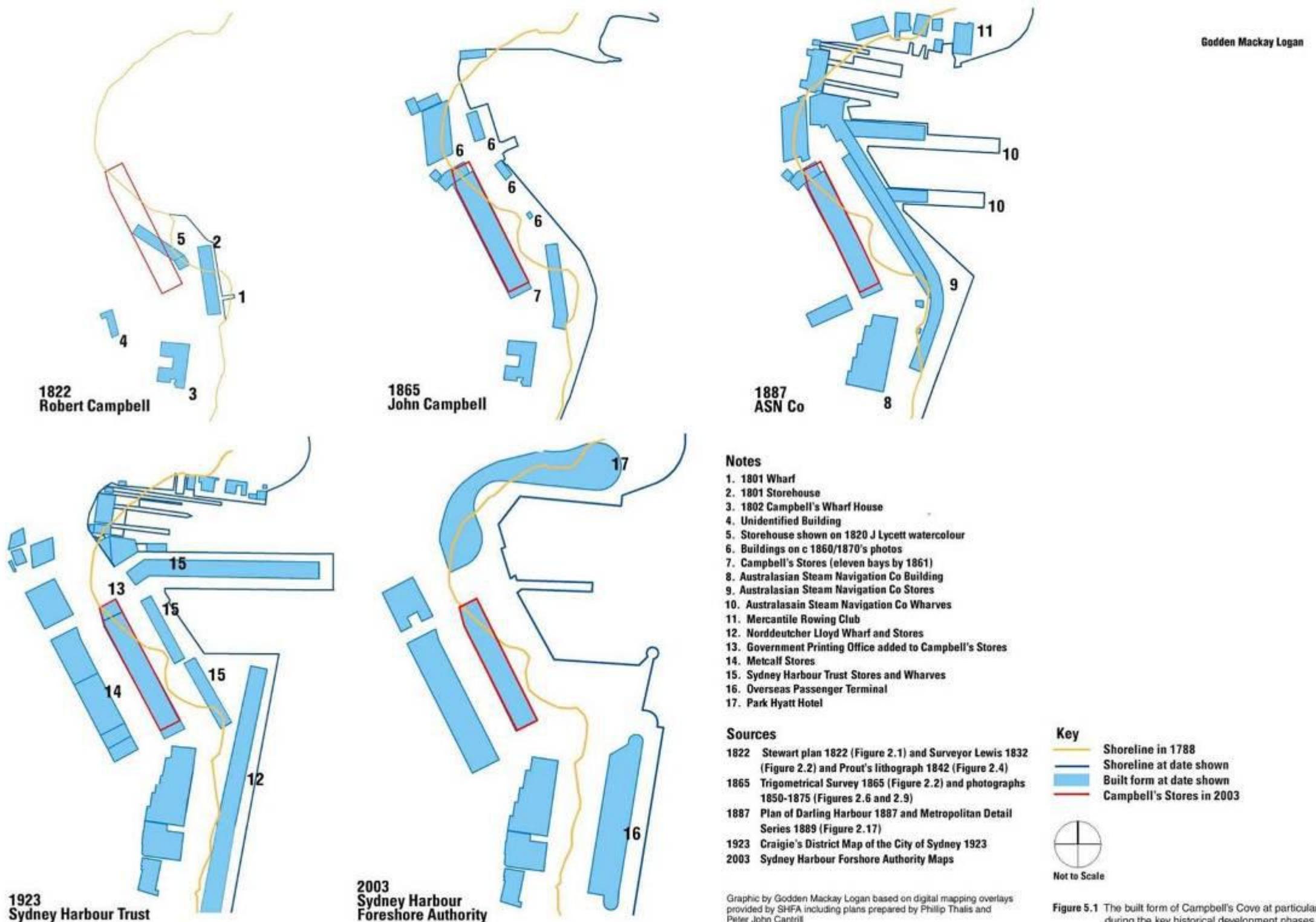


Figure 3.2 The built form of Campbell's Cove at particular dates during the key historical development phases.

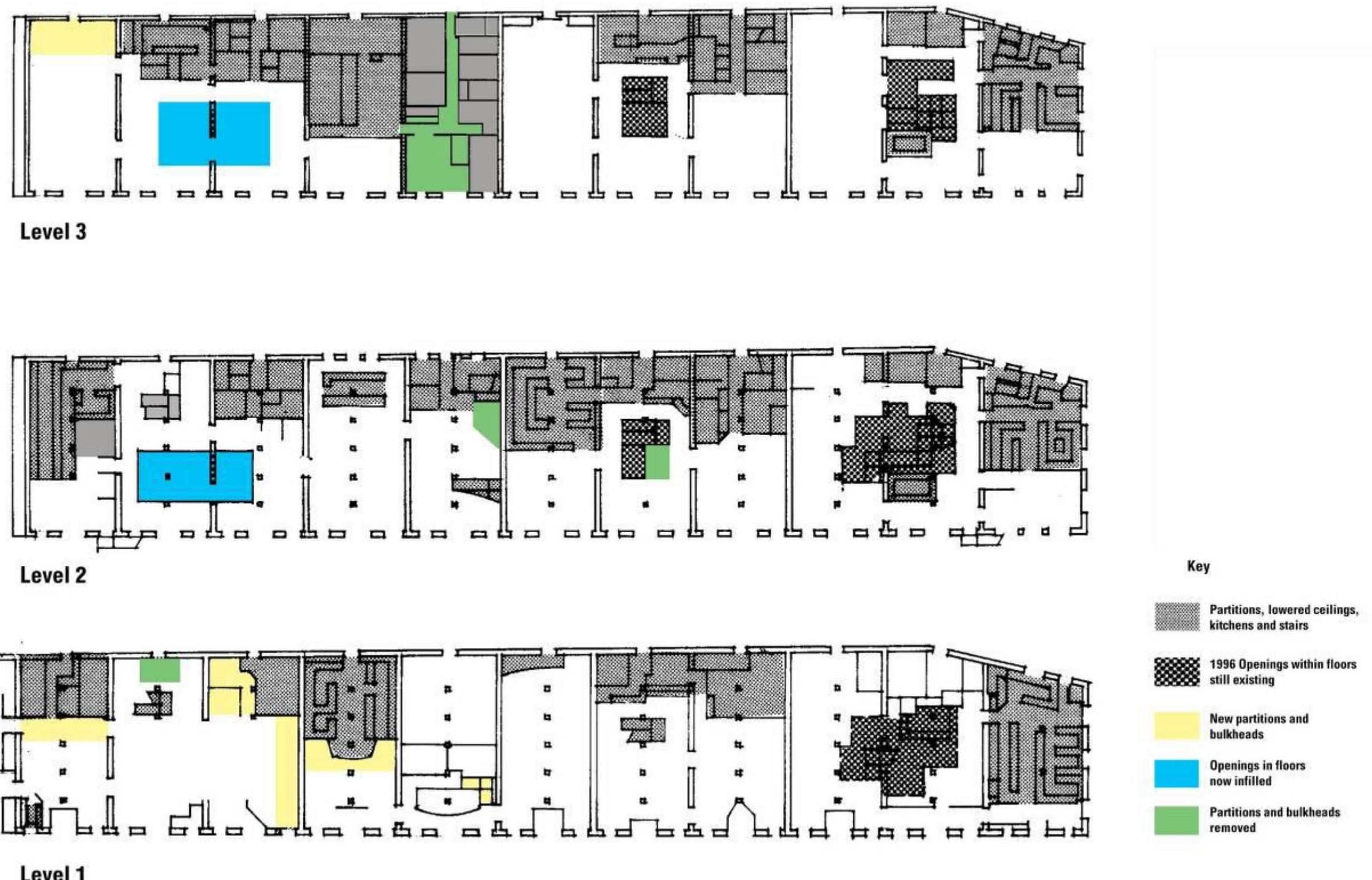


Figure 3.3 Analysis of elements that impact on the original internal spaces, and also showing changes since 1996 (grey tones), April 2003.



Figure 3.4 View of Campbell's Cove from the pedestrian walkway on the Sydney Harbour Bridge (2004).



Figure 3.5 The fabric canopy over the outdoor eating area obscures an appreciation of the facade of Campbell's Stores (2004).

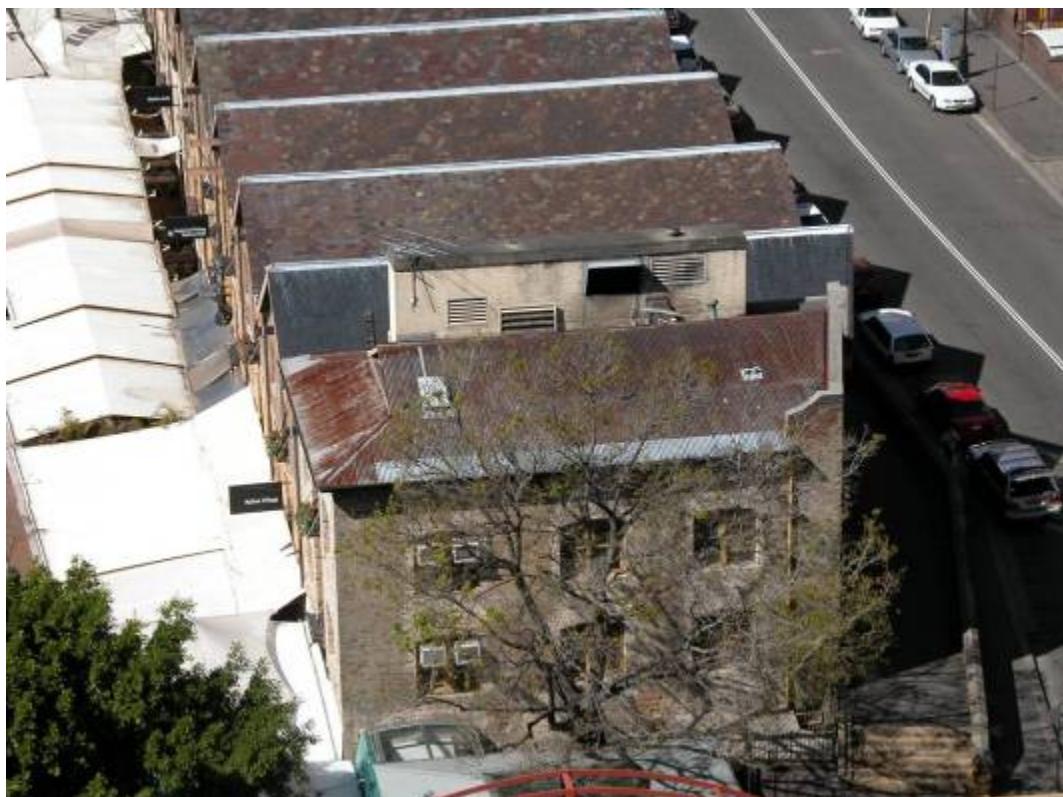


Figure 3.6 The roof of Bay 11 of Campbell's Stores (2004).



Figure 3.7 The setting of Campbell's Stores within Campbell's' Cove (2004).



Figure 3.8 The outdoor seating area with the ASN Co building in the background (2004).



Figure 3.9 The outdoor seating area (2010).



Figure 3.10 The stairs on the southern side of Campbell's Stores (2010).



Figure 3.11 The stairs on the southern side of Campbell's Stores, looking towards Hickson Road (2010).



Figure 3.12 The contemporary pavilion (c1990) at the northern end of Campbell's Stores (2010).



Figure 3.13 The hydraulic hoist, Bay 1 (SHFA c1978).



Figure 3.14 Hydraulic hoist, Bay 8 (SHFA c1978).



Figure 3.15 The Waterfront Restaurant, Bay 2, Level 2, showing the westernmost doorway in the north wall leading to Bay 3 (2004).



Figure 3.16 Waterfront Restaurant, Bay 3, Level 3, looking down through the void at the longitudinal stone wall. The beam and joist on this level have been machine-sawn (1996).



Figure 3.17 Bay 3, Level 2, showing the new opening supported on newly introduced 100mm by 200mm twin lintel boards (1996).



Figure 3.18 Waterfront Restaurant, 1996, Bay 2, Level 3, showing the original doorway connecting Bay 2 with Bay 3. Note the well-formed relieving arch above the lintel and the brick infill between the arch and lintel. The work has been very well executed (1996).



Figure 3.19 Waterfront Restaurant, Bay 3, Level 2. The western doorway leading to Bay 2. The work is meant to be identical with that on the eastern door (Figure 3.10). However, the work is very crudely executed and the pointing is in strongly contrasting mortar (1996).

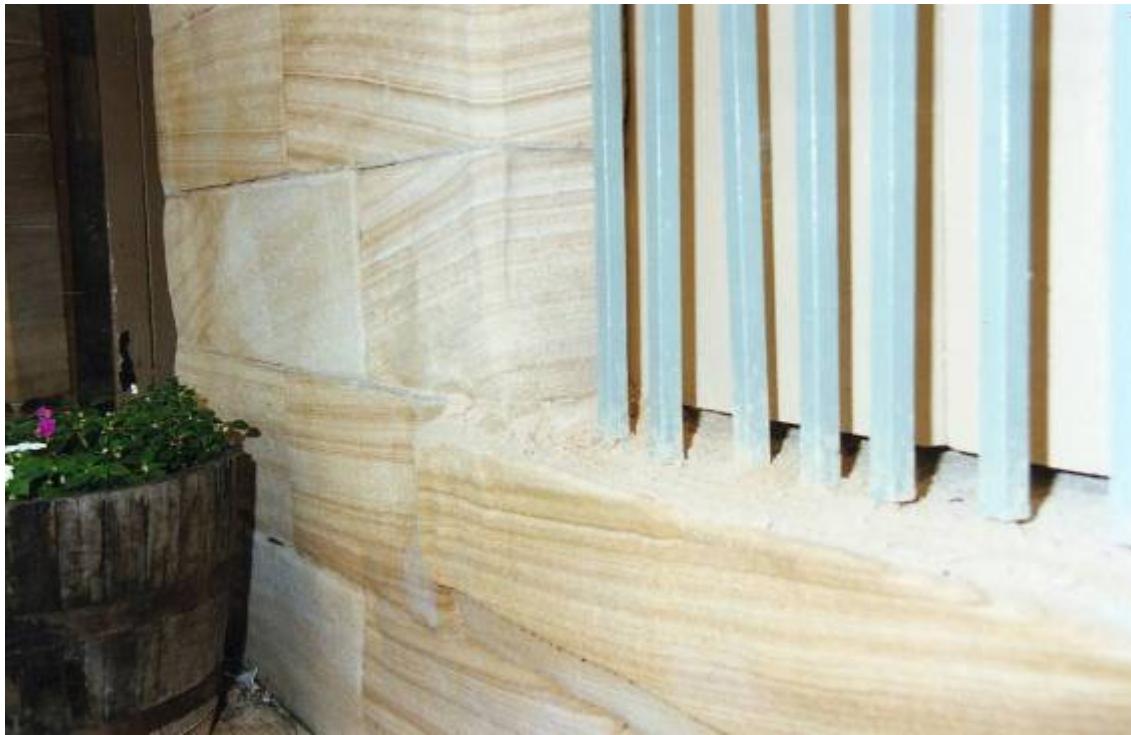


Figure 3.20 Bay 4, window sill and wall. The external wall that was re-skinned in the 1970s is now showing signs of rapid decay (1996).



Figure 3.21 Wolfies Restaurant, Bay 6, Level 1. The decay of the sacrificial render faced on the wall has been slowed to a certain extent by the erection of hardboard panelling that can be seen above the shadow line (1996).



Figure 3.22 Imperial Peking Restaurant, Bay 7, Level 1. The northwestern corner showing the opening between Bay 7 and Bay 8 and the aquarium within the opening (1996).



Figure 3.22 Imperial Peking Restaurant, Bay 6, Level 2, with the painted lintel above the doorway and painted beam joists and bottom of the floorboards (1996).



Figure 3.23 Imperial Peking Restaurant, Bay 7, Level 1. Southeast corner showing massive salt decay of sandstone (1996).



Figure 3.24 Imperial Peking Restaurant, Bay 7, Level 1. The deterioration in the south wall is extremely severe (1996).



Figure 3.25 Imperial Peking Restaurant, Bay 7, Level 3 in 1996. The blocked doorway can be seen immediately below the lintel board with the painted false rafter decorations above (2004).



Figure 3.26 Imperial Peking Restaurant, Bay 6, Level 3. The hoist mechanism located on the lower chord of the roof trusses. This winch was operated by engaging the drive wheel of this continually operating machine, and with the use of the brake (2004).

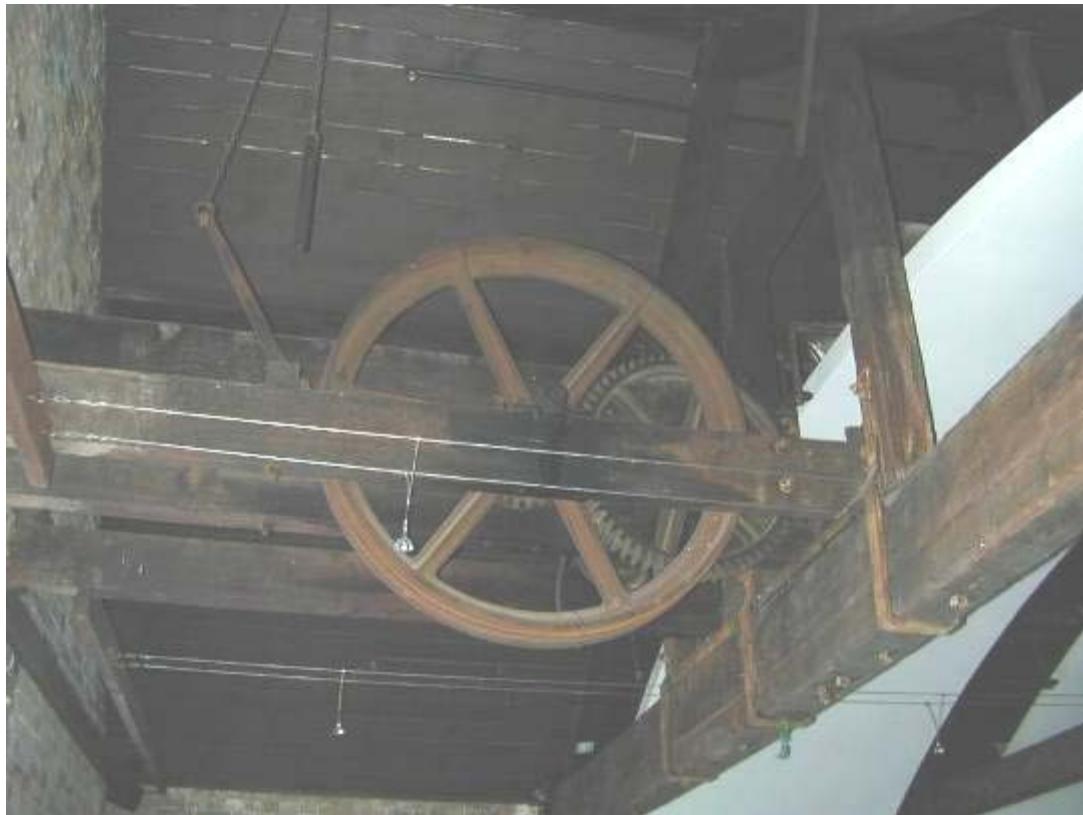


Figure 3.27 Waterfront Restaurant, Level 3, Bay 3. The winch mechanism located on the lower chord of the roof trusses. The brake mechanism is the curved element under the large wheel, connected to a chain with a hanging handle for operating the winch (2004).

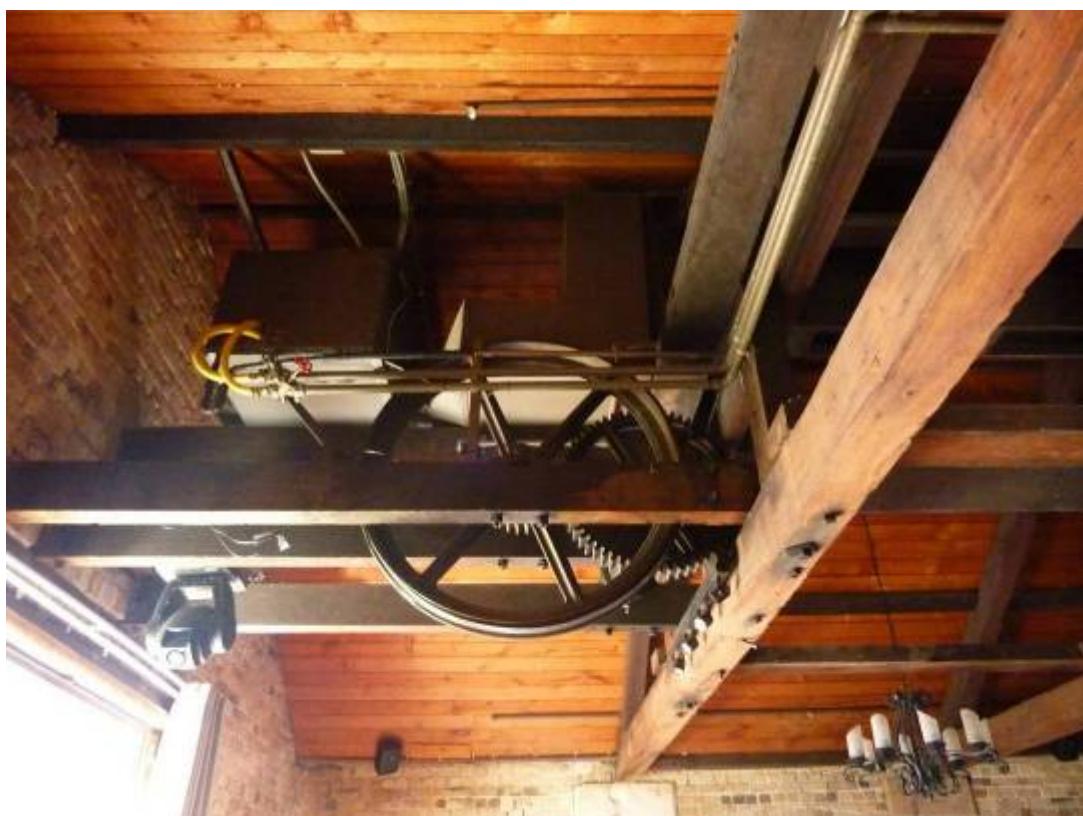


Figure 3.28 Winch in the Waterfront Restaurant, Level 3, Bay 3 (2010).



Figure 3.29 Winch gear in Level 3, Bay 9 (2004).



Figure 3.30 The timber roof beam of Level 3, Bay 3, sub-bay 3 is bowing towards the east, indicating stresses in the structure (2004).



Figure 3.31 The east facade of Bay 11. The goods aprons in the central goods bay indicate the former floor levels. The floors have been lowered, as have the windows and a fourth floor has been introduced beneath the roof (2004).

3.9 Endnotes

- ¹ From photograph CCM172 of August 1978.
- ² Photo CCM51, undated.
- ³ DEM drawing, Bays 1–5 WD8 of August 1973.
- ⁴ CCM194 nd.
- ⁵ CCM113, 8 July 1977.
- ⁶ CCM89 of March 1976.
- ⁷ CCM184 September 1975.
- ⁸ CCM174 August, 1978.
- ⁹ CCM51 nd.
- ¹⁰ CCM46 and CCM47UD.
- ¹¹ DEM WD9, August 1973.
- ¹² DEM working drawings WD3, August 1973.
- ¹³ Murray, J 1992, *Phoenix to the World: The Story of Clyde Industries and Sir Raymond Purves, CBE*, Playwright Publishers, Sydney, p 24.

4.0 Assessment of Cultural Significance

4.1 Assessment of Significance

Assessment of cultural significance endeavours to establish why a place or item is considered important and is valued by the community. Cultural significance is embodied in the fabric of the place (including its setting and relationship to other items), the records associated with the place and the response that the place evokes in the community.

The Burra Charter recommends that significance be assessed in categories such as aesthetic, historic, scientific and social significance.

The *NSW Heritage Manual*, published by the NSW Heritage Office outlines a total of seven criteria for significance assessment. Five of these cover the same four categories of significance found in the Burra Charter, with two criteria for assessing the comparative significance of an item.

Since the preparation of the *Heritage Manual*, the *NSW Heritage Act*, 1977, was amended in 1999. As part of this amendment, the NSW Heritage Office has adopted revised criteria for assessment of heritage significance. The evaluation of cultural significance in the following sections is based on these revised criteria. These are broadly consistent with the criteria contained in the Burra Charter and the *NSW Heritage Manual*.

4.1.1 Comparative Analysis

The *Survey of Warehouses and Woolstores Within the City of Sydney* by Trevor Howells and Mark O'Donnell for the City of Sydney in 1993/1995 identified and classified warehouses in central Sydney into six phases (Table 4.0). These phases were identified in an earlier analysis contained in Emery Balint and Trevor Howell's *Study of Historic Building Construction in NSW 1850–1918*.

Campbell's Stores can be placed within the first of these six phases dating between 1800 and 1940; representing the traditional warehouse form of load-bearing stone or brick masonry (and internal walls), with a timber internal structure of columns and beams. Of the traditional phase-one warehouses in the study only six warehouses/woolstores were constructed before Campbell's Stores (Campbell's Stores was not actually identified in the study); four of which are located in The Rocks or Millers Point: Munn Street Bond Store (Dalgety's); Gas Lane (AGL) Stores; Westpac Bank (Union Store) and the Argyle Centre. Campbell's Stores are therefore significant within a small group of early traditional warehouse buildings within central Sydney.

Later phases of warehouse construction from 1885 employed cast-iron, and then steel and concrete internal structures. The development of the warehouse form resulted from the evolution of these construction materials and techniques as well as from the associated evolution of goods handling and services, such as lifts and fire protection that resulted in bigger and taller warehouses. Nearby Campbell's Stores are the ASN Company building and the Metcalf Bond building which are illustrative of the progress made in the areas of construction and services subsequent to the construction of Campbell's Stores.

None of the early warehouse buildings are used for their original purpose and, as such, Campbell's Stores are an example within the set of similar warehouse buildings that have been adapted for new uses; either as restaurants or commercial offices. Campbell's Stores are one of the earliest examples of major adaptive re-use in Sydney.

The comparative rarity of Campbell's Stores within the group of early traditional construction warehouses is reinforced by its location adjacent to, and visible within, the waterfront setting of Sydney Cove. Rarity is also associated with the way in which the third level shows a transition of use (more open plan) and goods handling technology (hydraulic hoists) that can be found in larger and taller buildings of the mid-1880s but are found here in an 'adapted' early warehouse form.

A comparative analysis is provided below of woolstores which share key features and characteristics to Campbell's Stores.

Metcalfe Bond Stores

Metcalfe Bond Stores are located at 68–84 George Street, The Rocks, and were constructed between 1912 and 1916, some time after Campbell's Stores.¹ The buildings form a backdrop to Campbell's Stores and are quite prominent on the northern end of George Street. They are larger in scale and while they are similarly constructed of masonry, they differ in appearance and style.

The stores comprise of two adjacent buildings with a post and beam structure and face red brick exterior. The first building constructed in 1912, comprises of seven bays, is three levels high facing George Street, and five levels high facing Hickson Road. It contains a stucco parapet and semi-circular openings on the ground floor. The second building was constructed in 1916 and located to the south. It is similarly three levels facing George Street and five levels facing Hickson Road; however it consists of nine bays with a stepped parapet that divides the facade into three sections. The brickwork is more detailed and all openings are rectangular with two prominent string courses.²

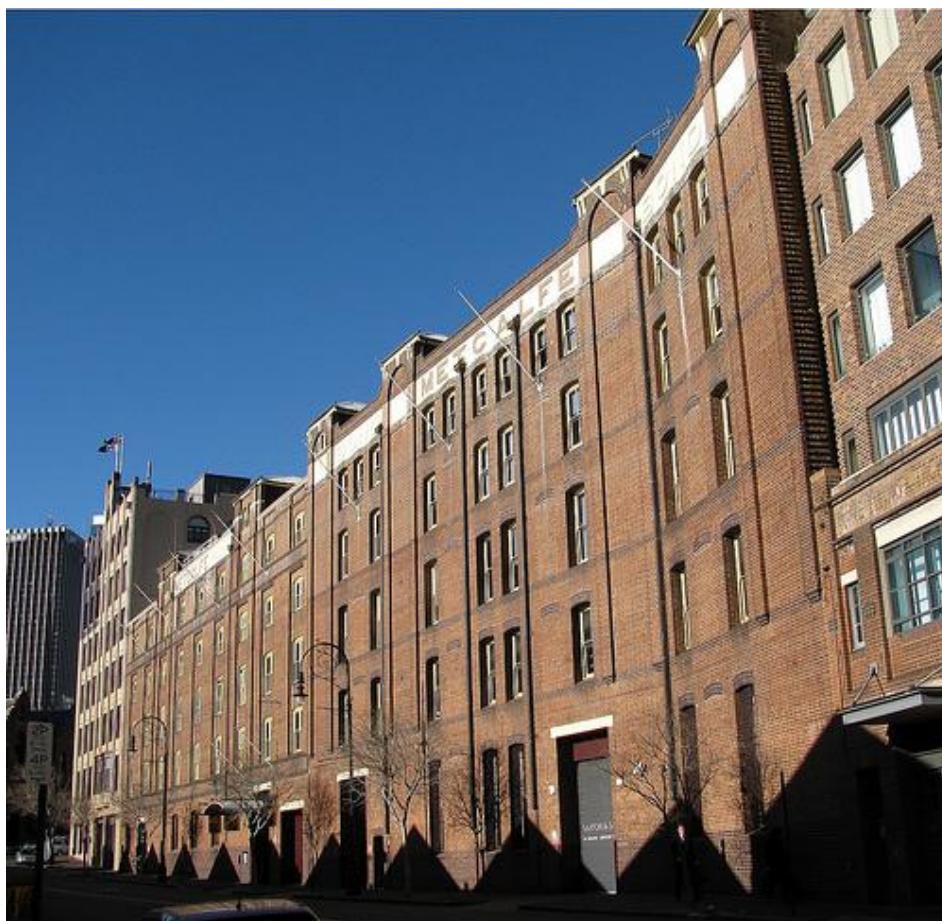


Figure 4.1 Metcalfe Bond Stores (Google Images)

Raphael A Mackeller Stores

The Raphael Mackeller Stores, located at 4–6 Kendall Lane, The Rocks, comprise of two narrow stone buildings that are three levels high, with a gable roof. They were constructed between 1853 and 1854, at around the same period as the construction of bays 6–10 (then 1–5) of Campbell's Stores. The Raphael Mackeller Stores are similar in appearance and materiality; however they are considerably smaller in scale. They were built in two stages, the first in 1853 and the second in 1854 evidenced by the continuous vertical joint along the west and east facades. The doors of the six loading bays are framed and braced with hardwood lintels.³



Figure 4.2 Raphael A Mackeller Stores (Google Images)

Table 4.0 Six Phases of warehouses in central Sydney (Sourced from the *Survey of Warehouses and Woolstores Within the City of Sydney* by Trevor Howells and Mark O'Donnell for the City of Sydney)

Phase	Description	Example
Phase 1 1800–1940	Load bearing stone or brick masonry external or internal dividing walls. Simple timber roof trusses. Timber columns and beams internally. Timber floor boards on timber joists.	Campbell's Stores, The Rocks
Phase 2 1885–1895	Load bearing stone or brick masonry external or internal dividing walls. Simple timber roof trusses. Cast-iron columns and timber beams internally. Timber floor boards on timber joists.	Former PMG store, Harbour Street, Haymarket

Phase 3 1890–1900	Load bearing stone or brick masonry external or internal dividing walls. Simple timber roof trusses. Cast- or wrought-iron columns and wrought-iron beams internally. Timber floor boards on timber joists.	F.L Barker Woolstore, Pyrmont Bridge Road, Pyrmont.
Phase 4 1887–1910	Load bearing stone or brick masonry external or internal dividing walls. Terra cotta/coke breeze fill flat roof. Cast- or wrought-iron columns and wrought-iron or steel beams. Terra cotta/coke breeze fill floors.	Former Corn Exchange, 173–185 Sussex Street, Darling Harbour.
Phase 5 1887–1910	Load bearing stone or brick masonry external walls. Reinforced concrete flat roofs. Cast- or wrought-iron columns and wrought-iron or steel beams internally. Reinforced concrete floors.	Culwalla Chambers, 67 Castlereagh Street, Sydney.
Phase 6 1915–Present	Non load bearing external masonry walls. Reinforced concrete slab roof. Full steel structural frame, concrete encased. Reinforced concrete slab floor.	Commonwealth Bank, 120 Pitt Street, Sydney.

4.1.2 Assessment Criteria

The following assessment of heritage significance has been prepared utilising the current evaluation criteria established by the New South Wales Heritage Council.

Criterion (a)—An item is important in the course, or pattern, of NSW's cultural or natural history (or the cultural or natural history of the local area)

Campbell's Stores is a rare example of mid-nineteenth-century warehousing in Sydney and the only of its type remaining on the foreshores of Sydney Cove.

Campbell's Stores provides evidence of the changing nature of activities around Sydney Cove, the importance of this area as the hub of commerce and international shipping transport until the late nineteenth century, and its recent role as a cultural focus of international importance.

The construction of the first five bays of Campbell's Stores in 1851–52 demonstrates the improved and changing commercial fortunes of Sydney and the Campbell family from the late 1840s, after the earlier depression. The construction of the additional six bays between 1858 and 1861 demonstrates further economic growth and the impact of the 1851 gold rushes that resulted in an increased colonial population and the need in Sydney and NSW for commercial storage.

The construction of the third level of Campbell's Stores by the Australasian Steam Navigation Company in the mid-1880s demonstrates a further period of commercial confidence in Sydney during a highly competitive period when the expansion and absorption of companies was reshaping the corporate structures of New South Wales shipping. The withdrawal of the ASN Co from Campbell's Stores in the late 1880s reflects the increasing dominance of Darling Harbour as the principal area of commercial shipping activity in Sydney.

The hydraulic hoists and winches fixed to the building demonstrate some of the technological changes in late-nineteenth-century handling of goods.

The external fabric of the Campbell's Stores demonstrates four phases of technological change in the handling of goods in and around the building. The loading doors on the two lower levels and the cat-head beams of the manual handling phase were in use during most of the nineteenth century. The presence of loading doors on both levels demonstrates that each space, on the upper and lower levels of the bays, were separate from one another.

The installation of hydraulic hoisting equipment following the addition of the third level to Campbell's Stores illustrates the goods handling and haulage technology introduced in the late nineteenth century. The hydraulic rams, the gas engine-driven winch and the two motor-driven winches were integral to the efficiency of third floor and demonstrate the development in hoisting equipment from the traditional cat-head hoist at the turn of the century.

The internal layout of the Campbell's Stores clearly demonstrates the importance of the lifting devices to the efficiency of operations on all levels. By the twentieth century, all of level three of the Campbell's Store building was served by mechanical lifting equipment, and previously separate bays were linked by openings in the walls on the upper level.

The acquisition of Campbell's Stores by the Government in 1887 is evidence of a broader government interest in controlling infrastructure and utilities, increasing interest in this area as a base for its own maritime activities. Evidence of the Government acquisition is provided by the establishment of the Branch Stores Office of the Government Printing Office, adjacent to the northern end of Campbell's Stores (now Bay 11). The construction of Hickson Road and its impact on Campbell's Stores is evidence of work of the Sydney Harbour Trust and the changes that occurred in this area as a result of the bubonic plague scares of 1900-1905. Evidence of the construction of Hickson Road is found in the alterations to the west facade of Bay 10 and the current alignment of Bay 11.

Campbell's Stores is significant for its association with commercial Bond and Free Store usage for over one hundred and twenty years from 1851 to c1970, with each successive owner, including the Sydney Harbour Trust and Maritime Services Board, leasing sections of the Stores to a variety of merchant companies.

The demolition of the southern-most bay of Campbell's Stores to facilitate construction of the elevated roadway for the Overseas Passenger Terminal development is evidence of a shift away from the traditional usage of this area.

The fabric and use changes instigated by the Sydney Cove Redevelopment Authority in the early 1970s are further evidence of the increasing importance of The Rocks as a tourist destination and cultural area and an interest in historic buildings generally. This was one of the first major adaptive reuse and restoration projects undertaken in Sydney and it reflects the philosophical approach to conservation at the time.

The changes that have occurred since the 1970s in the fit-out of the leased areas for restaurant use reflect different aspirations and approaches to the recycling of historic buildings. The interior of the Waterfront Restaurant reflects an 'historic' maritime character, the 'Imperial Peking'; and the 'Italian Village' are based on a transformation of character using Asian and European cultural imagery, while respectively, 'Wolfies' involves a contemporary fit-out which retains more of the original spatial character and finishes than the other examples.

Campbell's Stores has State significance value under this criterion.

Criterion (b)—An item has strong or special association with the life or works of a person, or group of persons, of importance in NSW's cultural or natural history (or the cultural or natural history of the local area)

Campbell's Stores is associated with the Campbell family. Campbell's Stores is a surviving element of an evolving complex of wharves and stores that began with the construction by Robert Campbell (Sen), the founder of the dynasty, of the first privately-owned wharf in Australia, in 1801. Although Campbell's Stores was not built for Robert Campbell (Sen), it was erected for the firm he created which was then managed by his sons. The first five bays of Campbell's Stores demonstrate the consolidation of this pioneer commercial dynasty rather than the pioneering phase of that family's growth.

Campbell's Stores was associated with one of the most important commercial shipping and transport companies in Australian history, the Australasian Steam Navigation Company. The construction of the third level of Campbell's Stores by the ASN Co in the mid-1880s demonstrates the company's commercial confidence in Sydney. The withdrawal of the ASN Co from Campbell's Stores in the late 1880s reflects the financial over-extension of the Company.

The technology in use in the operation of the hydraulic hoisting equipment and winch is associated with the Clyde Industries Group, one of the earliest and largest manufacturing organisations in Australian history. The large wheels of the winches mounted on Level 3 in Bays 3 and 9, each bear an embossed inscription bearing the words 'Hudson Brothers Limited Clyde'. Hudson Brothers dominated the Australian manufacturing industry in the late nineteenth century and was later amalgamated into Clyde Engineering Co Ltd, responsible for rolling stock, steam locomotives and, most famously, the structural steel for the northern approaches to the Sydney Harbour Bridge.

Campbell's Stores has Local significance value under this criterion.

Criterion (c)—An item is important in demonstrating aesthetic characteristics and/or a high degree of creative or technical achievement in NSW (or the local area)

Campbell's Stores is a superb example of mid-nineteenth century warehouse buildings of a type now rare in Sydney. It has iconic value as a representation of early Sydney, particularly in the area around Sydney Cove and The Rocks. It has landmark value as a dominant and easily recognisable form that is visible from a wide area of Sydney Harbour, the Harbour Bridge and North Sydney.

The design, form and materials of Campbell's Stores contribute to a complex of buildings of high visual and sensory appeal. Their design reflects and describes the buildings' original function in a simple but dignified manner. Their form is a coherent whole made up of repetitive gabled bays combined with an undulating rhythm of door and window openings. The consistent use of sandstone, brick and slate materials reinforces this visual coherence and provides an appearance of solidity and quality.

Campbell's Store has aesthetic significance at a technical level because its form and design details allow for an understanding of its original use. It also represents a surviving example of an older style set of warehouses; a building type once common around Sydney, but now rare. The gabled bay form, cat-head beams, hoists, goods aprons and doors are typical of the older, mid-nineteenth century warehouse buildings. The gabled bay form, external staircases, bars on openings and lack of internal connections between bays (on Levels 1 and 2) evidence its bond store use. This required secure and segregated spaces, not only between bays but also between levels.

The cat-head beams, goods aprons, pulleys, loading doors, hydraulic hoisting equipment and winches are demonstrative of the change from manual handling of goods to the use of hydraulic and other mechanical technology at Campbell's Stores during the nineteenth and twentieth centuries. The use of hydraulic hoisting equipment and mechanical winches became an integral part of the operations of the Stores, following the addition of a third level in the late nineteenth century. The hydraulic hoisting equipment and winches in particular are a prominent aesthetic element of the Campbell's Stores and are evocative of the industrial origins of this dockside site in Sydney Harbour.

The functional design of the third level of the Campbell Stores is significant as it represents a transition in usage between the older style traditional warehouse form of self-contained unit bays and the later, larger, warehouses with interconnected spaces. The adaptation of the earlier warehouse illustrates the application of new technology to an older building form.

The original internal spatial volumes, timber floor and roof structure and other fabric associated with the original usage of Campbell's Stores is also significant as evidence of their warehouse use and of changes made to them over time for that use.

The technology evident in this building such as the hydraulic hoisting equipment is evidence of technical innovation.

Campbell's Stores has State significance value under this criterion.

Criterion (d)—An item has strong or special association with a particular community or cultural group in NSW (or the local area) for social, cultural or spiritual reasons

Campbell's Stores is recognised by some of the contemporary community, due to its location in one of the key recreational and tourist areas in Sydney and because of its popular restaurant use and resultant public exposure. While the provision of restaurants in The Rocks, in buildings with historic character and in strategic locations with expansive harbour views and ambience, is appreciated by the public; this does not necessarily translate into a strong or special association for the building itself.

Campbell's Stores is not considered to meet the threshold for listing under this criterion.

Criterion (e)—An item has potential to yield information that will contribute to an understanding of NSW's cultural or natural history (or the cultural or natural history of the local area)

Campbell's Stores has the potential to contribute further to our understanding of the early maritime operation that occurred around Sydney Cove, and in particular within the Campbell's complex (see Section 4.0). It also has the potential to contribute further to our understanding of the use and operations of mid-nineteenth century warehouse buildings, particularly in the area of goods handling and the changes in technology that occurred over time.

The Campbell's Stores site has potential archaeological, scientific and research significance relevant to earlier uses and the development of the site. The archaeological significance may have been reduced due to disturbance from later alterations and refurbishment works to the site.

Campbell's Stores has State significance value under this criterion.

Criterion (f)—An item possesses uncommon, rare or endangered aspects of NSW's cultural or natural history (or the cultural or natural history of the local area)

Campbell's Stores is a rare example of mid-nineteenth-century warehousing in Sydney and the only building of its type remaining on the foreshore of Sydney Cove.

The collection of late-nineteenth-century goods-handling equipment is a rare assembly of different types of such equipment in a single location, providing a unique opportunity for comparison and interpretation.

Campbell's Stores has State significance value under this criterion.

Criterion (g)—An item is important in demonstrating the principal characteristics of a class of NSW's cultural or natural places or cultural or natural environments (or a class of the local area's cultural or natural places; or cultural or natural environments)

Campbell's Stores demonstrates the evolution and importance of Sydney Cove for maritime trading activities. The building is representative of a class of similar masonry warehouses that were once common on the shores of Sydney Harbour. The changes demonstrate the evolution of increasingly large warehouses in the second half of the nineteenth century, and the activities of the former Sydney Harbour Trust. The materials and construction are representative of mid-nineteenth century warehouses. It provides evidence for the lifting and storage of goods in nineteenth century maritime trade.

Campbell's Stores has Local significance value under this criterion.

4.1.3 Ability to Represent Historic Themes

NSW Historical Theme	Relevance to Campbell's Stores
Industry	The Stores were a typical warehouse built for commercial storage purposes in the 1850s. The association with commercial activities continues to this day, though servicing a different clientele with four restaurants tenanting the building.
Commerce	The Stores continual and evolving operation, from commercial storage in the mid-nineteenth century to commercial dining in the twenty-first century, evidences the increasing and changing commercial prosperity of the colony and of The Rocks area.
Persons	The Stores themselves are associated with the arrival, establishment and success of a prominent immigrant merchant dynasty – the Campbells – who were major figures in the early commercial trade and transport industry of the colony.

4.2 Statement of Significance

Campbell's Stores is a superb example of mid-nineteenth century warehouse buildings, now rare in Sydney. It is the only warehouse of its type remaining on the foreshore of Sydney Cove, the hub of commerce and international shipping transport until the late nineteenth century. As a memorable landmark in The Rocks, visible from a wide area of Sydney Harbour, it is a symbol of mid-nineteenth-century Sydney.

Campbell's Stores has historic significance for its association with the Campbell family, one of the most influential families in early Colonial Australia. It is the surviving element of a complex of wharves and stores that began in 1801 with the construction by Robert Campbell of the first privately-owned wharf in Australia. Later significant associations include the Australasian Steam Navigation Company, one of the most important commercial shipping and transport companies in Australia, and the Sydney Harbour Trust, established by the Government following of the bubonic plague scare of 1900.

Campbell's Stores is significant for its association with commercial Bond and Free store usage for over one hundred and twenty years, with each successive owner (including the Sydney Harbour Trust and Maritime Services Board) leasing sections of the Stores to a variety of merchant companies.

The changes made to Campbell's Stores provide evidence of the changing commercial fortunes of maritime Sydney. The construction of the first five bays demonstrates economic growth following the 1840s depression and the additional six bays demonstrate further economic growth following the 1850s gold rushes. The construction of the third level by the ASN Co in the mid-1880s demonstrates a further period of economic growth and also of a change in the functional operation of the Stores, as evidenced by the inter-connection of the top floor spaces. The adaptive reuse of the building in the 1970s represents an early approach to the conservation of historic buildings. The continued subsequent use of the building for a series of restaurants demonstrates the changing uses of Sydney Cove from industrial purposes to largely tourist-related purposes.

The design, form and materials of Campbell's Stores contribute to its aesthetic significance as a complex of buildings of high visual and sensory appeal. Their design elements reflect their original function in a simple but dignified manner. Their form is a coherent whole, made up of repetitive gabled bays combined with an undulating rhythm of door and window openings. The consistent use of sandstone, brick and slate materials reinforces this visual coherence and provides an appearance of solidity and quality.

Campbell's Stores represents a surviving example of mid-nineteenth-century style warehouses; a building type once common around Sydney Cove, but now rare. The gabled bay form, cathead beams, hoists, goods aprons and doors are evidence of an older warehouse style. The form, bars on openings and lack of internal connections between bays evidence the security required for bond store use.

Campbell's Stores has technical/research significance because of its potential to contribute further to our understanding of the early maritime activity around Sydney Cove and, in particular, within the Campbell's Wharf complex. It also has the ability to contribute further to our understanding of the use and operations of mid-nineteenth-century warehouse buildings, particularly in relation to goods handling and the changes in technology that occurred over time.

The remnant hoisting equipment of the Campbell's Stores building provides evidence of the changes of technology in goods handling and haulage that occurred during the nineteenth and twentieth century. The hydraulic hoisting equipment and the winches in particular are evocative of the industrial nature of the site and the hydraulic hoists are prominent examples within the Sydney area. They demonstrate the scale and efficiency of the industrial processes undertaken at Campbell's Stores during its use as dockside goods storage.

4.3 Curtilage

4.3.1 Definition of a Heritage Curtilage

Heritage curtilages are essential to the understanding of the cultural significance of heritage items. The heritage curtilage should contain evidence of any cultural associations as well as providing a visual and aesthetic context for the heritage item.

According to the NSW Heritage Office publication, *Heritage Curtilages*⁴:

A 'heritage curtilage' is the area of land (including land covered by water) surrounding an item or area of heritage significance, which is essential for retaining and interpreting its heritage significance. It can apply to either:

- land which is integral to the heritage significance of items of the built heritage; or
- a precinct which includes buildings, works, relics, trees or places and their setting.

The curtilage may not necessarily coincide with the property boundary but may include its visual setting, views and its context within the environment. The heritage curtilage should contain all elements contributing to the heritage significance, conservation and interpretation of a heritage item. There are four types of heritage curtilages⁵:

- *Lot boundary heritage curtilage*—arises when the property boundary containing the heritage item as shown on the lot plan is the heritage curtilage. The property may also contain associated buildings, gardens and other significant features, including walls, fences, driveways or tennis courts, which contribute to the heritage significance of the property. This is the most common form of heritage curtilages.
- *Reduced heritage curtilage*—where the heritage curtilage is less than the lot boundary of the property. This will occur when the significance of the item does not relate to the total lot but to a lesser area and is often only defined when development of the property is proposed.
- *Expanded heritage curtilage*—where the heritage curtilage is greater than the property boundary and an expanded curtilage is required to protect the landscape setting or visual catchment of a heritage item.
- *Composite heritage curtilage*—applies to heritage conservation areas and defines the boundaries of land required to identify and maintain the heritage significance of a historic district, village or suburban precinct. The curtilage includes items which have a distinctive homogeneous character.

4.3.2 Heritage Curtilage Methodology

In defining a heritage curtilage for Campbell's Stores, the following factors have been considered and analysed within the context of the established heritage significance of the place⁶:

- land subdivision pattern;
- design and setting;
- functional uses and interrelationships;
- views and visual links between the house and topographical features;
- provision of an adequate and suitably proportioned setting; and
- significant features.

Article 8 of the Burra Charter, titled 'Setting', states:

Conservation requires the retention of an appropriate visual setting and other relationships that contribute to the cultural significance of the place.

New construction, demolition, intrusions or other changes which would adversely affect the setting or relationships are not appropriate.

The accompanying explanatory note also states:

Other relationships, such as historical connections, may contribute to interpretation, appreciation, enjoyment or experience of the place.

4.3.3 Heritage Curtilage of Campbell's Stores

The site is located on the western edge of Circular Quay, north of Elevated Road and to the east of Hickson Road. The primary address is identified as 7-27 Circular Quay West, The Rocks, NSW 2000.

The site comprises the land to the north (towards the Park Hyatt Hotel), to the harbour sea wall to the east, and to the southeast covering the site of the original Campbell's Stores building. Campbell's Stores comprises eleven gable-fronted, three-storey high, rectangular-plan bays. The bays are oriented almost due east-west. The building as a whole is oriented almost due north-south.

The heritage curtilage is the area required for retention of significant views or for adequate presentation and interpretation of the item. In this case, Hickson Road provides Campbell's Stores enough space so the western façade can be viewed from across the street. To the east, it is the promenade in front of the Campbell's Stores. The proposed heritage curtilage extends from the Overseas Passenger Terminal overpass to the Park Hyatt Hotel (Figure 4.3). Note that the SHR listing does not include a heritage curtilage.

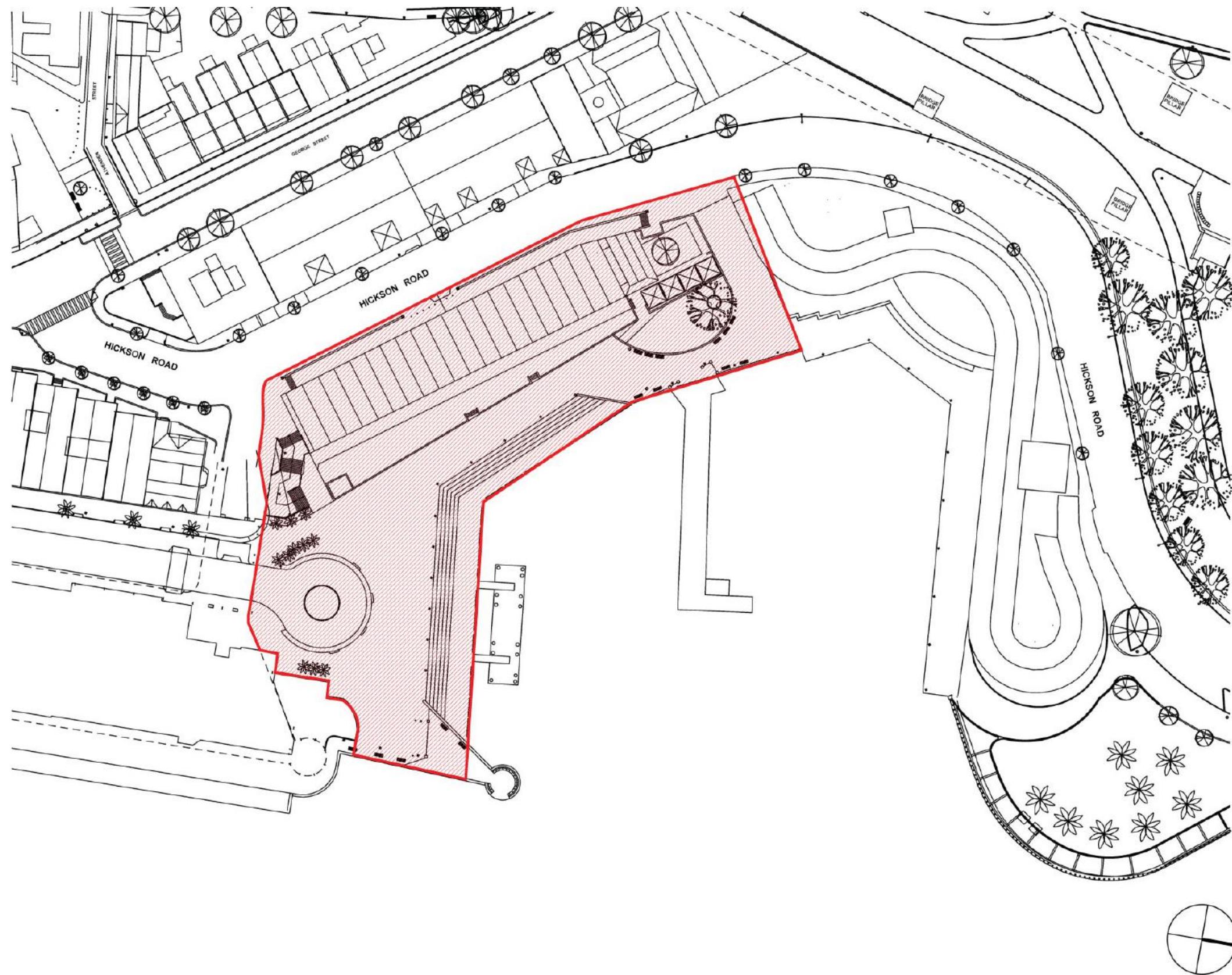


Figure 4.3 Proposed heritage curtilage of Campbell's Stores.

4.4 Endnotes

- ¹ NSW Heritage Branch, State Heritage Inventory Sheet, 'Metcalfe Bond Stores'
- ² NSW Heritage Branch, State Heritage Inventory Sheet, 'Metcalfe Bond Stores'
- ³ NSW Heritage Branch, State Heritage Inventory Sheet, 'Coach House - McKeller Stores'
- ⁴ NSW Heritage Office 1996, *Heritage Curtilages*, Department of Urban Affairs and Planning.
- ⁵ ibid pp 5–8.
- ⁶ ibid p 9.

5.0 Grading of Significance

5.1 Introduction

Grading reflects the contribution the element makes to overall significance of the item, and the degree to which the significance of the item would be diminished if the component were removed or altered. Campbell's Stores has been assessed to determine a relative grading of significance into five levels. This process examines a number of factors, including:

- original design quality;
- degree of intactness;
- relative age and authenticity (original, replaced);
- extent of subsequent alterations;
- association with important people or events; and
- ability to demonstrate a rare quality, craft or construction process.

The standard five-grade system has been applied to assess individual contribution of each element to the overall significance of the item. This system is a planning tool and assists in the development of a consistent approach to the treatment of different elements. The various grades of significance generate different requirements for retention and conservation of individual spaces and the various elements. The following grades of significance have been defined:

Exceptional Significance

Includes fabric that makes the greatest direct contribution to the item's significance. This particularly refers to rare or outstanding original fabric and spaces of particular historic and aesthetic value, and unaltered original elements and features.

Elements identified as being of Exceptional significance should be retained and conserved in situ. Any work which affects the fabric or external appearance of these elements should be confined to preservation, restoration and reconstruction as defined by the Burra Charter.

High Significance

Includes elements and features that make an important contribution to the recognition of the item's significance, although the fabric may not be in good condition. This may include elements that have been altered, or elements created as part of a generally sympathetic alteration to the building. This category is likely to include much of the extant fabric from the early phases of construction and many reconstructed early or original elements wherever these make an important contribution to the significance of the item.

Elements identified as being of High significance should also generally be retained, restored and conserved in situ, subject to other relevant factors, including technological feasibility of proposed works. Minor intervention into fabric including adaptation and alteration as defined by the Burra Charter is permissible, provided that the level of significance of each element is retained, with an aim not to remove or obscure significant fabric, giving preference to changes which are reversible.

Moderate Significance

Includes building fabric and relationships that are supportive of the overall significance of the item and have some heritage value, but do not make an important or key contribution to that significance. Also includes elements and features which were originally of higher significance, but have been compromised by later, less significant modifications or elements that have deteriorated beyond repair and cannot be reconstructed in a technologically feasible manner.

Where the fabric is of Moderate significance a greater level of intervention is permissible. Adaptation and relocation to components of these elements and spaces is acceptable provided that it protects the overall cultural significance of the item. Such work should take place within defined work programs and should not be the product of general maintenance or sporadic alterations.

Little Significance

Includes fabric which detracts from the heritage value of the item. Also includes most of the fabric associated with unsympathetic alterations and additions made to accommodate changing functional requirements. These are components generally of neutral impact on the complex's significance.

Elements assessed as being of Little significance are generally not regarded as essential to the major aspects of significance of a building or place, often fulfilling a functional role. Both retention and removal are acceptable options, depending on the element. Any major interventions to the item should be confined to areas where the fabric is of little significance.

Intrusive

Includes fabric which adversely affects the significance of the complex or fabric created without respect for the intangible values of the building. Removal of elements of this category would directly increase the overall heritage value of the item.

Elements identified as Intrusive can reduce or obscure the overall significance of the place, despite their role as illustrators of the site's progressive development. The preferred option is for their removal, conversion to a more compatible form, or replacement in a way which helps to retain the overall significance of the item. These works should be done without damage to adjacent fabric of significance. These items need not be addressed immediately.

This grading has been established as a tool to assist in developing appropriate conservation measures for the treatment of Campbell's Stores and its elements. Good conservation practice encourages the focussing on change or upgrading of historical buildings to those areas or components which make a lesser contribution to significance. The areas or components that make a greater or defining contribution to significance should generally be left intact or changed with the greatest care and respect.

5.2 Grading of Significant Elements and Spaces

Section 3.0 identified that the fabric of Campbell's Stores falls into the following general categories:

- original fabric from the first three phases of construction (up to c1885);
- Bay 11 (Branch Stores Government Printer) c1895–1915;
- alterations made to original fabric from the above two periods (eg new door openings made on Level 2) as a result of the construction of Hickson Road in 1915;

- reconstruction of fabric in 1970s works (eg sandstone restoration of eastern ground floor facade);
- fabric introduced to provide for new uses in 1970s works (eg service tunnel at rear);
- non essential fabric introduced primarily for decorative reasons (eg sailboat masts in front of the Waterfront restaurant); and
- recent commercial restaurant fitouts, such as kitchens, bars and stores.

5.2.1 Fabric Significance Gradings

Based upon the significance analysis in Section 5.0, the fabric groupings can be ranked according to categories. The following gradings of significance should be read in conjunction with the inventory and plans of significance gradings contained in Section 5.3.

Exceptional Significance

- Original fabric from the first three phases of construction (up to c1885).

High Significance

- Bay 11 (Branch Stores Government Printer) c1895.
- Bay 10 alterations made to facilitate construction of Hickson Road, c1915.
- New door openings made on Level 2 as a result of the construction of Hickson Road in c1915.

Moderate Significance

- Restoration and reconstruction of fabric as part of 1970s restoration program (eg sandstone restoration of eastern ground floor facade).

Little Significance

- Ongoing restaurant fitouts that are reversible and allow for an appreciation of original spaces and fabric.
- Fabric introduced to provide for new uses in 1970s works (eg service tunnel at rear, original restaurant adaptation work, etc).

Intrusive Fabric

- Fabric introduced primarily for decorative reasons (eg sailboat masts in front of the Waterfront restaurant, 'themed' fit-out works, etc, that obscure original spaces and fabric, or cause visual impacts on an appreciation of original spaces and fabric).
- Fabric which obscures and contributes to the deterioration of fabric requiring maintenance and conservation works. Such fabric includes linings added to walls, cool rooms and so on.

5.2.2 Fabric and Spaces of Exceptional Significance

Exterior

- All fabric dating from the period 1850s to c1885, including original structure and wall fabric Bays 1–10.
- Goods aprons, handles and reveals.
- Bay 11 apron and door openings indicating original floor levels.
- Hydraulic hoists in front of Bays 1 and 8.
- The hand-operated jib crane associated with Campbell's Wharf from the 1840s and later moved to Goat Island, and now on display in the Museum of Sydney.

Interior

- Original internal sandstone walls.
- Original window openings.
- Window bars on Levels 1, 2 and 3.
- Level 2 and 3 floors structure and boarding.
- Posts on Level 1 and 2.
- Beams and joists on Level 2 and 3.
- Roof carpentry (including cathead beams and structure).
- Original doors Levels 1, 2 and 3.
- Original timber shutters.
- All evidence of internal staircases including limewash/paint on walls on Level 2.
- Original doorways and reveals between bays on Level 3.
- Closed in original openings between bays (eg Bay 3 Level 3).
- Original plaster and remnant sections/fragments of limewash (particularly around reveals of openings).

Internal and external hoists.

- Original internal spaces, particularly those unaltered by later partitions staircases and atrium openings.
 - i. Hand-sawn timber pads for former position of trusses on Level 2.
 - ii. Winches on Level 3 of Bays 3 and 6, and associated mechanical equipment.

5.2.3 Fabric and Spaces of High Significance

Exterior

- Changed openings to Level 2 as a result of 1915 Hickson Road works.
- Rear wall realignment Bay 10 — Hickson Road works.
- Bay 11 original or early brickwork.
- Replacement cathead beams.
- Northern courtyard space.

Interior

- Door openings formed as a result of 1915 works.

5.2.4 Fabric and Spaces of Moderate Significance

Exterior

- 1970s sandstone replacement works.
- Slate roof replacement, barge boards, rain water heads and down pipes from 1970s works.
- External stone paving at east forecourt (1970s work).
- The forecourt (1988 work).

Interior

- 1970s timber floors.

5.2.5 Fabric and Spaces of Little Significance

Exterior

- Glazing to door openings.
- Projecting signs fixed to sandstone walls.

Interior

- Door openings made between spaces in the 1970s works.
- Recent internal fitout to the Waterfront.
- Recently infilled 1970s and 1980s void spaces.
- Level 1 changes to original windows on west elevation for access to 1970s service tunnel.
- Hickson Road service tunnel and 1970s duct wall adjacent to southern external staircase.

5.2.6 Intrusive Fabric and Spaces

Exterior

- Sailing boat masts in the forecourt in front of the Waterfront restaurant.
- Plastic shade fabric covering outdoor eating area.
- Staircase linking Hickson Road to the harbourside promenade.

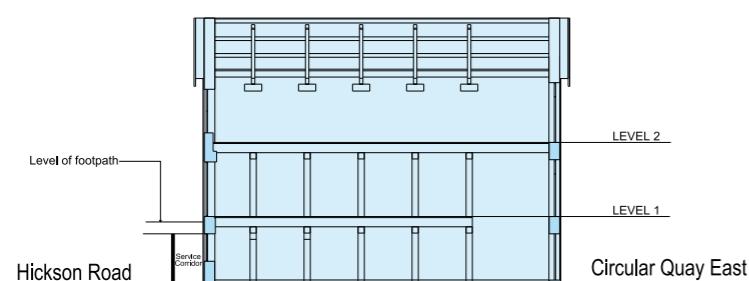
- Glass pavilion in the northern courtyard.

Interior

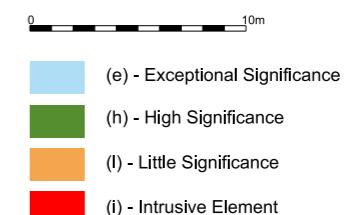
- Introduced staircases and decorative wall finishes and fixtures fixed to original fabric.
- Partitions and walls subdividing internal spaces that diminish an appreciation of the original spaces, including most kitchen and toilet facilities.
- Decorative internal elements including false mezzanines, and partitions attached to original fabric.
- Modern floor finishes.
- Paint and finishes to internal timber structure.
- Sand-blasted finish to original masonry and timber structure.
- Services and fittings attached to original structure.
- Floor voids.

5.3 Inventory Plans and Significance Gradings

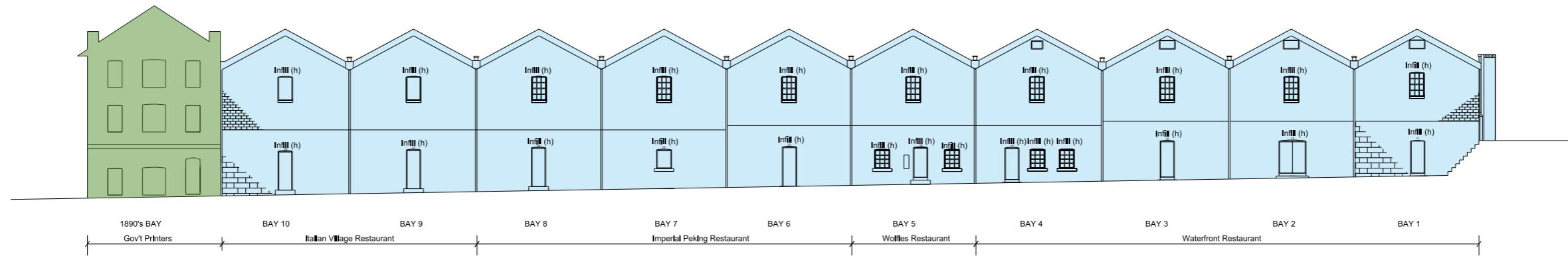
The following plans, prepared by SHFA, indicate the gradings of significance for the fabric of Campbell's Stores.



TYPICAL EAST-WEST SECTION



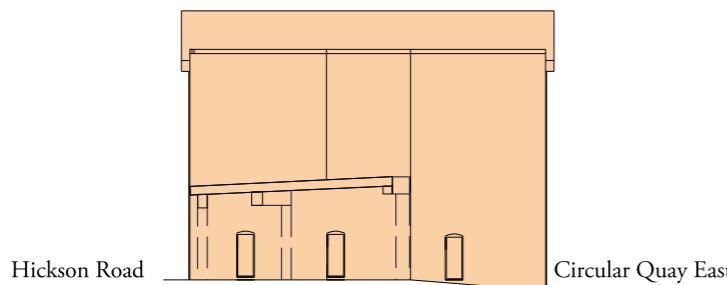
PLEASE NOTE:
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WEST ELEVATION



EAST ELEVATION

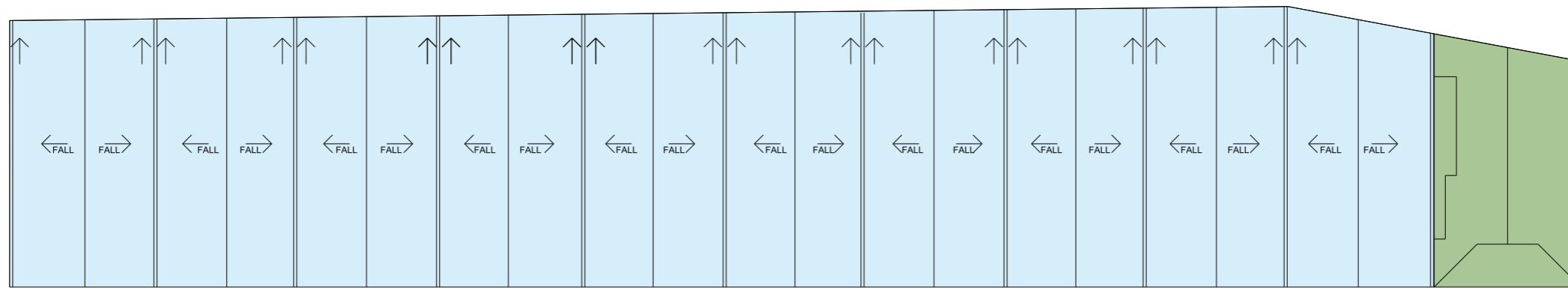


SOUTH WALL ELEVATION

- (e) - Exceptional Significance
- (h) - High Significance
- (l) - Little Significance
- (i) - Intrusive Element

PLEASE NOTE:
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0 10m

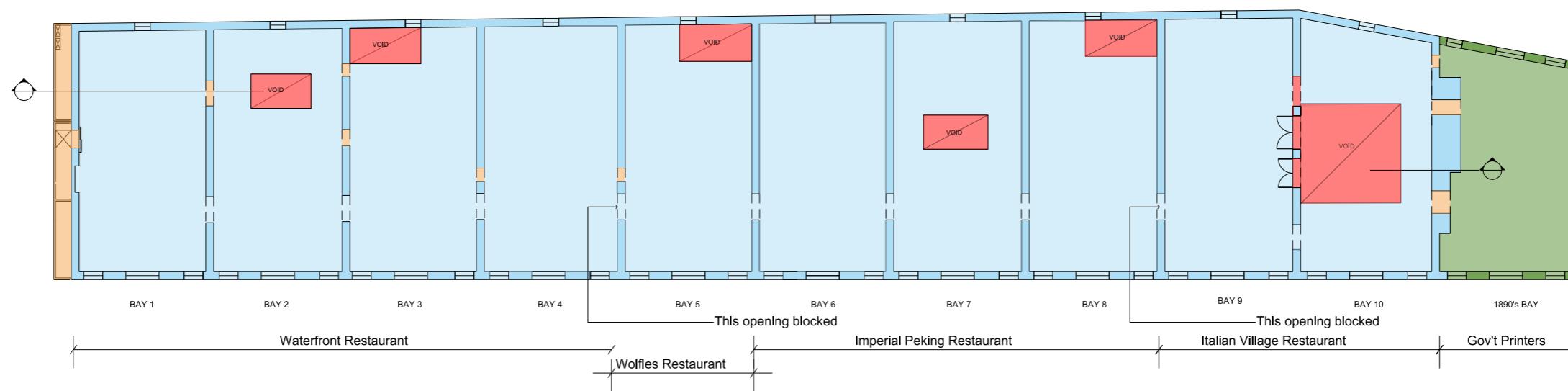


ROOF PLAN

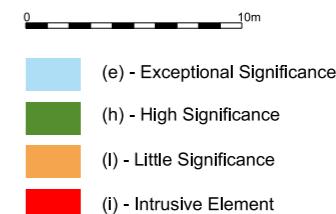
0 10m

- (e) - Exceptional Significance
- (h) - High Significance
- (l) - Little Significance
- (i) - Intrusive Element

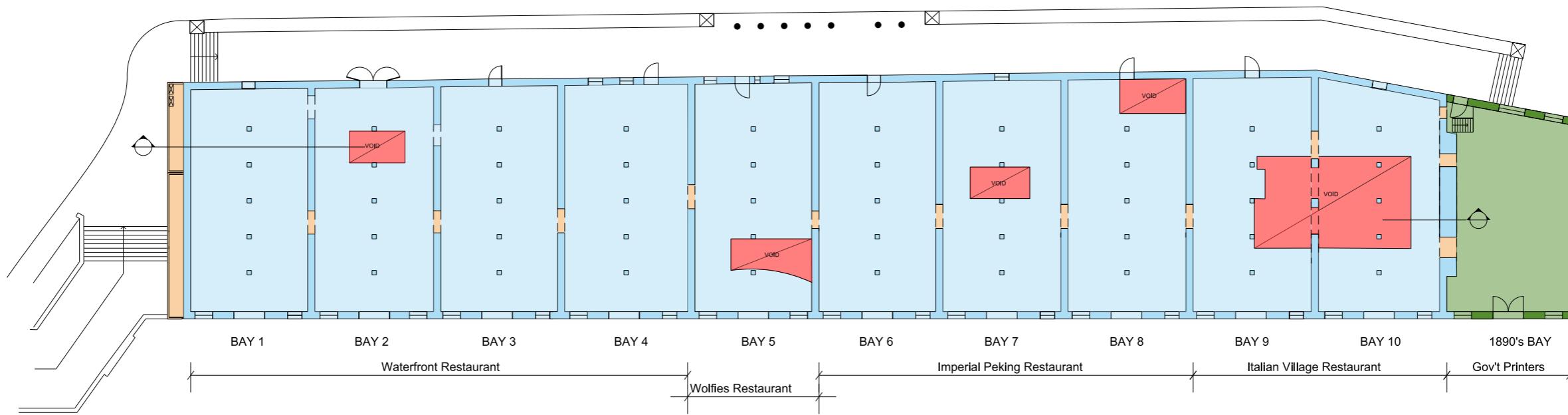
PLEASE NOTE:
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FIRST FLOOR



PLEASE NOTE:
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GROUND FLOOR (Hickson Road)

0 10m

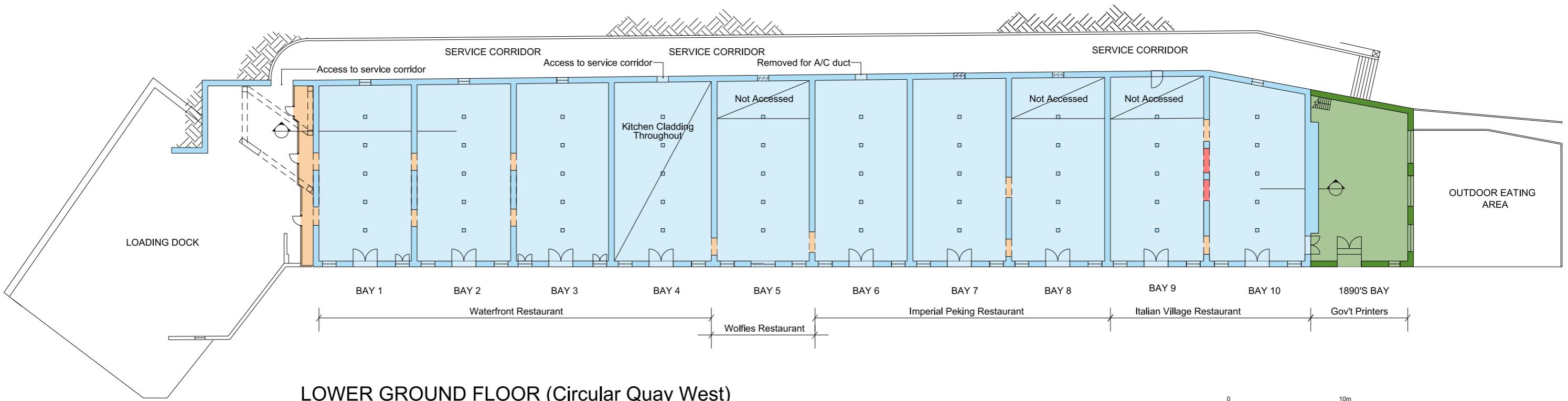
(e) - Exceptional Significance

(h) - High Significance

(l) - Little Significance

(i) - Intrusive Element

PLEASE NOTE:
Print in colour for legibility



0 10m

- (e) - Exceptional Significance
- (h) - High Significance
- (l) - Little Significance
- (i) - Intrusive Element

PLEASE NOTE:
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5.4 Archaeological Significance

The Campbell's Stores site has potential archaeological, scientific and research significance relevant to earlier uses and the development of the site. The archaeological significance may be reduced due to disturbance from later alterations and refurbishment works to the site.

The archaeological potential is considered low. The site should be monitored by a qualified archaeologist during any ground disturbance in accordance with the *NSW Heritage Act 1977* archaeological provisions.

6.0 Constraints and Opportunities

This section outlines major issues for the conservation of the site. It takes into consideration matters arising from the statement of significance and procedural constraints imposed by cultural conservation methodology, such as that of the Burra Charter, and identifies all statutory and non-statutory listings that apply for the site, as well as describing constraints and opportunities arising from these listings.

6.1 Issues Arising from Physical Condition of the Place

Campbell's Stores was the subject of an extensive conservation program in the 1970s and has undergone other conservation works and maintenance since that time. Externally, the building is generally in fair condition but has not been the subject of a co-ordinated works campaign for some considerable period.

Readily apparent deterioration has occurred to many of the eaves and fascia boards, in particular in the areas surrounding the rainwater heads. An inspection of the roof also revealed that the masonry dividing wall that projects above the box gutters is not capped and is in poor condition, possibly resulting in water ingress. The timber apron boards hanging from chains at the loading door entries were in very poor condition; however, maintenance has been carried out on these during 2010.

The application of a sandstone skin externally on the ground floor did not solve the inherent rising damp problem. The relatively recent construction of a thin concrete topping slab against the eastern wall of the ground floor is likely to further exacerbate any rising damp problems.

Internally, the majority of the fabric is in fair condition. However, some sections of internal sandstone walling on the lowest floor level are in a highly deteriorated condition, and it is apparent that this deterioration is continuing. Various techniques have been attempted to arrest the rate of deterioration that is due to salt damp attack caused by salt water entering the building through its foundations. The water evaporates above floor level in the building leaving the salt crystals in the outer surface of the stone which later absorb further moisture, expand and blow away the stone surface.

Various treatments had been attempted to alleviate the problem, including the application of a sacrificial render at ground floor level on some of the most damaged internal surfaces. This is a palliative treatment only and does not overcome the cause of the problem. The introduction of a positive damp course is an option that could be further investigated. This would be a costly and disruptive process which is likely to have some impact on the operation of the various restaurants while work is in progress. Positive falls away from the building need to be introduced to shed water away from the sandstone walls. This could be achieved by addressing the levels around the building and dropping the forecourt level in front of the building and moving the outdoor dining area away from the eastern façade. This should be addressed during the preparation of a masterplan for the area. In addition, a cleaning policy should be developed and enforced to ensure that daily hosing of internal and outdoors areas ceases.

The loss of significant original fabric and the continued deterioration of the sandstone may eventually lead to structural damage within the building. There is clear evidence that, in some

cases, some of the sandstone blocks, notably in parts of the Imperial Peking restaurant, have almost reached the limit of their structural integrity.

In some areas, the problem is being exacerbated by the fixing of wall linings over the base levels of the sandstone walls which masks the problem and does not allow the extent of deterioration to be monitored. Further, in some areas, linings have been applied over the layer of sacrificial render, thus compromising its ability to act 'sacrificially'.

Elsewhere throughout the building, alteration and fit-out work has occurred that has caused damage to, or is obscuring, significant fabric. It would appear that some of this work has been planned and been given formal approval, while other work appears to be more haphazard and spontaneous, with little thought given to the significance of the fabric so affected. Examples of the latter include the inappropriate placement of light fittings, heaters, ceiling fans, electrical cabling and the like. Similar works have been undertaken externally, including the introduction of an aerial mast on the Hickson Road elevation, the insertion of fixing bolts into the sandstone walls, the fixing of umbrella posts and the like, and the increasing accumulation of awnings, umbrellas and planter boxes and tubs.

The use of Campbell's Stores as restaurants has also resulted in the loss of fabric and interruption of internal spatial volumes due to the introduction of services and facilities such as kitchen fit-out, toilets and bar areas, air conditioning, exhaust ducting, alterations to accommodate air conditioning services, etc. The regular hosing of the forecourt area to remove food scraps may also be contributing to the sandstone deterioration within the building.

The use of the property for other purposes such as commercial offices or retailing could potentially place fewer demands on the fabric than the current restaurant uses. However, these alternative uses would not provide the same level of public access and exposure to the property as is currently afforded. Further, the restaurant use per se is not the main cause of these above problems, but rather the manner in which the fit-outs have been designed and the work carried out. Incremental alterations over time, some apparently without a great deal of consideration, have exacerbated the situation.

Some of these problems may be attributed to the difficulty of SHFA, as owners of the building, to effectively control and/or monitor the activities of its tenants due, in part, to the long-term nature of the lease arrangements and the (not unreasonable) provisions relating to the tenants' rights for the 'quiet enjoyment' of the lease. It is important that both SHFA and its tenants understand the cultural significance of Campbell's Stores and the constraints that must be observed if the property is to retain its significance for the future.

6.2 Sydney Harbour Foreshore Authority

The Rocks is managed by SHFA, constituted on 1 February 1999 under the *Sydney Harbour Foreshore Act 1998*.

The primary functions of SHFA, stated in its Act, are to:

- protect and enhance the natural and cultural heritage of the foreshore area;
- promote, coordinate, manage, undertake and secure the orderly and economic development and use of the foreshore area, including the provision of infrastructure; and

- promote, coordinate, organise, manage, undertake, secure, provide and conduct cultural, educational, commercial, tourist, recreational, entertainment and transport activities and facilities.

In recognition of the importance of heritage conservation in the area, The Rocks Heritage Management Plan (adopted in February 2002 and revised in 2010) has been prepared to provide a basis for understanding and conserving the heritage value of The Rocks and to assist in preparing individual CMPs for heritage buildings within The Rocks.

6.2.1 Sydney Harbour Foreshore Authority's Vision and Purpose

Vision

To make unique places in Sydney that the world talks about.

Our purpose

To create and sustain living places and great experiences.

The Authority does this by:

- Managing places profitably and socially to deliver excellence in our role as manager for Sydney's significant waterfront and other precincts, balancing visitor, community and commercial expectations and as a custodian, to ensure the preservation and interpretation of our natural and cultural heritage.
- Effective promotion, capitalising on the economic and cultural worth of our places as core attractions for both visitors and Sydneysiders.
- Developing places responsibly to demonstrate leadership in creating quality environments that are enriching, diverse, accessible and sustainable. To add value by redeveloping surplus government land through a highly-skilled organisation.
- Managing the organisation efficiently to meet customer and stakeholder needs and expectations.

6.2.2 Asset Owner's Requirements

SHFA proposes to continue to use Campbell's Store for commercial restaurant and dining uses.

6.3 Heritage Management Framework

In 2009, the NSW Government announced amendments to the State's planning portfolio effective from 1 May 2009. These amendments require Development Applications (DAs), previously assessed by the Authority, to be assessed by the Council of the City of Sydney or Department of Planning.

It is essential that SHFA and the relevant Consent Authority are consulted in order to determine any approvals that may be required for the execution of any works. Approvals for works may be required under both the Environmental Planning and Assessment Act and the Heritage Act. All statutory applications require the signature and consent of the landowner to enable lodgement, as such all applications should be forwarded to SHFA for endorsement prior to submission with the NSW Department of Planning. Further information about the granting of landowner's consent can be sourced from SHFA's website.

6.3.1 New South Wales Heritage Act

Architectural Works

Campbell's Store is listed in the NSW SHR, under No.01536. This listing recognises the site as being of State significance and provides statutory protection under the Heritage Act.

Under Section 57(1) of the Act, Heritage Council approval is required for a wide range of works to a SHR item. Unless an item constitutes a danger to its occupants or the public, demolition of a SHR item is *prohibited* under the Heritage Act. An element of a SHR item may only be demolished if it does not contribute to the significance of the item.

To gain approval for any of the above works, an application must be made to the Heritage Council (Section 60 application). SHFA can determine Section 60 applications for minor works which do not materially affect the significance of the item under delegation. SHFA can advise applicants whether applications can be assessed under delegation, or whether they need to be approved by the Heritage Council. Section 60 application forms are available from the NSW Heritage Office, or from SHFA.

Archaeological management

Under Section 57(1) of the Heritage Act Heritage Council approval is required to move, damage, or destroy a relic listed in the SHR, or to excavate or disturb land which is listed on the SHR and there is reasonable knowledge or likelihood of relics being disturbed. The Act defines a 'relic' as:

Any deposit, object or material evidence which:

- (a) relates to the settlement of the area that comprises New South Wales, not being an Aboriginal settlement; and
- (b) is 50 or more years old.

A Section 60 application is required to disturb relics on an SHR listed site. The delegation to SHFA to approve minor Section 60 applications extends to archaeological works.

Standard exemptions for works to State Heritage Register listed items

Under Section 57(2) of the Heritage Act, the Minister may make exemptions from approval otherwise required under Section 57(1) for works to SHR items. Such exemptions are intended to streamline the approvals process. There are two types of exemptions:

- Standard exemptions: apply to all SHR items and cover maintenance and repair and minor alterations.
- Site specific exemptions: apply only to an individual SHR item.

If proposed works are covered by a standard exemption, an Exemption Notification Form (not a Section 60 application) must be completed, with sufficient detail provided to determine whether the proposed works meet the standard exemption guidelines. SHFA has delegation from the Heritage Council to approve standard exemption applications. Forms are available from the Authority.

Applicants should confirm with SHFA whether proposed works fall within the standard exemptions and what documentation will be required.

Site-specific exemptions relate to the particular requirements of an individual SHR item, and can only be for works which have no potential to materially affect the significance of the item. Site specific exemptions are only applicable if the works to which they refer are identified as exempt development in a CMP endorsed by the Heritage Council, or in a Conservation Management Strategy endorsed under delegation by the Executive Manager of the Property and Asset Management division, SHFA.

Minimum Standards of Maintenance and Repair

Section 118 of the Heritage Act provides for the regulation of minimum standards for the maintenance and repair of SHR items. These standards were regulated in 1999 and apply to all SHR items. The minimum standards cover the following areas:

- Weatherproofing.
- Fire protection.
- Security.
- Essential maintenance.

An inspection to ensure that the item is being managed in accordance with the minimum standards must be conducted at least once every year (or at least once every three years for essential maintenance and repair standards).

Failure to meet the minimum standards may result in an order from the Heritage Council to do or refrain from doing any works necessary to ensure the standards are met. Failure to comply with an order can result in the resumption of land, a prohibition on development, or fines and imprisonment.

6.3.2 NSW Government Total Asset Management Guidelines

The Total Asset Management (TAM) manual prepared by NSW Government's Asset Management Committee requires that: 'sustainable management of heritage values should be treated by an agency as part of its core business'. Similarly, TAM Guidelines for Government agencies prepared by NSW Treasury require 'planning use of heritage assets to maximise their service delivery while protecting their cultural values'.

While protection of built heritage in The Rocks area is part of the core business of SHFA, these TAM guidelines can be interpreted to indicate that the retention of heritage value overrides the financial feasibility of the ongoing reuse of items. The cultural importance of The Rocks is widely recognised as paramount, and in any eventual situation where retention of heritage significance could collide with the financial feasibility of the project, the importance of retention of the heritage significance shall be given a priority.

6.4 Environmental Planning Instruments (EPIs)

There are three main EPIs under the Environmental Planning and Assessment Act 1979 that are applicable to The Rocks:

A. State Environmental Planning Policy (Major Developments) 2005

Consent Authority Roles

On 1 May 2009 an amendment to the State Environmental Planning Policy (Major Developments) 2005 was made that transferred to consent authority for certain development on Sydney Harbour Foreshore sites from the Minister for Planning to the City of Sydney Council.

Sydney Harbour Foreshore sites are those identified on Map 9 of Schedule 2 of the Major Developments SEPP including:

- Circular Quay;
- The Rocks;
- Walsh Bay (Wharf 2/3);
- Darling Harbour;
- Casino (Switching Station site); and
- Bank Street (including the Sydney Fish Market).

From 1 May 2009 all new DAs under Part 4 of the Act (Capital Investment under \$5 Million), within the above areas, are to be lodged with the City of Sydney Council, except as provided for by the provisions of the amended SEPP. Within these provisions The Minister will remain the consent authority in these areas for development that:

- requires an approval under section 57 of the Heritage Act 1977 (unless that approval has already been obtained);
- does not comply with the approved Sydney Cove Redevelopment Authority Scheme for The Rocks, which is essentially the height and building envelop prescribed for each site (See Section 1.2C); or
- has a Capital Investment Value in excess of \$5 Million, which requires a Major Project application under Part 3A of the Act.

Any DA subject to the provisions listed above is to be lodged with the Government Land & Social Projects Branch of the NSW Department of Planning.

Any application under section 96 of the Act, for a modification to a development consent issued by the Minister, including consents issued under delegation by the Department or SHFA, will continue to be lodged with the Department.

Exempt Development

The amended SEPP has introduced additional Exempt Development for Circular Quay, The Rocks and Darling Harbour. This will allow for a range of temporary uses and other activities to be undertaken without the need to obtain development consent.

These temporary uses and activities include:

- short-term events at places such as First Fleet Park (The Rocks) and Tumbalong Park (Darling Harbour);
- premises trading out onto the public street, for up to six occasions per year, such as on Australia Day and ANZAC Day;
- fireworks and lighting displays; and
- Christmas decorations

These temporary uses and activities are subject to other limitations on hours and duration, required approvals under the Heritage Act and controls applying to limit impacts on the environment.

The consent of the landowner must always be obtained before undertaking any exempt development.

B. Sydney Regional Environmental Plan (Sydney Harbour Catchment) 2005

The Rocks and Darling Harbour is included in the 'Foreshores and waterways area' defined in the Sydney Regional and Environment Plan (SREP), and as such any development must be considered in terms of the criteria set out in clauses 21 to 27 of the SREP. The SREP does not identify any heritage items within The Rocks and Darling Harbour; however, it does identify the Sydney Harbour Bridge and the Circular Quay ferry wharves as heritage items and any development within their vicinity needs to be considered in terms of impacts on these items, including views to the items. Amendment 1 to the SREP, gazetted 27 July 2007, also identifies a 'buffer zone' for development in the vicinity of Sydney Opera House, which covers a majority of The Rocks area, and any impacts on views to the Opera House are to be taken into consideration.

The SREP also requires the consideration of the potential to impact on archaeological or potential archaeological relics.

The DCP that exists under the SREP is also relevant to development in The Rocks and Darling Harbour. Its primary focus is on ecological and landscape character areas, and built form design guidelines for development very close to the water's edge.

C. Sydney Cove Redevelopment Authority (SCRA) Scheme

The Rocks is covered by the SCRA scheme which has the status of an EPI under Schedule 6 of the Environmental Planning and Assessment Act 1979. The SCRA scheme comprises a series of building site control drawings which provide for the maximum height of buildings on any particular site, and which also specify permissible uses for a particular site or precinct.

Non-statutory listings and community groups

6.4.1 National Trust of Australia (NSW)

Campbell's Store has been classified by the National Trust of Australia (NSW).

Listing on the register of the National Trust carries no statutory implications. The Trust's opinions, however, are usually sought when major proposals are being formulated in heritage precincts or in relation to heritage buildings.

6.4.2 Australian Heritage Commission—Register of the National Estate

'Campbell's Storehouse' has been classified by the Australian Heritage Commission and listed in the Register of the National Estate (RNE).

Following amendments to the *Australian Heritage Council Act 2003*, the RNE was frozen on 19 February 2007, which means that no new places can be added, or removed.

The Register will continue as a statutory register until February 2012. During this period the Minister for the Environment, Heritage and the Arts (the Minister) is required to continue considering the Register when making some decisions under the *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act). This transition period also allows states, territories, local and the Australian Government to complete the task of transferring places to appropriate heritage registers where necessary and to amend legislation that refers to the RNE as a statutory list.

From February 2012 all references to the Register will be removed from the EPBC Act and the AHC Act. The RNE will be maintained after this time on a non-statutory basis as a publicly available archive.

6.5 Building Regulations

The Building Code of Australia (BCA) is concerned with establishing uniform building regulations across Australia. The BCA is implemented in NSW through the *Local Government Act 1993*.

The main provisions of the BCA concern structural requirements, fire resistance, access and egress (including provisions for people with disabilities), services and equipment and health and amenities. Generally, minimum standards are required to be reached in building works.

An assessment of compliance of the complex with the BCA has not been carried out for this report. It is appreciated however that any DA/CC application will need to comply, or be deemed to comply with the BCA.

In general, when considering the BCA in heritage buildings, proposals must ensure that significant fabric and spatial qualities are not compromised while full BCA compliance is achieved and public safety is assured.

6.6 Obligations Arising from the Burra Charter

The Burra Charter, as revised in 1999, provides specific guidelines for the treatment of places of cultural significance. This study has been prepared in accordance with those principles. The Charter provides specific guidance for physical and procedural actions that should occur in relation to significant places. The following are constraints that arise from the relevant articles of the Charter:

Article 2 Conservation and Management

- 2.1 *Places of cultural significance* should be conserved.
- 2.2 The aim of *conservation* is to retain the *cultural significance* of a *place*.
- 2.3 *Conservation* is an integral part of good management of *places of cultural significance*.
- 2.4 *Places of cultural significance* should be safeguarded and not put at risk or left in a vulnerable state.

Article 3 Cautious Approach

- 3.1 *Conservation* is based on a respect for the existing *fabric, use, associations* and *meanings*. It requires a cautious approach of changing as much as necessary but as little as possible.
- 3.2 Changes to a *place* should not distort the physical or other evidence it provides, nor be based on conjecture.

Article 4 Knowledge, Skills and Techniques

- 4.1 *Conservation* should make use of all the knowledge, skills and disciplines which can contribute to the study and care of the *place*.
- 4.2 Traditional techniques and materials are preferred for the *conservation* of significant *fabric*. In some circumstances modern techniques and materials which offer substantial conservation benefits may be appropriate.

Article 7 Use

- 7.1 Where the *use* of a *place* is of *cultural significance* it should be retained.
- 7.2 A *place* should have a *compatible use*.

Article 8 Setting

Conservation requires the retention of an appropriate visual *setting* and other relationships that contribute to the *cultural significance* of the *place*.

New construction, demolition, intrusions or other changes which would adversely affect the setting or relationships are not appropriate.

Article 15 Change

- 15.1 Change may be necessary to retain *cultural significance*, but is undesirable where it reduces cultural significance. The amount of change to a *place* should be guided by the *cultural significance* of the place and its appropriate *interpretation*.
- 15.2 Changes which reduce *cultural significance* should be reversible, and be reversed when circumstances permit.
- 15.3 Demolition of significant *fabric* of a *place* is generally not acceptable. However, in some cases minor demolition may be appropriate as part of *conservation*. Removed significant fabric should be reinstated when circumstances permit.
- 15.4 The contributions of all aspects of *cultural significance* of a *place* should be respected. If a place includes *fabric, uses, associations* or *meanings* of different periods, or different aspects of cultural significance, emphasising or interpreting one period or aspect at the expense of another can only be justified when what is left out, removed or diminished is of slight cultural significance and that which is emphasised or interpreted is of much greater cultural significance.

Article 16 Maintenance

Maintenance is fundamental to *conservation* and should be undertaken where *fabric* is of *cultural significance* and its *maintenance* is necessary to retain that *cultural significance*.

Article 20 Reconstruction

- 20.1 *Reconstruction* is appropriate only where a *place* is incomplete through damage or alteration, and only where there is sufficient evidence to reproduce an earlier state of the *fabric*. In rare cases, reconstruction may also be appropriate as part of a *use* or practice that retains the *cultural significance* of the place.
- 20.2 *Reconstruction* should be identifiable on close inspection or through additional *interpretation*.

Article 21 Adaptation

- 21.1 *Adaptation* is acceptable only where the adaptation has minimal impact on the *cultural significance* of the *place*.
- 21.2 *Adaptation* should involve minimal change to significant fabric, achieved only after considering alternatives.

Article 22 New Work

- 22.1 New work such as additions to the *place* may be acceptable where it does not distort or obscure the *cultural significance* of the *place*, or detract from its *interpretation* and appreciation.
- 22.2 New work should be readily identifiable as such.

Article 25 Interpretation

The *cultural significance* of many *places* is not readily apparent, and should be explained by *interpretation*. Interpretation should enhance understanding and enjoyment, and be culturally appropriate.

Article 27 Managing Change

- 27.1 The impact of proposed changes on the *cultural significance* of a *place* should be analysed with reference to the statement of significance and the policy for managing the *place*. It may be necessary to modify proposed changes following analysis to better retain cultural significance.
- 27.2 Existing *fabric*, *use*, *associations* and *meanings* should be adequately recorded before any changes are made to the *place*.

Article 28 Disturbance of Fabric

- 28.1 Disturbance of significant *fabric* for study, or to obtain evidence, should be minimised. Study of a *place* by any disturbance of the *fabric*, including archaeological excavation, should only be undertaken to provide data essential for decisions on the *conservation* of the *place*, or to obtain important evidence about to be lost or made inaccessible.

Article 30 Direction, Supervision and Implementation

Competent direction and supervision should be maintained at all stages, and any changes should be implemented by people with appropriate knowledge and skills.

Article 31 Documenting Evidence and Decisions

A log of new evidence and additional decisions should be kept.

Article 32 Records

- 32.1 The records associated with the *conservation* of a *place* should be placed in a permanent archive and made publicly available, subject to requirements of security and privacy, and where this is culturally appropriate.
- 32.2 Records about the *history* of a *place* should be protected and made publicly available, subject to requirements of security and privacy, and where this is culturally appropriate.

Article 33 Removed Fabric

Significant *fabric* which has been removed from a *place* including contents, fixtures and objects, should be catalogued, and protected in accordance with its *cultural significance*.

Where possible and culturally appropriate, removed significant fabric including contents, fixtures and objects, should be kept at the place.

6.7 Statutory Requirements

6.7.1 Sydney Harbour Foreshore Authority Act

The site is subject to the controls contained within the *Sydney Harbour Foreshore Authority Act 1998* (SHFA Act).

The principal functions of SHFA, under the SHFA Act, are as follows:

- (a) *to protect and enhance the natural and cultural heritage of the foreshore area;*
- (b) *to promote, co-ordinate, manage, undertake and secure the orderly and economic development and use of the foreshore area, including the provision of infrastructure;*
- (c) *to promote, co-ordinate, organise, manage, undertake, secure, provide and conduct cultural, educational, commercial, tourist, recreational, entertainment and transport activities and facilities.*

SHFA's Statutory Planning Scheme identifies Campbell's Stores as a 'historic' building complex which may not be demolished.

6.7.2 Planning Instruments

State Environmental Planning Policy No. 56 (SEPP 56) establishes the Minister for Planning as the authority for development consent, including works to heritage buildings. The NSW Department of Infrastructure Planning and Natural Resources is the consent authority of Campbell's Stores, although the Minister has delegated authority to SHFA for minor works.

Draft Sydney Regional Environmental Plan No. 32 covers The Rocks and would apply to Campbell's Stores.

6.7.3 NSW Heritage Act 1977

Campbell's Stores are listed under the Heritage Act as a heritage item on the SHR and on the Section 170 Register of the SHFA. Any proposal for alteration to the building would need the

approval of the Heritage Office, although SHFA has delegated authority to approve minor works to heritage items on the SHR.

6.7.4 Building Code of Australia (BCA)

Work undertaken by the tenants is subject to the provisions of the *Local Government Act* and work undertaken by the Authority is subject to the health and safety standards contained in the *Building Code of Australia* and the *Occupational Health and Safety Act 2000 (NSW)*.

The *Building Code of Australia* establishes requirements for structural integrity, access to buildings (including provisions for people with disabilities) egress and fire resistance, essential buildings services, health and amenity facilities.

In early 2003, SHFA engaged BCA consultants Trevor Howse and Associates to prepare a series of BCA Building and Fire Safety Audit reports on the various tenancy spaces within Campbell's Stores. The reports identify prescriptive areas of non-compliance (including accessibility) and provide recommendations through which it is considered that the circumstances may be upgraded, as required, to achieve compliance with the intent of the BCA. The reports note that:

Given the limitations associated with the existing status of the building, it has been recognised that upgrade to comply with the prescriptive provisions may not be practical in parts, such that performance based recommendations have been included.

Any specific proposals that result from these BCA reports should aim to minimise impacts on the significant heritage fabric of Campbell's Stores and the impacts of new works on the spaces and aesthetic values of Campbell's Stores as a whole.

6.8 Sydney Harbour Foreshore Authority Requirements

6.8.1 SHFA Objectives

Campbell's Stores is seen by SHFA as an iconic building that represents the core authenticity of The Rocks area. SHFA recognises the heritage significance of the property, as evidenced by its resourcing for this CMP review and in the program conservation and maintenance works proposed.

SHFA recognises that in recent years the physical condition and presentation of Campbell's Stores has 'slipped' relative to other tourist attractions such as the refurbished Overseas Passenger Terminal. Key priorities in the brief for this CMP were the conservation of building fabric, an improved external presentation of the building, in particular the external public forecourt area, and in the interpretation of the building's significance.

In addition to recognising the buildings' heritage value, SHFA is also aware that they are an important tourist venue and a popular place for Sydneysiders to visit. The property has also become an important source of income for SHFA which, in turn, helps to finance the conservation of heritage items throughout The Rocks.

Over the last ten years, SHFA has recognised the value, both in economic and social terms, of conserving The Rocks in an appropriate manner and now seeks to ensure that all its heritage buildings are managed in the manner which enhances their value.

6.8.2 The Rocks Heritage Management Plan

The Rocks Heritage Management Plan, revised during 2010, provides a firm understanding of the heritage value of The Rocks and to guide SHFA in the conservation and management of the place. As such The Rocks Heritage Management Plan provides an overarching document within which this CMP should sit and respond directly to. The most relevant conservation policies contained in The Rocks Heritage Management Plan in relation to Campbell's Stores are:

Retention of significance: the importance of authenticity, the primacy of heritage values where values conflict and the importance of compatible uses;

Conservation of tangible and intangible values: the importance of layers of meaning, the 'fine grain', patina, movable heritage and historic uses and associations;

Consultation: practical and effective mechanisms to actively inform and involve community (including tenants) in heritage management and interpretation; and

Interpretation: the importance of conveying interpretative messages both on and off sites.

6.9 Issues and Opportunities

The need for a significant conservation works program continues to be a significant issue facing the building. An opportunity exists to undertake fundamental rectification work and external works to ground levels to alleviate rising damp issues.

Another key issue is the need for ongoing change as an important part of restaurant use while maintaining the key heritage fabric and spaces in the building. The forecourt outdoor dining area is a key example where the opportunity exists to improve the visibility and appearance of Campbell's Stores while retaining this essential outdoor use further away from the building.

The lack of interpretation of the history and significance of Campbell's Stores is an issue that can be addressed by providing interpretation that places Campbell's Stores within a historical and physical setting, makes more of its actual use and operations (including industrial heritage) and emphasises authenticity.

7.0 Conservation Policies

7.1 Policy Discussion

Conservation can be regarded as the management of change. It seeks to safeguard that which is significant to an item within a process of change and development. It is essential to establish criteria, policies and recommendations of the conservation and ongoing use of the item, to ensure best practice heritage management. Within this framework owners and managers of the item will be able to formulate suitable proposals, and planning authorities will be able to assess those proposals against the site specific criteria.

The conservation policies are intended to assist in the ongoing use, maintenance and conservation of the site. These policies are intended to manage change, rather than prohibit it. Each policy is supported by a series of guidelines which will ensure that future decisions are made in an informed manner, ensuring the significance of the place.

There are a number of key issues for the long-term conservation, use and management of Campbell's Stores. The need to undertake catch-up conservation works and to provide for an ongoing maintenance program is the core policy in relation to retaining the integrity of Campbell's Stores.

The ongoing use of the building for restaurants since the late 1970s has provided public access to, and appreciation of, the building. This use should be allowed to continue to provide for this important function. However, the commercial kitchen and restaurant fitout requirements have had adverse heritage impacts and this use needs to be managed carefully in future to avoid additional adverse heritage impacts. In addition, changes to the building's envelope have incrementally eroded the integrity of the building. The answer is to provide clarity of heritage objectives to retain, and where possible recover, heritage fabric and spaces. The answer is also to clarify the role of tenants in the ongoing protective care of the building and to provide for sensitive response to statutory controls.

The external setting of Campbell's Stores is vitally important to the retention of its heritage significance. While the external forecourt seating area (including the northern glazed enclosure that is part of the Italian Village lease) should be acknowledged as an essential operational requirement of the restaurant usage, the structure is intrusive and a number of small ad-hoc changes have further impacted the building's unattractive appearance.

As noted in the 1996 and 2004 Campbell's Stores Conservation Plans and in The Rocks Heritage Management Plan 2010, the non-authentic ship's mast structure in front of the Waterfront Restaurant confuses an understanding of the authentic Campbell's Stores. The mast should be removed during the refurbishment of the forecourt area. This report recommends a new canopy structure be designed by SHFA to replace the existing. The design of the new canopy should allow for a better appreciation of the facade of Campbell's Stores. Its structure could also respond to the imagery of maritime activity and machinery that once surrounded Campbell's Stores, in particular the ship and shore jib crane structures, but without introducing replicas or fake reconstructions.

7.2 Overall Guiding Principles

Campbell's Stores is an icon of the mid-nineteenth-century maritime development of Sydney whose authenticity of external form and fabric is a key element in its tangible significance. The ongoing

conservation and adaptive re-use of the building should primarily aim to reveal authenticity by removing intrusive elements, recover lost authenticity by reinstating removed elements, and when appropriate, reconstructing damaged or lost significant elements. In association with this primary aim, it is important not to undertake actions that would further obscure this authenticity and to provide for active interpretation of those aspects that contribute to its authenticity.

Campbell's Stores is recognised as a place of outstanding cultural significance and should be managed in accordance with the principles and guidelines established by the Burra Charter. http://australia.icomos.org/wp-content/uploads/BURRA_CHARTER.pdf

The original and early form and fabric of Campbell's Stores should be retained, conserved and interpreted. Original elements that have been removed and placed in storage should be restored, and original elements that are missing or have been altered may be reconstructed based on historical evidence. A complete inventory of items belonging to Campbell's Stores that have been removed and placed in storage should be compiled by SHFA, and a strategy developed for their future interpretation and possible reinstatement.

Adaptation should be designed to minimise impacts on significant spaces, fabric and functional relationships and no adaptive re-use should be permitted that will have major adverse impact on the heritage significance of Campbell's Stores or on surrounding heritage items. Adaptive re-use works should respect, and where appropriate reflect, the architectural style of the original interiors.

Later alterations and additions of little or no significance, or which are intrusive, may be removed, particularly where this would recover significance and/or restore the earlier appearance of the building and/or historic spatial relationships or setting.

The recovery or revealing of significance by the reconstruction of spaces, the removal of fabric of Little or Intrusive significance, the reconstruction of lost elements of fabric, and the reinstatement of fabric that has been removed and stored, should be an aim of conservation.

The above guidelines applies to the entire site which includes the eastern forecourt area, the service tunnel area to the west and the open space area to the north and northeast, including the site of the glass pavilion structure.

7.3 Current and Potential Future Use

Background

Campbell's Stores was originally used as commercial warehousing during the mid-nineteenth century. It provides evidence of the changing nature of activities around Sydney Cove, from the importance of this area as a hub of commerce and international shipping transport until the late nineteenth century, and the recent role as a cultural focus of international tourism. In the 1970s, the commercial warehouse was converted to restaurants; Campbell's Stores is currently home to the restaurants Waterfront, Wolfies, Imperial Peking, and Italian Village.

Policy 1

The future use of Campbell's Stores should be consistent with its outstanding cultural significance, should not impact on significant fabric and spaces, and should provide for public access to the building.

Guidelines

- SHFA shall ensure that any new uses selected for the existing building shall adopt the principle of 'loose fit', where the new use is adjusted as necessary to work within the available spatial and architectural configuration.
- Adaptation of the building's interior should ensure that the original fabric or significant architectural and spatial features are retained and interpreted as far as possible.
- The detailed requirements of the new uses should not generate undue changes to the existing fabric that cannot be reversed in the long term, or which do not respect and work within the existing architectural framework.
- Subdivision of internal spaces, where appropriate, should be undertaken in a secondary manner, using such items as partitions that can eventually be removed and which do not impact on the existing finishes or details.
- The introduction of new services and associated fittings as part of approved re-use programs should be carried out with the minimum of disruption to the fabric and spaces.
- Uses which require an unacceptable degree of intervention for upgrading to ordinance compliance should be avoided.
- Future uses should be compatible with the nature and significance of the building components and should enable Campbell's Store to remain a vital and important component of The Rocks precinct.
- The use should allow for the general conservation policies outlined above to be realised, and in particular, should not require any further loss or concealment of significant fabric.
- Public access to the interior of Campbell's Stores should be retained and encouraged.
- The use should facilitate the interpretation of the place in a manner consistent with its cultural significance.

7.4 Asset Management

7.4.1 Sydney Harbour Foreshore Authority (SHFA) Responsibilities

Background

Campbell's Store, as part of The Rocks, is owned by the Crown, vested in SHFA. SHFA has overall responsibility for the management of the place. Additional responsibilities derived from the NSW Total Asset Management (TAM) guidelines are also recognised.

Policy 2

Sydney Harbour Foreshore Authority shall retain management of Campbell's Store as a single entity within the overall context of The Rocks precinct.

Guidelines

- SHFA should implement a management structure that integrates development and conservation work with the overall management of Campbell's Stores.
- SHFA should make this CMP a publicly accessible document.
- SHFA should ensure that this document and any subsequent recording and investigations are achieved in such a manner that provides an accurate record of the changes to the significant fabric of Campbell's Stores.
- The intention, aims and policies of this CMP shall be disseminated through, and implemented by, relevant key staff of SHFA.
- SHFA shall adequately assess the impact of proposed modifications to significant fabric, prior to the granting of owner's consent.
- All recording, when required, should be undertaken and archived in accordance with the recording guidelines prepared by the Heritage Branch of the NSW Department of Planning.
- A program of regular monitoring should be established, covering both the physical changes within Campbell's Stores and visitor experience issues, and be incorporated, where relevant, into management decisions.
- SHFA should investigate mechanisms which give it better control of the design, documentation and implementation of work to heritage buildings. One option, which should be considered, is that all such activities be undertaken directly by SHFA and paid for by the tenant.

7.4.2 Total Asset Management Plans and CMPs

Background

This CMP will be one of a number of documents used by the asset owners, managers and tenants for management and maintenance of the Campbell's Store and needs to be fully compatible with other relevant documents addressing Campbell's Stores, including any current maintenance plans and Total Asset Management (TAM) plans. All the relevant asset management documents were included in the preparation of this CMP.

Policy 3

This Conservation Management Plan should be used to provide feedback to its assessment management systems and plans.

Guidelines

- SHFA should include findings of this CMP into its asset management systems and plans, particularly TAM plans and maintenance plans for Campbell's Stores.

7.4.3 Adoption, Endorsement and Review of the CMP¹

Background

The CMP for Campbell's Store proposes a framework for the management of heritage issues in relation to upgrading and adaptive re-use projects and, into the long term, to ensure that the identified significance of the complex is retained and maintained.

Circumstances will change over the years as various recommendations are implemented and new use requirements emerge. Conservation policies need to progressively respond to changing situations if they are to remain relevant.

Policy 4

This Conservation Management Plan should be endorsed by the NSW Heritage Council and adopted by SHFA as one of the basis for the future management of the site. Conservation policies should be reviewed every five to ten years.

Guidelines

- Reviews of the CMP should be based on the Burra Charter and other guidelines by the Heritage Branch of the NSW Department of Planning.
- Reviews should also take into account any other relevant legislation, planning frameworks, appropriate literature and widely recognised conservation practices and procedures.
- Reviews should be undertaken by experienced conservation practitioners in conjunction with relevant ownership and management representatives.
- Irrespective of the requirement to review the document every five years, the CMP should remain as a valid basis for ongoing heritage management until such reviews are completed.

7.4.4 Approvals' Process and Landowners Consent

Background

To prevent loss of cultural significance through gradual change, a mechanism needs to be established for controlling any modifications by tenants to the significant fabric.

Forms for development applications, Section 60 applications and standard exemption applications are available from SHFA.

Policy 5

Prior to the lodgement of applications and the commencement of works the proponent should liaise with the Foreshore Authority's heritage architect and obtain the consent of Sydney Harbour Foreshore Authority as landowner.

Guidelines

- As SHFA is the owner of land and buildings in The Rocks, the consent of the Authority is required prior to lodging an application for works. This part of the process is separate from approving the works, and the landowners consent to lodge an application does not present the approval to actually undertake the works.

- Prior to granting owner's consent to a proposal, SHFA should, in its capacity as land owner and manager, be satisfied that there are no adverse heritage impacts associated with the proposal.
- A Heritage Impact Assessment and adequate documentation of the proposed works should be required for all proposals that involve intervention of significant fabric or spaces, to assist in the assessment of owner's consent applications.
- Assessment and adequate documentation of the proposed works should be required to assist in the assessment of owner's consent applications.

7.4.5 Tenancy

Background

To prevent the loss of cultural significance through incremental change, a mechanism for controlling any modifications undertaken by tenants to the significant fabric needs to be established.

Policy 6

All tenants of the building should be made aware of the cultural significance of the item. Tenancies should only be selected and approved by SHFA on the basis that the proposed or future uses are compatible with the significance of the item and the sensitive fabric and spaces, and that the required changes to the item can be installed and removed without impact.

Guidelines

- This CMP should be provided to the building's tenants.
- Tenants shall adopt the guidelines of this CMP in their planning and design. A full compliance with the requirements of this CMP is required for SHFA consent to the proposed development.
- Tenants should be aware of SHFA policies.
- The impact of proposed modifications to significant fabric should be adequately assessed, prior to the granting of land owner's consent.

7.5 Management of Significance

7.5.1 Retention of Significance

Background

Campbell's Stores is a superb example of a mid-nineteenth-century warehouse building, now rare in Sydney. It is the only warehouse of its type remaining on the foreshores of Sydney Cove. It is a good example of Georgian vernacular warehousing and its evolution evidences the rise of commerce and international shipping transport until the late nineteenth century.

Policy 7

The statement of significance should be adopted as the basis for heritage management. All decisions should consider and seek to retain the values identified in the statement of significance.

Guidelines

- The significant aspects of the architectural character of Campbell's Store should be retained and conserved.
- The conservation, adaptation and maintenance of Campbell's Store should be approached with the general principle of changing 'as much as necessary but as little as possible'.
- Structural alteration to the building components, which impact on the integrity or significance of Campbell's Stores should not occur.
- Internal alterations and renovations are acceptable within the context of compatible use. However they should not impact on the significance of the internal original fabric and spatial qualities of the building, or the external facade.
- No externally mounted air-conditioning, ventilation equipment, water heaters or service components should be visible from the street or impact negatively on the facade.
- Evidence of the progressive evolution of Campbell's Store should be respected and retained.

7.5.2 Conservation of Significant Fabric

Background

One of the key objectives of contemporary conservation practice is that as much as possible of the significant original fabric of the building or place should be retained and conserved in order to preserve the essential integrity of the heritage resource for future generations. While any conservation activity will affect the building in some way, the aim, consistent with responsible re-use or management aims, should be to minimise the work necessary. In this way the authenticity of the item will be retained as far as possible within a process of evolutionary changes and good maintenance practice.

Article 3 of the Burra Charter indicates that conservation is based on a respect for the existing fabric of a place and should therefore involve the least possible physical intervention in order not to distort the evidence provided by the fabric.

Policy 8

Significant fabric should be conserved using conservation processes appropriate to the assessed level of significance. Restoration and reconstruction should aim to recover or reveal significance.

Guidelines

Conservation Processes appropriate to the Assessed Level of Significance identified in Section 5.0

- The primary conservation process should be that of preservation, restoration and reconstruction. As stated in the Burra Charter, preservation means maintaining the fabric of a place in its existing state and retarding deterioration. Restoration means returning the existing fabric of a place to a known earlier state by removing accretions or by reassembling existing components without the introduction of new material. Reconstruction means returning a place to a known earlier state and is distinguished from restoration by the introduction of new material into the fabric.

- Fabric identified as being of Exceptional significance (fabric up to 1885); High significance (up to 1915), and Moderate significance (1970s conservation works) should be retained and conserved.
- No conservation or maintenance work should alter or negatively impact on the elements of the external facades or internal fabric/space that have been identified as elements of high or exceptional level of significance.
- SHFA may, as part of the tenancy contracts, require tenants to execute the required works and provide auditable trace of the completion of required works.

Existing Building Fabric

- The existing building fabric of Campbell's Store both internally and externally, is highly significant and in reasonable condition given the building's age and original and current function. All original and early sandstone and brickwork should be retained and appropriately conserved. No new openings or alterations should be made to the walls. Where possible the openings in the sandstone walls created in the 1970s should be blocked up to allow interpretation of the way in which the Stores originally functioned.
- Aged building fabric, which is not likely to be causing ongoing deterioration should not be repaired for visual reasons if by doing so the patina of age and ability to successfully interpret various stages of use is degraded.
- Internal sandstone walls should be retained and appropriately conserved. Damaged stonework should be appropriately repaired or replaced if there is no other alternative.
- All original or early timber structural members, including posts, beams, trusses, portals and floorboards, are highly significant and should be retained and appropriately conserved. Significant timber structural elements should not be concealed with new linings or finishes.
- Original joinery items removed from the building and placed in storage should be reinstated as soon as the opportunity arises. Missing joinery items, such as the original loading doors, could be considered for reconstruction at an appropriate time. Window joinery should be reconstructed where the original joinery profiles are known.
- Existing accretions attached to the external fabric of the building such as fixing bolts, cable stays, canopies, aerials and masts should be removed as soon as possible. No further accretions should be permitted, particularly where these are fixed into the fabric of the existing building. Damaged fabric should be repaired with appropriate materials immediately upon removal of existing accretions.
- The original spatial configuration of the internal bays, in particular their unobstructed volumes, should be conserved wherever possible.
- Original or early openings within the internal sandstone walls should be retained to allow interpretation of the way in which these stores functioned.
- All original or early bars to window openings should be retained. The fabricated timber bars located on Level 2 and Level 3 goods doors should be removed.

- The evidence of early staircases should be retained.
- Where repairs or alterations are required, new material should closely match original or adjacent materials. However, evidence of change should be identifiable on close inspection.

Reconstruction of Missing or Altered Fabric

- Restoration or reconstruction work should aim to recover the significance of the place.
- Damaged or defective significant fabric may be reconstructed. The fabric of missing elements may be reconstructed or, if no evidence of the original form remains, may be replaced with new fabric in an appropriate design. The latter approach would be appropriate where this would assist the interpretation of the place.
- Openings in the sandstone walls created during the 1970s and later may be blocked up in appropriate materials when the opportunity arises.
- Alterations previously made to significant fabric to accommodate services, for example, the removal of sections of brickwork at the apex of some of the gables, should be made good through reconstruction when the opportunity arises.
- Non-original floor voids should be enclosed in appropriate materials when the opportunity arises in the future. No new openings should be made in the floors other than where essential to satisfy fire safety requirements.

Roofing

- The roof joinery is generally in poor condition and requires detailed inspection, including by a structural engineer and reconstruction schedules prepared. A similar situation exists for the loading door aprons generally.
- The roof configuration of Bays 1–10 should be retained and should continue to be clad in slate. Inspect the slate roofing regularly. Retain or re-use slates where possible and replace when necessary.
- The roof form of Bay 11 could either be retained in its current configuration or reconstructed to its original form if this can be determined.
- The sandstone party wall between bays that projects above the roof box gutter line should be capped in lead to ensure that further deterioration and ingress of water is prevented.
- Repair all barge boards, rainwater heads and rainwater goods.

Policy 9

Fabric identified as Intrusive should be removed where this would recover significance, or replaced in more appropriate materials and/or finishes.

Guidelines

- The brick southern wall constructed in the 1970s is assessed as being intrusive. There is an opportunity to interpret the missing bay.

- The painted advertising signs on this wall are excessively large and, due to their location adjacent to the eastern facade of the building, intrude into the setting of Campbell's Stores. These signs should either be repositioned towards the western end of the wall or, preferably, removed altogether from this location.
- Aluminium windows should be replaced with appropriately-designed timber-framed windows when the opportunity arises.

7.5.3 Appropriate Conservation Skills and Experience

Background

The Burra Charter encourages the use of skilled and appropriate professional direction and supervision from a range of disciplines for conservation activities.

The skills and experience required and creative approaches taken in the context of a conservation project are quite different to those applied to the design and construction of new buildings.

Policy 10

Appropriate conservation skills and experience should be employed for documentation and supervision within project teams to deal with any programs of conservation and upgrading of the building components of Campbell's Store.

Guidelines

- Appropriate professional skills and experience assembled to work on the detailed conservation of the building could include, as appropriate, researchers, archaeologists, conservation architects, structural engineers, building code compliance advisers, materials conservation specialists and cost planners.
- Building contractors, project managers and trades personnel who are experienced with working on historic buildings should be selected to work on the project.

7.5.4 Alterations or Additions

Background

Campbell's Stores is a superb example of a mid-nineteenth century warehouse building, now rare in Sydney. It is the only warehouse of its type remaining on the foreshores of Sydney Cove. It is memorable and an easily recognisable landmark in The Rocks, visible from a wide area of Sydney Harbour.

Policy 11

External alterations or additions should be discouraged; however, if required to meet approved interpretation, re-use or cultural tourism requirements, these should be of a minor nature, and subservient to the primary architectural features and composition of the existing structure. New works should not obscure significance.

Guidelines

- No new openings or alterations should be made to the walls and when the opportunity arises existing, not original doorways, particularly between the Stores, should be infilled to reinstate the original spatial qualities of the Stores.
- Future adaptation should not require the subdivision of the spaces and would preferably allow the removal of some of the current partitions and elements that intrude within these spaces.
- New work which would conceal significant fabric should not be undertaken, but where this is unavoidable it should be constructed in a reversible manner.
- New work should complement the original fabric, with appropriate materials and finished, but be distinguishable as new additions.
- New work should be identifiable on close inspection as such.
- Later masonry such as the Bay 11 north and east facades could be altered provided this does not destroy existing historical evidence.
- Campbell's Stores should be painted in an appropriate period colour scheme. Only those surfaces that were previously painted or intended for painting should be painted. Previously unpainted surfaces should not be painted.

Policy 12

The spatial qualities of Campbell's Store contribute to its significance and interpretation and therefore should be conserved, as part of the ongoing use, ongoing management and any future development strategy.

Guidelines

- Alterations to the original spatial qualities and configuration of the Campbell's Stores should be removed, particularly where this would recover significance and/or restore the earlier appearance of the building and/or historic spatial relationships or setting.
- The shading structure over the outdoor seating area for the restaurants currently provides a dominant horizontal element that detracts from the appreciation of the vertical bays of the Stores. This should be removed and/or replaced with a more sympathetically designed shading structure that is detached from the façade of the Campbell's Stores and improves the views to the building from significant vantage points.

7.5.5 Setting and Heritage Curtilage

Background

The existing setting of Campbell's Stores does little to evoke the strong historic association between the building and the past maritime activity of this area. The existing awning over the outdoor seating area is a dominant horizontal element which obscures views of the vertical bays of Campbell's Stores and is causing physical damage by directing rainwater onto the façade of the building.

Policy 13

An appropriate physical and visual setting should be maintained for Campbell's Stores by allowing no development within the setting that would adversely impact on the place or on views to and from the place.

Guidelines

- The proposed heritage curtilage recommended in Section 4.0 should be endorsed by the NSW Heritage Council and included in the heritage inventory on the Heritage Branch database. The current listing does not define a heritage curtilage.
- The need to retain a suitable setting for Campbell's Stores should be considered when assessing any proposal for new development or alterations within or around the site. No development that would detract from the maritime setting of the property or obscure key views to or from Campbell's Stores should be permitted.
- Landscaping should be carefully controlled to ensure that it is consistent with the historic setting of the place.
- New development within the site or vicinity of Campbell's Stores should be carefully controlled so as not to adversely affect the significance of the place.
- The area of eastern forecourt that is used for external restaurant seating does not have particular heritage significance as its shape has been created post-1970s. The level of the eastern forecourt is the original ground level and this level should ideally be extended across the forecourt to the harbour.
- The continued use of the external forecourt in front of Campbell's Stores as an outdoor eating area is acceptable, provided that this use and its associated fabric do not impact on the fabric, setting and appreciation of Campbell's Stores.
- The existing canopy, the non-authentic ship's mast and the glazed pavilion structure in the northern forecourt should be removed. The existing canopy could be replaced with a new canopy that follows the following principles (refer to Figure 7.1):
 - the position of the canopy should provide a generous gap between the canopy and building and should provide sun and rain protection from above, but should have no fixed glass or plastic pull-down sides;
 - the canopy should not be attached to or touch the facade of Campbell's Stores and should be ideally set further away than the existing canopy to allow for access to and appreciation of the façade. Covers over entries should not be attached to the building;
 - the canopy should be separated into segments to allow views to the building elevation, to allow for a better appreciation of the facade of Campbell's Stores and should be visually permeable, uncluttered and transparent;
 - the canopy should be primarily low and mono-pitched and horizontal in its form with some vertical support structure elements, located in response to the building rhythm;
 - the canopy and its structure should be of a consistent design across the tenancies;

- the structure of the canopy should also respond to the imagery of maritime activity and machinery that once surrounded Campbell's Stores, in particular the ship and shore jib crane structures, but without being replicas or fake reconstructions (ships' masts are historically inappropriate in this location); and
- while the canopy and its structure should be recessive in colour and materials, the use of carefully controlled elements of colour on the canopy structure such as small banners and flags is appropriate within this former maritime setting.

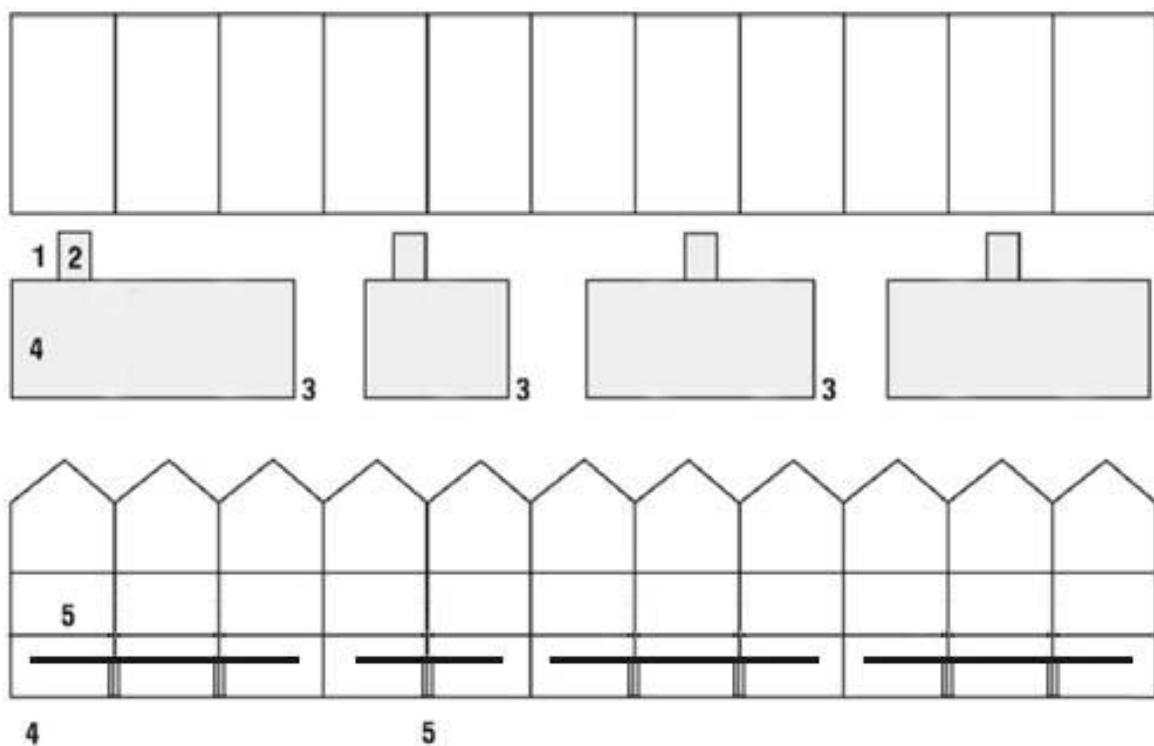


Figure 7.1A Campbell's Stores external forecourt shade canopy design principles.

1. Provide generous gap between canopy & building.
2. Small covers over entries should not be attached to the building.
3. Break canopy into segments to allow views to full building elevations and to differentiate between tenancies.
4. Shade canopies should be low and mono-pitch without facias or valances.
5. Canopy support structure to respond in placement to building rhythm.



Figure 7.1B Campbell's Stores external forecourt shade canopy design principles.

7.5.6 Ongoing Maintenance and Repair

Background

The nature of any building is that its fabric will deteriorate due to the effects of age, maintenance, weather, vegetation incursion and use. To ensure the ongoing conservation of significant building fabric, a regular maintenance schedule should be implemented, which provides for regular inspection and for remedial action to be taken where necessary.

Policy 14

As a necessary minimum, the ongoing maintenance should include works that will ensure that each element retains its current level of significance and not allow the loss of significance due to the deterioration of fabric.

Guidelines

- A conservation program, including a maintenance plan, should be prepared by the Authority to ensure that the property is appropriately conserved and maintained. The conservation program should include a 'catch-up' works program and an ongoing maintenance program.
- High priority in a works program should be given to the insertion of a damp-proof course throughout the property, the repair of damaged stonework (in order to prevent further damage occurring), and the progressive removal of the salt from the sandstone walls by means of sacrificial render once the damp-course has been inserted.

- The building fabric and services should be subject to continuing care and maintenance by SHFA.
- In addition to regular maintenance activities, prompt preventative action and repair should be taken as necessary.
- Prevention of continuing deterioration should take priority over widespread repair or reconstruction.
- Inspection and maintenance works should only be conducted by those with professional knowledge and demonstrated experience with buildings and materials of this nature.
- No maintenance work or repairs should negatively impact on significant fabric.
- New internal floor coverings are permissible, but should have minimal impact on the floor structure.

7.5.7 Retention of Fixtures and Fittings

Background

The industrial goods handling and hoisting equipment are relics of the site's historic use that contribute to the special character of the building components and Campbell's Store as a whole.

Policy 15

Features and fixtures associated with the site's historic use as a commercial warehouse should be retained and preserved. Those that have been removed should be reinstated or reconstructed and interpreted.

Guidelines

- The retention and conservation of these features and fixtures should be considered as part of the design work for future adaptive re-use programs.
- An inventory should be prepared of original or early fixtures and fittings that have been removed from the building and is currently in storage.
- Significant items removed from the building should be reinstated in their known locations when the opportunity arises in the future. This includes the Tangy gas engine which was removed and placed in storage by SHFA c1992. Documentary evidence shows that this sat on the floor of Level 3 in Bay 6. Consideration should be given to its reinstatement in this location at an appropriate time in the future.
- Original or early fixtures or fittings previously removed from the building and that is currently in storage should be restored to its original position when the opportunity arises. Items that should be located, inventoried and considered for reinstatement are:
 - the timber loading doors;
 - the overhead platforms for the two hydraulic rams, including their pulley wheels;

- control rods and cables for the hydraulic rams alongside the loading doors at each level;
- control cables and drive belts for the electric and gas engine-driven winches; and
- lifting cables, pulley blocks and lifting hooks (interpretation only).

7.5.8 Interpretation

Background

Interpretation of historic places essentially reveals long-term connections and cohesions which underpin our cultural identity. To ‘interpret’ a historic place, in its geographic and physical setting, is to bring its history to life to increase the public’s understanding, and, through this extended understanding, to give them an enhanced perception of the significance of the place.

Due to its history, significance and setting in The Rocks area, Campbell’s Store is capable of being interpreted for promotional and educational purposes.

Policy 16

The heritage significance of Campbell’s Stores should be interpreted on site by appropriate methods.

Guidelines

- One of the primary components of the conservation management of the Campbell’s Store should be to make the values of its cultural significance physically, intellectually and/or emotively accessible to the public.
- The ongoing management of Campbell’s Store should include the development and implementation of an interpretation strategy, which reveals the cultural significance of the place.
- In making significance accessible, SHFA should take into account the policies and guidelines contained within the *ICOMOS International Cultural Tourism Charter*.
- Interpretation programs should provide equitable physical, spiritual and intellectual access to the cultural significance of the Campbell’s Store.
- Interpretation at Campbell’s Store should take into account all periods of development and occupation in the context of the history of The Rocks area under the management of SHFA, and be presented in an accurate and insightful manner.
- Interpretation should be developed and implemented in consultation with the building’s tenants.

7.6 New Work Policies

7.6.1 Integration of New Work

Background

Because of the significance of Campbell's Stores, scope for new development is limited. Within the general principles outlined in this CMP, some relatively minor internal alterations and additions could occur. These should generally be confined to less significant areas and should only affect limited amounts of original fabric. New work should be carefully designed and integrated with the original character of the significant components.

Policy 17

No external alterations or additions should occur to the Campbell's Stores building including minor additions for services.

Guidelines

- Any new development should be carried out within the existing building envelopes, unless research establishes traces of a demolished, but more extensive footprint of the original Campbell's Stores.
- Where a function is being re-established, new fittings and fixtures may be adopted to complement the existing architectural character.

Policy 18

Any new development must respect the cultural significance of the property and its setting and not destroy or obscure historical associations. The introduction of new fabric should be undertaken in such a manner that it does not result in a lessening of the cultural significance of the place. New work should be identifiable as such and should, wherever possible, be capable of being removed without damage to significant fabric or spaces.

Guidelines

- The existing, intrusive temporary structure at the northern end of Campbell's Stores should be removed and replaced with a new structure that responds to the qualities of the existing building, including materials, rhythms and scale.
- When a new function is being introduced, a new architectural vocabulary of details and materials may be adopted to complement the existing architectural character.
- Period detailing should be used to reconstruct elements for which there is clear evidence of the original detail, either remaining in the fabric or in documents.
- Where there is no evidence of the original detail, it is not necessary or appropriate to invent a period detail unless the original character of the space is to be recreated and the new element is required to blend in.
- Careful detailing will ensure minimal damage to the significant fabric and will allow for reversibility.

- The glass pavilion at the northern end of the site is intrusive and should be removed.
- Future fit-out work should be undertaken in a manner consistent with the cultural significance of Campbell's Stores.
- Future fit-outs should aim to reveal as much as possible of the original and early character of the stores, rather than imposing a new character.
- Fit-out elements should be movable, rather than fixed, where possible.
- Fixtures should preferably not extend from floor to ceiling, in order to allow for interpretation and appreciation of original spatial volumes.
- Existing fit-out elements identified as being intrusive should be removed when the opportunity arises. This includes partitions, bars, kitchen fit-outs etc, which detract from appreciation of the spaces and/or inhibit interpretation. False ceilings, mezzanines and other decorative elements identified as intrusive should also be removed when possible.
- Future fit-outs should endeavour, where possible, to enhance the character and historic use of the building, either in the manner of finishes or by responding to the types of businesses that occupied Campbell's Stores (for example, the importation of materials from the South Pacific, or wine and spirit merchants that operated in Campbell's Stores).
- All original or early timber floor and wall finishes should be retained and left exposed.
- Existing timber floors should not be covered with carpets or other such finishes. Where this is unavoidable, carpets should be laid in a manner that does not damage significant fabric.
- Original or early sandstone brick wall surfaces should not be painted and should not be obscured by linings or fit-out elements. However, where evidence indicates an early or original finish, such as a lime-wash finish, then such a finish could be reconstructed.
- Original or early timber structural elements should not be painted. Previously painted surfaces of significant elements could be stripped, where this could be undertaken without causing damage to the fabric.

7.6.2 Integration of Services

Background

Adaptations of existing fabric for practical reasons such as installation of new services and equipment, and the need to meet fire safety and other statutory requirements may be required in terms of securing a viable use for the building components as a whole, and satisfying the changing needs of the general public.

Policy 19

The extension or alteration of existing services in Campbell's Store is acceptable in the context of re-use, but should not have a detrimental impact to the significance of the building components as a whole.

Policy 20

Plant and equipment should be discreetly located and is not to impact on the significant roovescape of the Stores. Ideally plant and equipment should be contained within existing ancillary structures and not impede on the original building envelope.

Guidelines

- Any proposed upgrading of services should be carefully planned. The preparation of schematic layouts is not sufficient: service routes must be planned so as not to damage the significant fabric or disrupt significant spaces.
- The introduction of lift to Campbell's Stores needs to be a solution that addresses the entire building and not a single tenant. The design and placement of the lift will need to be within the constraints of the policies outlined in this section.
- Any upgrading of services is subject to the proper approval process.
- Existing or old service chases or conduits should be re-used in preference to new chases. Services should be rationalised, grouped and treated to minimise intrusion.
- Areas previously modified for services should be re-used, in preference to modifying intact fabric. Intervention into any building fabric should respect the integrity of the extant material, be carefully controlled, and be limited to that required by the proposed works.
- The surface mounting of services is preferable to chasing services into significant fabric.
- Brackets or fixings for services should not damage significant fabric.
- No externally mounted air conditioning, ventilation equipment, water heaters, solar panels or service components should be visible from either Hickson Road or the promenade on the harbour side of Campbell's Stores, or impact negatively on the facade.
- New services should be concealed wherever possible, in a manner that does not interrupt existing spatial volumes. Where this is not possible, essential services should be exposed and painted or treated in an unobtrusive but honest manner.
- Fittings should be fixed in a manner that causes least intervention in significant fabric.
- The design and location of fittings such as lights, ceiling fans, heaters, etc, should have regard to the significance of adjacent elements. Where possible, these fittings should be designed as free-standing rather than fixed elements.
- Existing fittings that detract from the appearance of the interiors or obscure interpretation of the fabric should be relocated, or preferably removed when the opportunity arises.

7.6.3 Ordinance Compliance

The Building Code of Australia is the operative building ordinance in New South Wales.

In terms of ongoing use programs, the key issues are usually compliance with fire resistance, egress provisions, and provision of disabled access and facilities. It is essential that the cultural

values of the buildings and the overall complex are not degraded by inappropriate responses to meeting ordinance requirements.

Policy 21

Approaches to compliance with building ordinances for the conservation and upgrading and re-use programs of the various building components of Campbell's Store should focus on responding to the spirit and intent of the ordinances if strict compliance would adversely affect the significance. Uses which require an unacceptable degree of intervention for upgrading to ordinance compliance should be avoided.

Guidelines

- Conservation and ongoing use programs should not place undue stress on the building fabric in order to meet excessive requirements of ordinance compliance.
- Methods of complying with ordinance requirements which utilise fire or smoke detection and active fire suppression are preferred to the addition of fire-rating material, which may obscure extant finishes.
- Future upgrades of the buildings should take into consideration any newly developed approaches for the implementation of fire safety standards that do not harm the existing significant fabric.
- When dealing with disability access issues, refer to the document Access to Heritage Buildings for People with Disabilities prepared by E.J. Martin (Cox), August 1997.
- Consultation with the Heritage Branch of the NSW Department of Planning FASAP advisory panel may also assist in achieving appropriate compliance solution.

7.6.4 Signage and External Lighting

Background

It is recognised that commercial and retail tenancy identification and temporary signage are an intrinsic component of the commercial use and viability of Campbell's Stores as a whole. External lighting of buildings can add to their character.

Policy 22

Signage and external lighting should have no adverse impact on significant heritage fabric and the overall character of the place.

Guidelines

- In addition to conforming to relevant SHFA signage policy, a building-specific signage policy and manual should be developed by SHFA to ensure a consistency of all identification, tenant and interpretation signage.
- No signage should be attached to the facade, ie above Level 1 (ground level) of Campbell's Stores.

Policy 23

External signage and lighting should be in harmony with the overall character of the place, and complement the historic character of the building on which it is mounted. All signage is to be consistent with a whole of building signage strategy which is to be developed to the approval of SHFA.

Guidelines

- Suitably located signs that provide a legible and clear message and contribute to the vitality of the Campbell's Stores are encouraged.
- Signs on the individual building components should be discreet and complementary in terms of colour, material, proportion, positioning and font.
- Signs should be harmoniously integrated with the architecture of the building and should not obscure or damage the significant features or fabric.
- It is not necessary to attempt to create or recreate a 'historic' character in the signage, but modern standardised 'trademark' signage is not appropriate.
- Fluorescent and iridescent paints are inappropriate. Signs should preferably be illuminated by floodlighting.
- The illumination of Campbell's Stores should highlight architectural features rather than floodlighting whole façades. Care should be taken to ensure that over-illumination does not occur.
- Contemporary painted signage should be removed if its removal will not damage original or early fabric.

7.6.5 Foreshore Authority Policies

Background

In addition to specific requirements for new work as presented in this section, all new work has to comply with other relevant planning policies to assure grant of consent to the proposal.

Policy 24

Any proposed work to Campbell's Stores will fully comply with all the relevant policies of the Authority applicable to development in The Rocks.

Guidelines

- In addition to the Authority's policies for lighting and signage, as presented in this section, all new work will comply with the policies for commercial outdoor seating, sustainability and fitouts.

7.7 Archaeological Resources

7.7.1 Prehistoric (Aboriginal) Archaeological Resources

Background

Sites of pre-historic archaeological potential are protected under clauses of the *NSW National Parks and Wildlife Act 1979*.

Policy 25

Any potentially encountered Aboriginal archaeological resources should be conserved in accordance with the requirements of the NSW National Parks and Wildlife Act 1979 and their potential for interpretation considered.

Guidelines

- Works on this site should avoid areas of high archaeological potential or significance.
- In the event archaeological material is unexpectedly discovered during works to this site, work shall immediately cease in the affected area and SHFA and the NSW National Parks and Wildlife Service will be contacted for advice.
- Should disturbance be required where Aboriginal archaeological material has been identified, an application under Section 90 of the National Parks and Wildlife Act will be required for this disturbance.

7.7.2 Historic (European and Aboriginal) Archaeological Resources

Background

Sites of historic archaeological potential are protected under clauses of the *NSW Heritage Act 1977*.

Policy 26

Any potential archaeological resources on the property should be conserved in accordance with the requirements of the NSW Heritage Act 1977 and their potential for interpretation considered.

Guidelines

- Wherever possible, works to this site should avoid areas of high archaeological potential or significance.
- Should disturbance be required to areas of archaeological potential or significance, an application under Section 60 of the Heritage Act will be required for this disturbance.
- Any archaeological resources must be managed in accordance with the recommendations arising from the archaeological assessment and any approval issued by SHFA or the Heritage Branch of the NSW Department of Planning.

In the event archaeological material is unexpectedly discovered during any works to this site, work shall immediately cease in the affected area and SHFA will be contacted for advice.

Campbell's Stores is of State heritage significance as a landmark building in The Rocks and for its association with the history of Sydney Cove.

The purpose of the conservation policies set out in this section is to provide an in-principle guide to actions to be followed in the long-term use, conservation and maintenance of Campbell's Stores so that its cultural significance is retained and enhanced.

The conservation policy statements in this section embrace the following principles:

- the maximum retention of cultural significance;
- respect for the existing fabric, use, associations and meanings requiring a cautious approach of changing as much as necessary but as little as possible;
- the maximum retention of significant functional relationships, spaces and fabric;
- that conservation/use is to have regard to the relative significance of individual elements;
- the use of professional conservation advice;
- the maintenance of records;
- the enhancement of significance through interpretation; and
- allowing ongoing change while retaining key elements of significance.
- All original or early mechanical equipment, including hydraulic hoists, should be retained in their current locations and appropriately maintained. Missing elements currently in storage should be reinstated as a matter of priority and interpreted.
- The integration of the mechanical lifting devices with the layout of the internal spaces should be interpreted.

7.7.3 Archaeological Monitoring

Background

The Campbell's Stores site has potential archaeological, scientific and research significance relevant to earlier uses and the development of the site. The archaeological significance may be reduced due to disturbance from later alterations and refurbishment works to the site.

Detailed assessment of the archaeological potential of Campbell's Store was carried out as part of The Rocks and Millers Point Archaeological Management Plan prepared by Higginbotham, Kass & Walker in 1991 and in the *Campbell's Store—Conservation Management Plan* prepared by GML in 2004.

The *NSW Heritage Act 1977* states that any excavation, where relics may be disturbed, requires an excavation permit.

Policy 27

Where the site is identified as having archaeological potential all excavation should be carried out under supervision by a qualified archaeologist. Appropriate permits should be sought from the NSW Heritage Council prior to the commencement of works.

Guidelines

- Based on the significance of the site, The Rocks and Millers Point Archaeological Management Plan 1991, and The Rocks Archaeological Report 2000, it is the recommendation of the CMP that any ground disturbance on the property in the future be subject to further archaeological monitoring
- Provision should be made for the preparation of an Archaeological Assessment of the impact of any proposed works involving disturbance of ground level floors or external ground levels prior to their approval to determine the need for archaeological investigation and/or monitoring. Work involving such disturbance will require an Excavation Permit from the New South Wales Heritage Council. Such works would include the following:
- external refurbishment or alterations involving disturbance of existing ground levels or subsurface deposits. In particular, any proposed subsurface disturbance within the area north of Bay 11 should be thoroughly assessed as this area contains high archaeological potential to reveal intact deposits relating to the former cottage within this area;
- internal refurbishment that would involve disturbance of the lowest floor level or below-floor deposits, or involve new openings in original or early walls or the service tunnel walls;
- any excavation for new or replacement external and/or internal services; and
- repair or replacement of old services in existing trenches or conduits.
- The existing service tunnel may be retained. No alterations to the tunnel should be made that would adversely affect the adjacent significant fabric of Campbell's Stores.

7.8 Sustainability

Background

In 2009 the Authority adopted a Sustainability Policy with commitments to reduce the carbon footprint of its precincts by 80% from a 2000 baseline, with a 20% reduction by 2012. The policy aims to achieve climate change targets through:

- Reductions – increased energy efficiency and reduced energy consumption.
- Renewables – a portion of energy from non-fossil fuel sources.
- Offsets – the remainder of fossil-derived energy being sourced from green energy tariffs and/or offset through accredited providers.

In its aim for a socially inclusive and sustainable community the Authority:

- Acknowledges its responsibility to protect heritage buildings and their cultural significance on behalf of the people of NSW.
- Aims to provide improved tenant amenity through improved air quality, comfort levels and natural light, in ways that support heritage considerations.

In line with this Policy the Authority has adopted a Sustainable Development Fit-out Guide which aims to reduce the environmental impact of new construction and building fit-outs. It has been

shown that the implementation of the measures outlined in the Fit-out Guide have the potential to reduce operational costs and improve indoor environmental quality (IEQ) and occupant satisfaction.

Policy 28

All applications for building modifications and fit-outs are to demonstrate measures which work toward the objectives of the Authorities Sustainability Policy.

Guidelines

- The factors contributing to discomfort should be identified and quantified before the development of any solutions.
- Building users and Managers should understand the impacts that the ‘use’ of the building will place on the existing thermal properties of the building. If the use will increase the levels beyond that which can be managed by passive solutions, and the required modifications present an unacceptable level of intervention, the use may not be supported.
- Sustainable and passive solutions should be investigated and implemented to demonstrate and measure their effectiveness before moving to mechanical solutions.
- Proposals should reflect both the sustainability and conservation objectives of SHFA.
- The design of mechanical ventilation systems should be developed from a clear and informed understanding of the buildings significance.
- The Policies of this CMP, The Rocks Heritage management Plan, the Foreshore Authority Sustainability Policy, Sustainable Development Fit-out Guide, the Burra Charter and best conservation practice should be key guidelines in the design and development of mechanical ventilation systems.
- The introduction of solar panels to the significant building fabric and roofscape of the Stores could not be supported.

7.8.1 Passive Design

Background

Historically, buildings were designed, constructed and sited to maximise the effectiveness of passive solutions, using the seasons to provide for thermal comfort needs. Historic buildings also often have thermal properties that need little modification to maintain good thermal comfort levels for the majority of operational hours without the need for air-conditioning.

Policy 29

Full and mixed mode mechanical ventilation systems (air-conditioning) in heritage buildings will not be considered by Sydney Harbour Foreshore Authority unless it can be demonstrated that passive ventilation solutions and the inherent properties of the building envelope cannot provide comfortable thermal environments throughout the majority of the seasonal calendar.

Guidelines

Studies have indicated that the implementation of passive measures can result in a 50% decrease in energy consumption within historic buildings.² This figure is reflective of the inherent thermal properties of many historic buildings. The following measures should be considered in the development of building specific design solutions, with the potential impacts to both the interpretation of the building in the streetscape and the potential impact to the fabric of the building evaluated.

- Solar heat loading can provide valuable warmth in the cooler months, but can contribute to uncomfortable environments during summer.
- Technological advancements in glass tinting have seen the development of clear film which can keep out up to 60% of heat and 99% of ultra-violet radiation.
- Air movement can play an important role in thermal comfort levels and is proven to be more energy efficient than air conditioning within a closed building envelope. Fans can be used when there is not sufficient air movement to maximise the effectiveness of natural ventilation. Fans which are used to enhance summer thermal comfort can also be used to de-stratify indoor air to save heating energy in winter.³
 - Ensure that ceiling fans do not adversely impact, visually and/or materially, on significant fabric.
 - Ensure that the reticulation of power does not impact on significant fabric.
 - Ensure that floor-standing fans remain unfixed.
 - Ensure that power cables do not pose a trip hazard.
 - Floor-standing fans should be placed to achieve optimum circulation.
 - Select energy efficient products.
 - Seek advice to ensure that the placement of fans does not result in chaotic air movement, low ceiling pressure zones against the ceiling and undifferentiated air pressure in other parts of the building.
- Internal partitions, forming part of a tenancy fit-out, should be designed or modified to enhance the passive thermal capacity of the building. Ensure that partitions are low in height so as not to obstruct natural light, views to and from the building and the transfer of natural ventilation. Ensure that windows are not obstructed by partitions or other furniture such that they cannot be easily accessed and opened.

7.8.2 Mechanical Ventilation Systems

Background

Mechanical ventilation systems should be designed to supplement existing passive measures. This will ensure that any necessary mechanical system are not overloaded, operate efficiently and in turn reduce energy consumption.

Careful system selection and implementation is integral to the success of any mechanical heating, cooling and ventilation (HVAC) system. This is even more critical with heritage buildings, where mechanical systems are often coupled with passive systems, and often have a significant visual and physical impact on the building.

It should be understood that there is no one size fits all approach and each building will have different features and limitations which must be carefully considered and catered for. It is important to look at the benefits and drawbacks of each system and see which one is best suited to the building, and its use.

Policy 29

Mechanical solutions, following the implementation and analysis of passive solutions, should be designed and selected in consideration of the heritage significance of the place and the objectives of the Sustainability Policy. The design of mechanical solutions should be supported by an options analysis demonstrating that the proposal presents the least impact to the significant fabric of the place and accounts for ongoing energy consumption. Mechanical solutions may not be supported if they present an adverse impact to the significance of the place.

Guidelines

- Reference should be made to the Foreshore Authorities Sustainable Development Fit-out Guide and the Green Building User Guide for criteria relating to the installation and alteration of mechanical ventilation systems.
- Consider the physical and visual impact the system will have on the heritage value of the building.
- The notching of timber floors in the reticulation of services can adversely impact on the structural loading capacity of timber floors. Any such solutions will require input from a Structural Engineer with demonstrated experience with heritage buildings.
- New services should be introduced in a manner that will have minimal adverse impact on significant fabric. New services should be unobtrusively located and preferably concealed within existing ducts and voids.

7.9 Implementation

7.9.1 Implementation of Conservation Policies

Policy 30

Conservation policy should be implemented using appropriate expertise in consultation with tenants. Any future proposed works activities should be checked for compliance with conservation policy.

Guidelines

- Any new development or alterations of the fabric of Campbell's Stores and its site, or any change of use, should be the subject of a development application. Such development applications should be assessed using this CMP as a guide for decision-making.

- Tenants should consult with SHFA before undertaking any works on Campbell's Stores except the specific list of maintenance tasks that are agreed with SHFA that are tenant responsibilities.
- All future work to the place, whether undertaken by the SHFA or its tenants, should be carefully monitored by SHFA's heritage specialists.
- Conservation works should be designed and documented under the supervision of a qualified conservation architect.
- SHFA should seek the co-operation of the proprietors of the restaurants to ensure that management practices and operations are consistent with the protection of the cultural significance of the place.
- When the opportunity arises to enter into new lease agreements with tenants or amend existing agreements, SHFA should ensure that suitable provisions are included to enable these conservation policies to be effected. The responsibilities of the respective parties in relation to the carrying out of conservation works, maintenance, or any future development proposals should be clearly stated.

7.10 Endnotes

1 Adoption (SHFA), Endorsement (NSW Heritage Council) and Review (SFHA)

2 <http://www.nps.gov/hps/tps/briefs/brief03.htm>

3 <http://www.bigassfans.com/research.php?id=16>

8.0 Appendices

Appendix A

The Burra Charter

http://australia.icomos.org/wp-content/uploads/BURRA_CHARTER.pdf

Appendix B

Inventory Plans and Significance Gradings

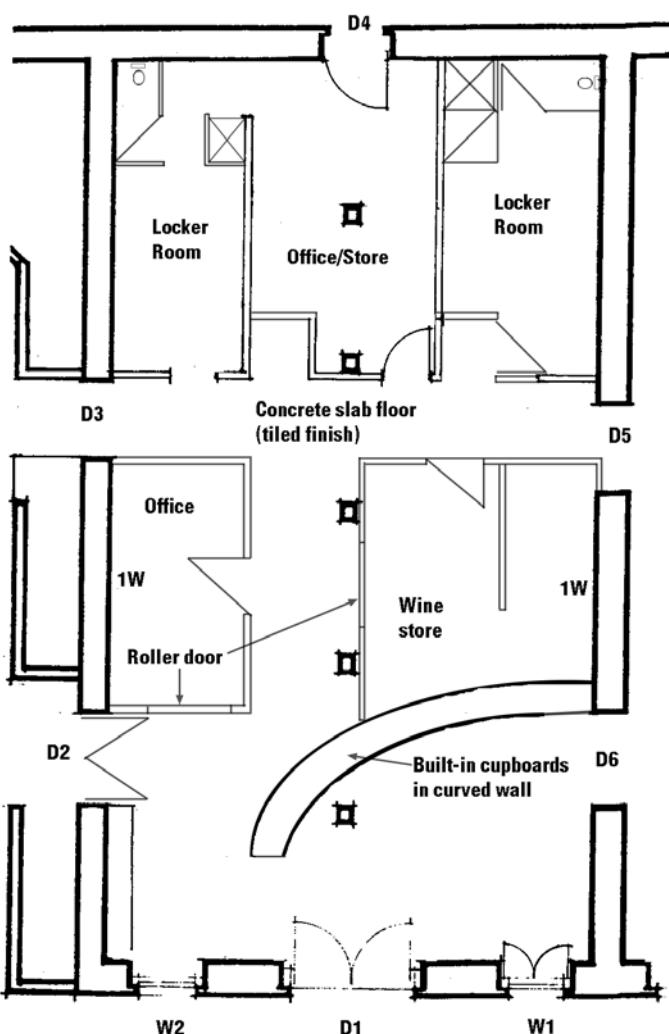
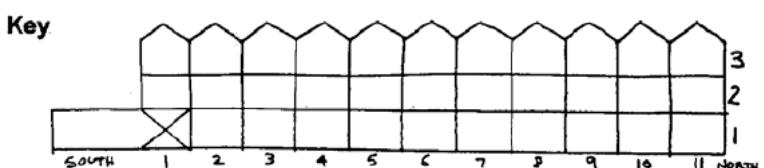
The pages following are Inventory Plans of each floor level within all bays and Significance Gradings of all elements within each of the spaces. Drawings date from 2004 CMP and layouts may have been subject to modification since this time.

Bay: 1

Level: 1

Assumed North →

Date: December 2010



Significance Gradings

Exceptional
 All posts
 All beams
 All joists
 IW Internal sandstone walls
 D1, doors, door opening, reveals and gudgeons
 W1, window opening, reveals, bars, gudgeons, shutters and furniture
 W2, window opening, reveals, bars and gudgeons

Moderate
 Wall pointing

Little
 Concrete slab floor
 Doorway 2 (1970s)
 Doorway 3 (1970s)
 Doorway 4, formed by opening former window
 Doorway 5 (1970s)
 Doorway 6 (1970s)

Intrusive
Internal
 W1 fixed perspex sheet
 W2 fixed perspex sheet
 Furniture fixed to walls
 Bar
 Partitions (P1–P4)
External
 Attachments to wall for mast and canopy guy wires

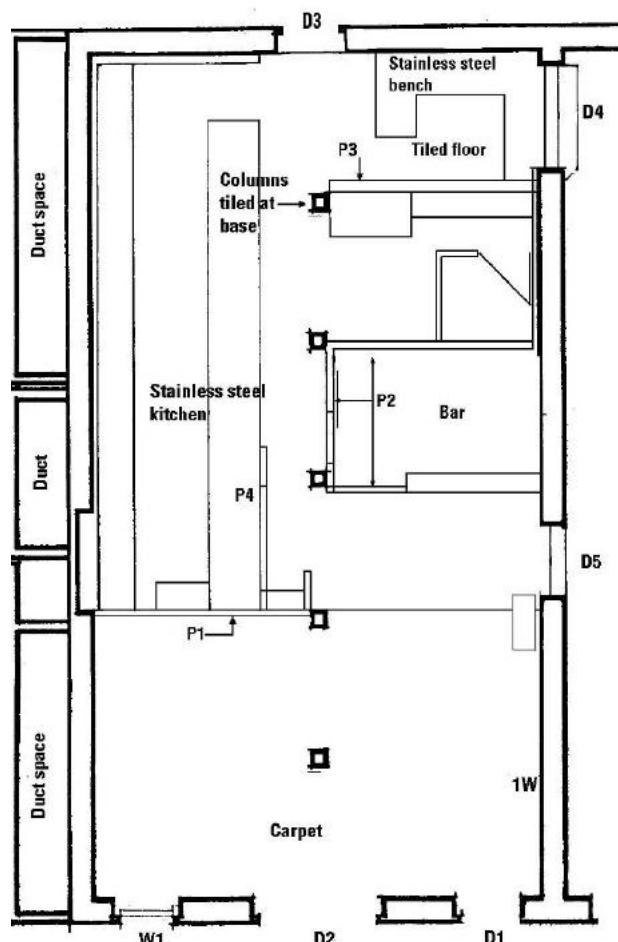
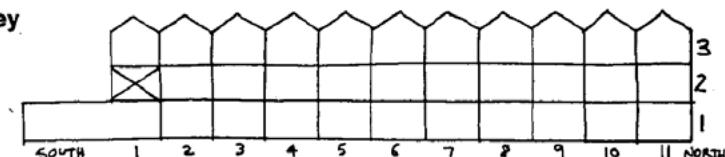
Bay: 1

Level: 2

Assumed North →

Date: December 2010

Key



Significance Gradings

Exceptional

Internal
All posts
All beams
All joists
All floorboards
IW Internal sandstone walls
D1, doors, door opening, reveals (door missing)
D2, door opening, reveals, gudgeons, goods
apron, chains and handles (doors missing)
W1, window opening, reveals, bars and
gudgeons

Hydraulic ram

Moderate
Doorway 3

Little
Doorway 4 (1970s)
Doorway 5 (1970s)

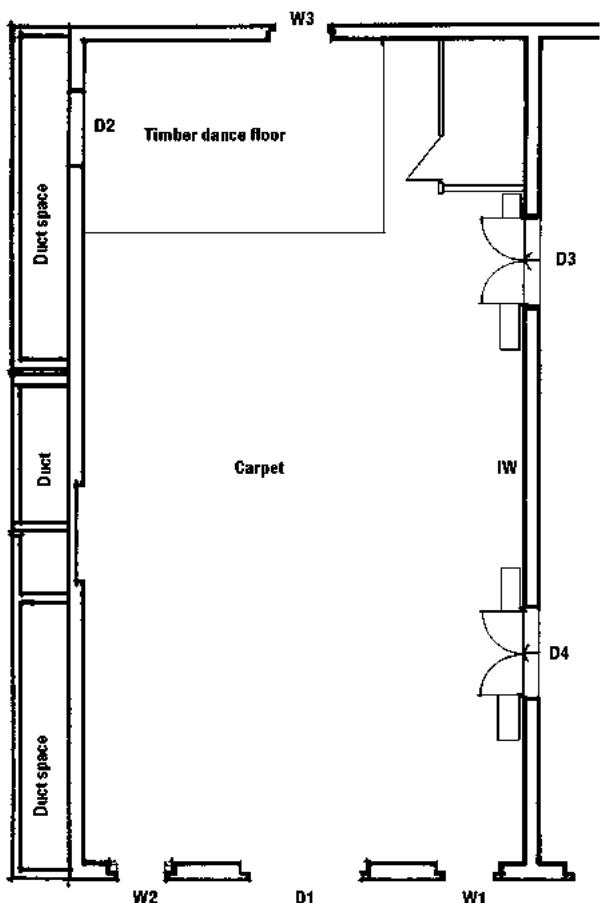
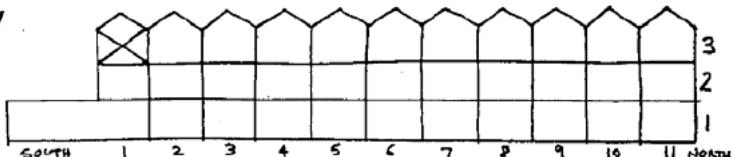
Intrusive
D1, D2 and W1 aluminium fixed-light windows
D1, D2 replacement bars
Stainless steel kitchen
Tiling at base of columns
Partitions (P1-P2)

Bay: 1

Level: 3

Assumed North →

Date: December 2010

Key**Significance Gradings**

Exceptional

Internal

All trusses

All purlins

All floorboards

Cathead beam

Ceiling lining

IW internal brick walls

D1, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing) — deteriorating goods apron/corroding iron
Doorway 4 (1880s)W1, double-hung sash, window opening and reveals
W2, double-hung sash, window opening and revealsW3, double-hung sash, window opening, reveals and bars
External
Hydraulic ram

Little

Doorway 2 (former 1970s service door)

Doorway 3 (1970s)

Ducts and duct spaces

Intrusive

D1 aluminium fixed-light window

D1 replacement bars

Decorative mezzanines fixed to brick walls

Carpet dance floor

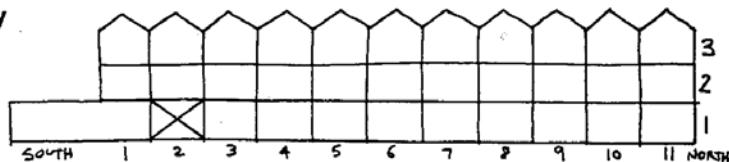
Bay: 2

Level: 1

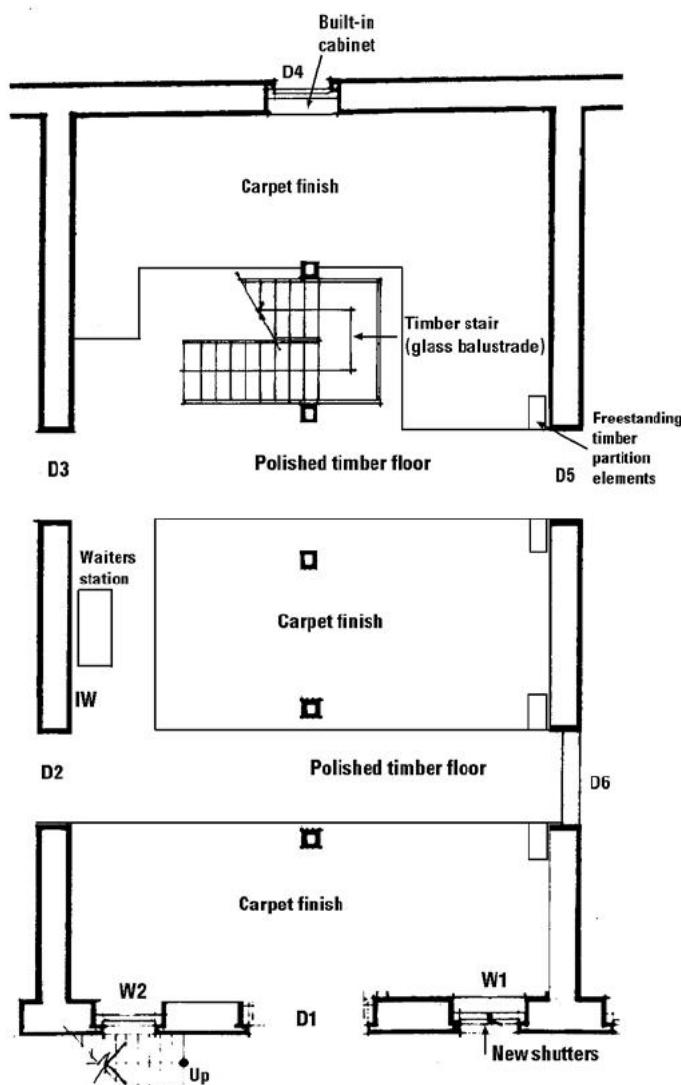
Assumed North →

Date: December 2010

Key



Significance Gradings



Exceptional

All posts

All beams

All joists

IW Internal sandstone walls

D1 doors, door opening, reveals and gudgeons

W1, window opening, reveals, bars and

gudgeons

W2, window opening, reveals, bars and

gudgeons

Moderate

New shutters window 1

Little

Concrete slab floor

Doorway 2 (1970s)

Doorway 3 (1970s)

Doorway 4 (1970s)

Doorway 5 (1970s)

Doorway 6 (1970s)

Timber stair

Intrusive

Internal

W2 fixed perspex sheet

New mezzanines, north and south walls

Fish tank

Furniture fixed to walls

Partitions (P1)

External

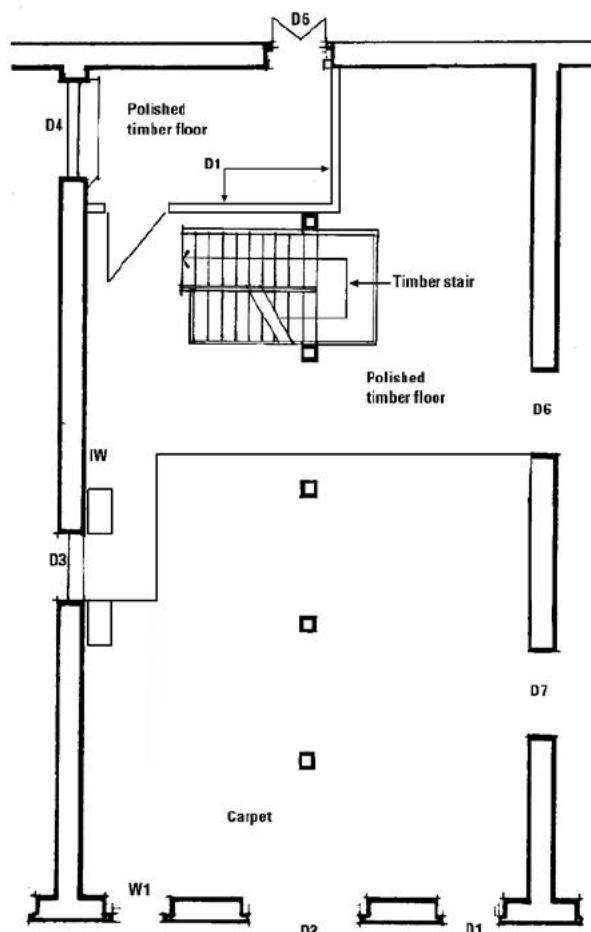
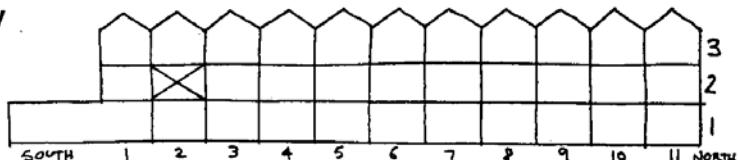
Attachments to wall for mast and canopy
guy wires

Bay: 2

Level: 2

Assumed North →

Date: December 2010

Key**Significance Gradings**

Exceptional
 All posts
 All beams
 All joists
 All floorboards
 IW Internal sandstone walls
 D1, door opening, reveals and gudgeons (doors missing)
 D2, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing)
 W1, window opening, reveals, bars and gudgeons

High
 Doorway 5

Moderate
 Doorway 3 (1970s)
 Doorway 4 (1970s)
 Doorway 6 (1970s)
 Doorway 7 (1970s)
 Timber stair

Intrusive
 D1 aluminium double-hung window
 D1, D2 replacement bars
 D2 aluminium sliding window
 W1 fixed perspex sheet
 Void
 Railing around void
 Furniture fixed to walls
 Partitions (P1)
 Fixed furniture
 P1 glass partitions, freestanding timber elements containing partitions/doors

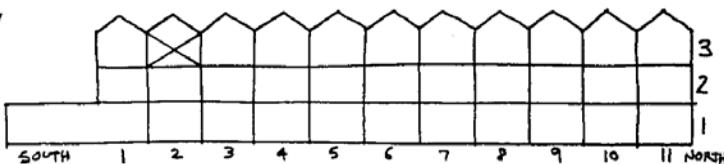
Bay: 2

Level: 3

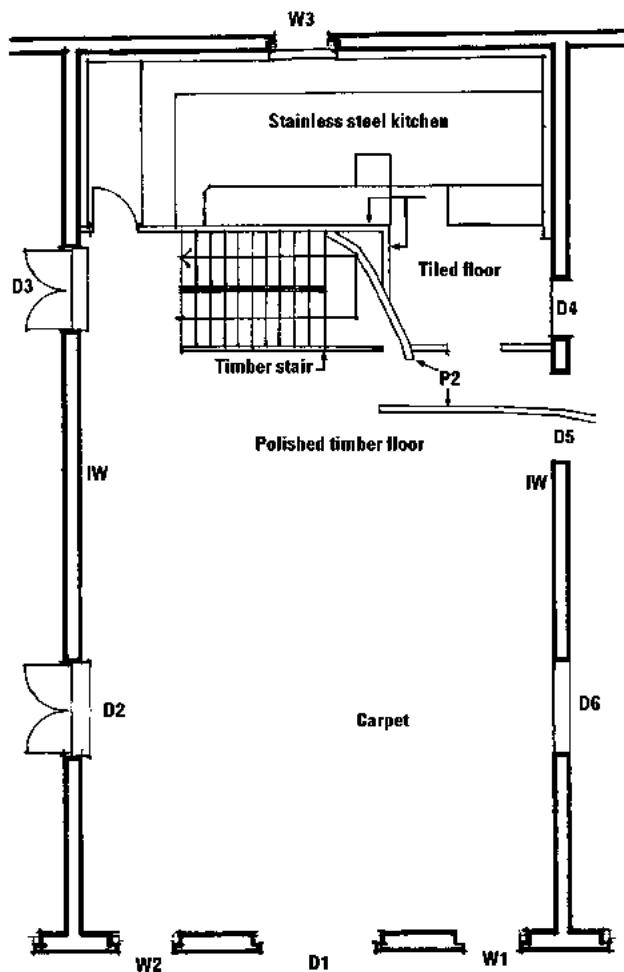
Assumed North →

Date: December 2010

Key



Significance Gradings



Exceptional

All trusses

All purlins

All floorboards

Cathead beam

Ceiling lining

IW Internal brick walls

D1, doors, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing)

Door opening 2

Door opening 6

W1, double-hung sash, window opening and reveals

W2, double-hung sash, window opening, reveals and bars — blocked

Moderate

Doorway 3

Doorway 4

Doorway 5

Timber stair

Intrusive

D1 aluminium and timber fixed-light window

D1 replacement bars

Stainless steel kitchen

Partitions (P1-P2 and benches)

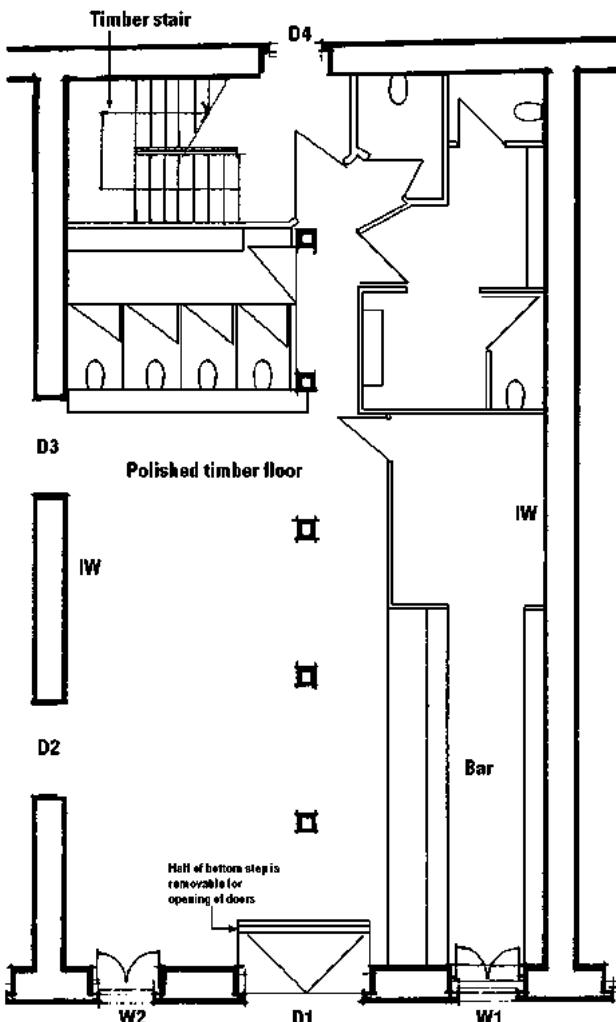
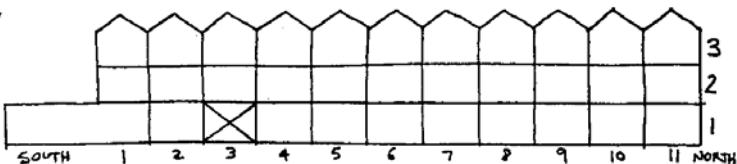
Decorative wall fixtures

Bay: 3

Level: 1

Assumed North →

Date: December 2010

Key**Significance Gradings**

Exceptional
All posts
All beams
All joists
IW Internal sandstone walls
D1, doors, door opening, reveals and gudgeons
W1 window opening, reveals, bars, gudgeons, shutters and furniture
W2 window opening, reveals, bars, gudgeons, shutters and furniture
Moderate
Doorway 2 (1970s)
Doorway 3 (1970s)
Little
Concrete slab floor
Doorway 4, formed by opening former window
Timber stair
Intrusive
<i>Internal</i>
W2 fixed perspex sheet
Furniture fixed to walls
Bar
Toilets
Partitions (P1–P2)
Timber plinth floor
<i>External</i>
Attachments to wall for mast and canopy guy wires

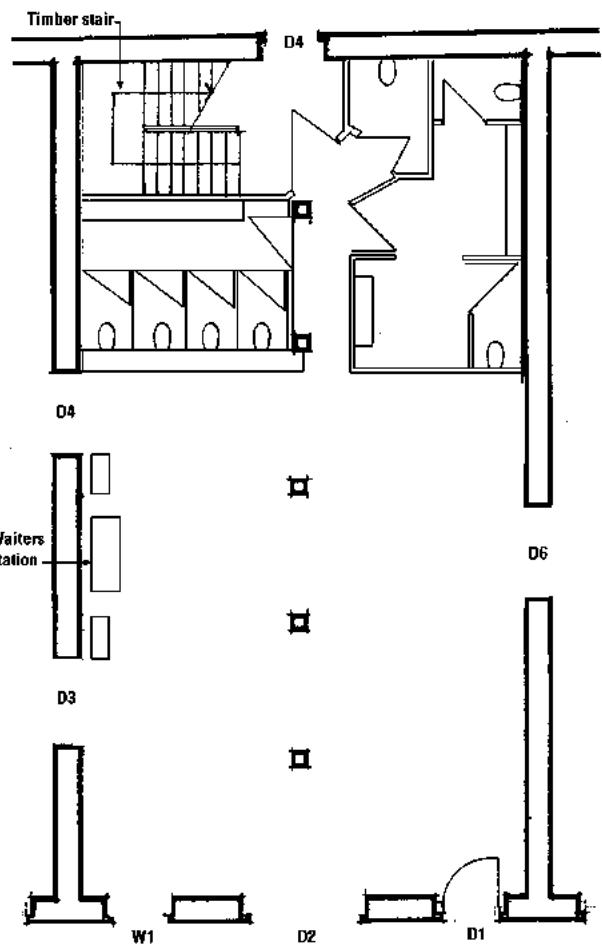
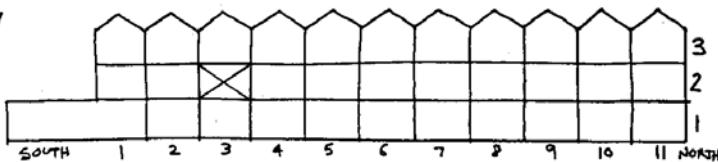
Bay: 3

Level: 2

Assumed North →

Date: December 2010

Key



Significance Gradings

Exceptional
All posts
All beams
All joists
All floorboards
IW Internal sandstone walls
D1, doors, door opening, reveals and gudgeons (doors missing)
D2, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing)
W1, window opening, reveals, bars and gudgeons

High
Doorway 5

Moderate
Doorway 3
Doorway 4
Doorway 6
Timber stair

Little
Timber stair

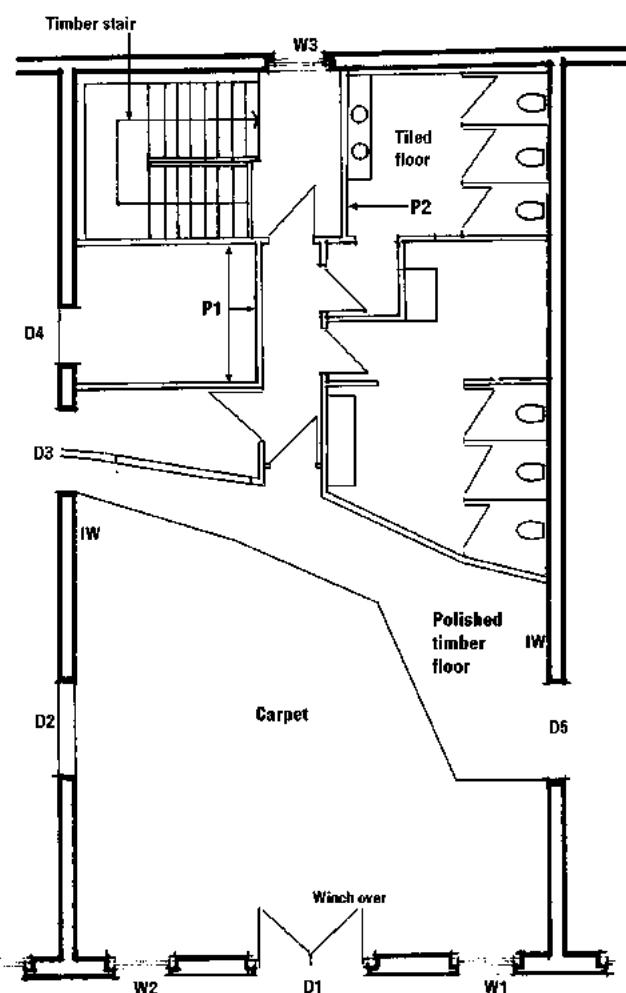
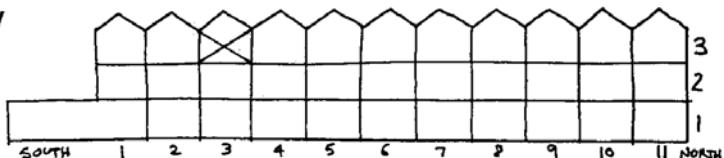
Intrusive
D1 aluminium double-hung window
D1, D2 replacement bars
D2 aluminium sliding window
W1 fixed perspex sheet
Void
Railing around void
Furniture fixed to walls
Toilets
False ceiling in toilets
Partitions (P1–P2)

Bay: 3

Level: 3

Assumed North →

Date: December 2010

Key**Significance Gradings**

Exceptional
All trusses
All purlins
All floorboards
Cathead beam
Ceiling lining
IW Internal brick walls
D1, doors, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing)
Doorway 2
Doorway 5
W1, double-hung sash, window opening and reveals
W2, double-hung sash, window opening and reveals
W3, double-hung sash, window opening, reveals and bars
Winch

Moderate
Doorway 3
Doorway 4

Little
Timber stair

Intrusive
D1 aluminium fixed light window
D1 replacement bars
Toilets
Partition (P1-P2)
Bar

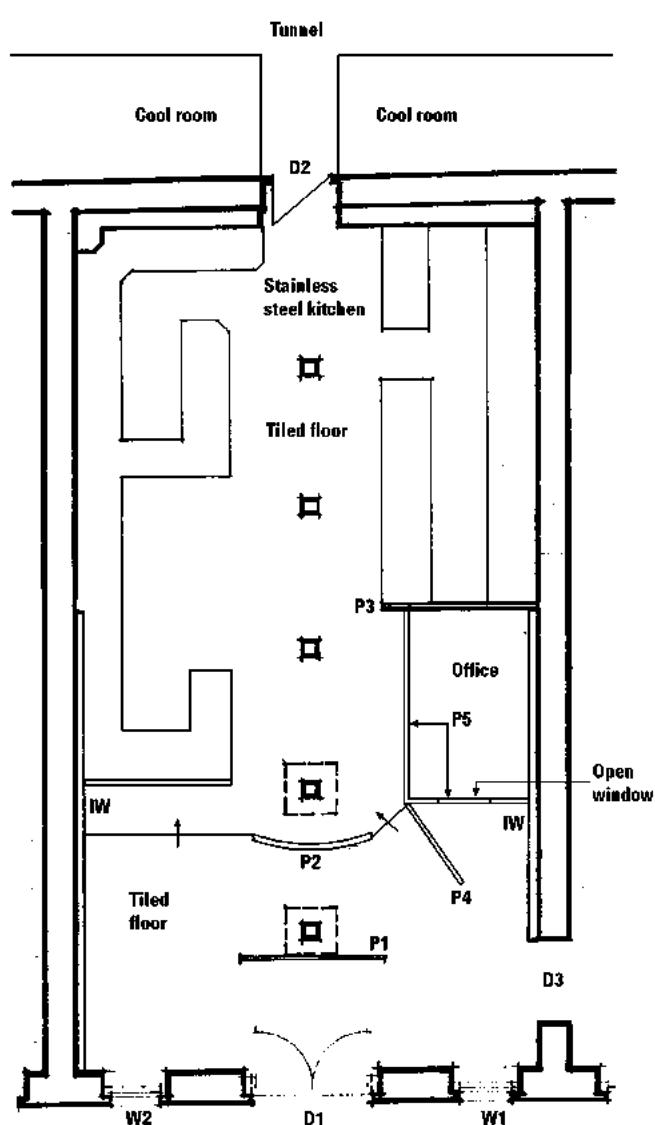
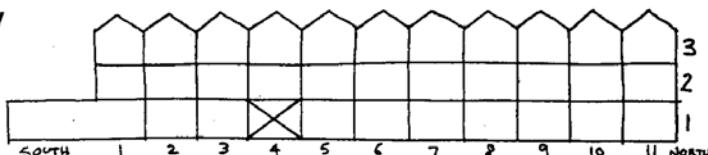
Bay: 4

Level: 1

Assumed North →

Date: December 2010

Key



Significance Gradings

Exceptional

All posts

All beams

All joists

IW Internal sandstone walls

D1, doors, door opening, reveals and gudgeons

W1, window opening, reveals, bars and gudgeons

W2, window opening, reveals, bars and gudgeons

W3

Moderate

Doorway 2, formed by opening former window

Doorway 3

Little

Concrete slab floor

Intrusive

W1 fixed perspex sheet

W2 fixed perspex sheet

Stainless steel kitchen

Lowered ceiling in kitchen

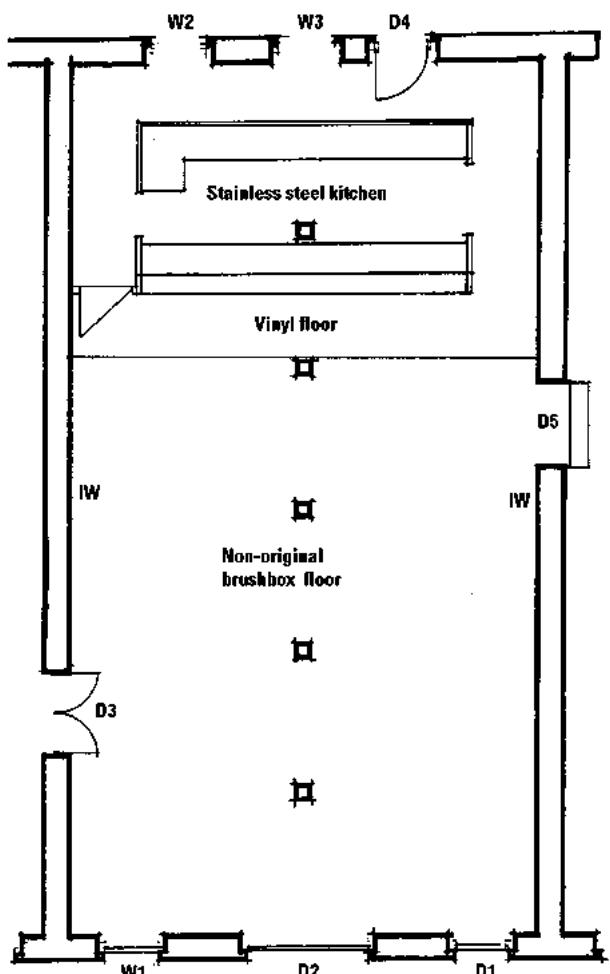
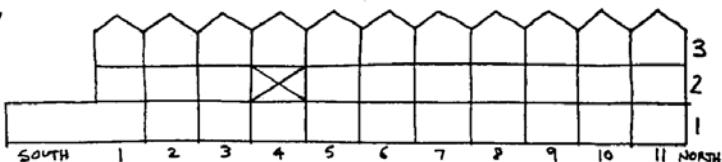
Partitions (P1-P3)

Tiled floor in kitchen

P4 timber partition/screen

Bay: 4
Level: 2
Assumed North →
Date: December 2010

Key



Significance Gradings

Exceptional
All posts
All beams
All joists
IW Internal sandstone walls
D1, door opening, reveals and gudgeons (door missing)
D2, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing)
W1, window opening, reveals, bars and gudgeons

High
Doorway 4
W2, double-hung sash, window opening, reveal, architraves and bars
W3, double-hung sash, window opening, reveal, architraves and bars

Moderate
Doorway 3
Doorway 5
New brushbox floor

Intrusive
Internal
D1 fixed perspex sheet
D2 aluminium sliding window
W1 fixed perspex sheet
Stainless steel kitchen
Tiled floor
External
Wooden bracket

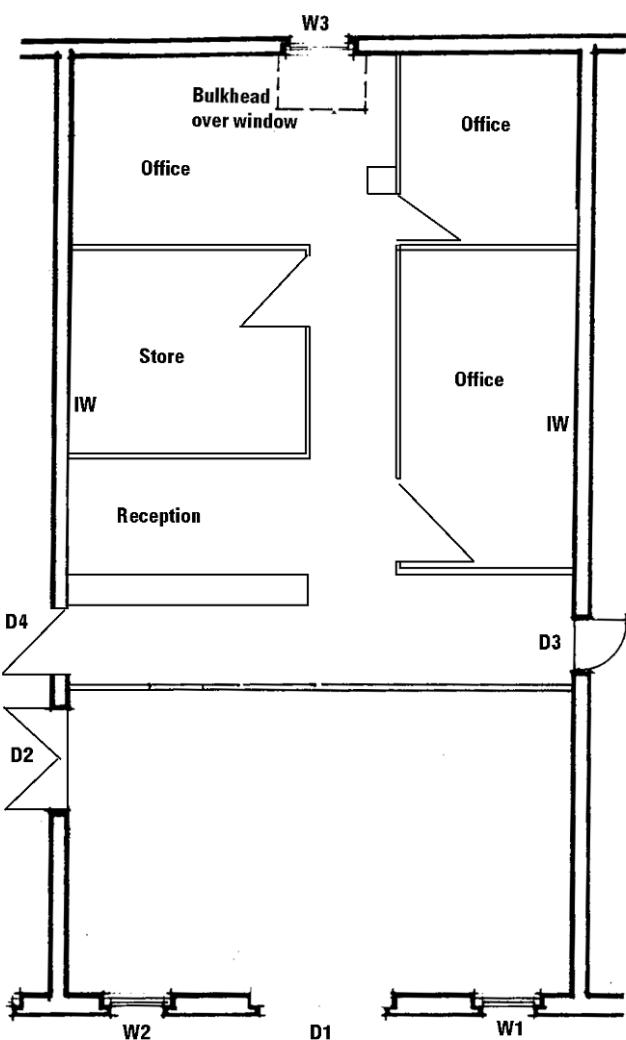
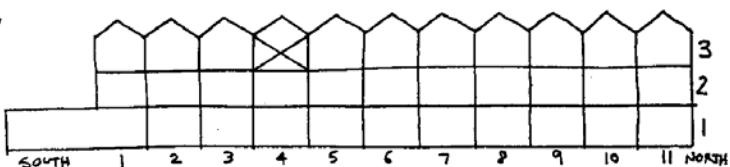
Bay: 4

Level: 3

Assumed North →

Date: December 2010

Key



Significance Gradings

Exceptional
All trusses
All purlins
All floorboards
Cathead beam
Ceiling lining
IW internal brick walls
D1, doors, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing)
Doorway 2
W1, double-hung sash, window opening and reveals
W2, double-hung sash, window opening and reveals
W3, double-hung sash, window opening, reveals and bars

Moderate
Doorway 3

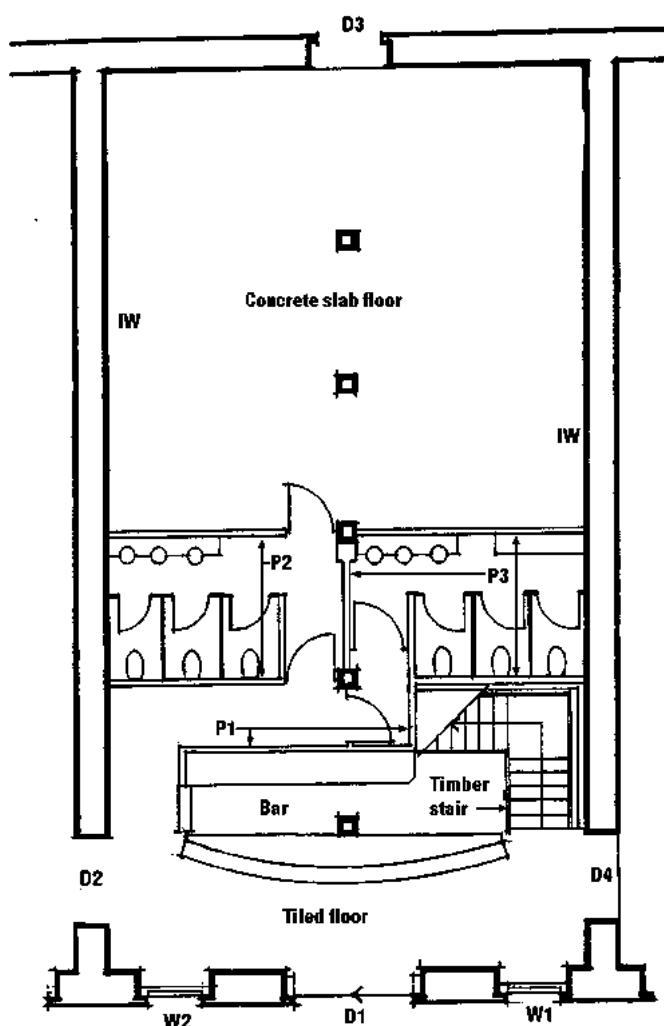
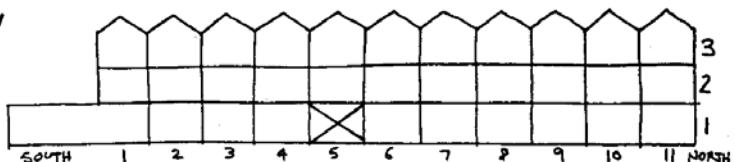
Intrusive
D1 fixed light window
Lowered ceiling in storage and office area
Partitions (P1-Pe)
Bulkhead over window 3
D4 new opening

Bay: 5

Level: 1

Assumed North →

Date: December 2010

Key**Significance Gradings****Exceptional**

- All posts
- All beams
- All joists
- IW Internal sandstone walls
- D1, door opening, reveals and gudgeons
- W1, window opening, reveals, bars and gudgeons
- W2, window opening, reveals, bars and gudgeons

Moderate

- Doorway 2
- Doorway 3
- Doorway 4

Little

- Concrete slab floor

Intrusive

- D1 aluminium sliding door
- W1 fixed perspex sheet
- W2 fixed perspex sheet
- Stair
- Bar
- Fixed shelving
- Toilets
- Partitions (P1-P3)

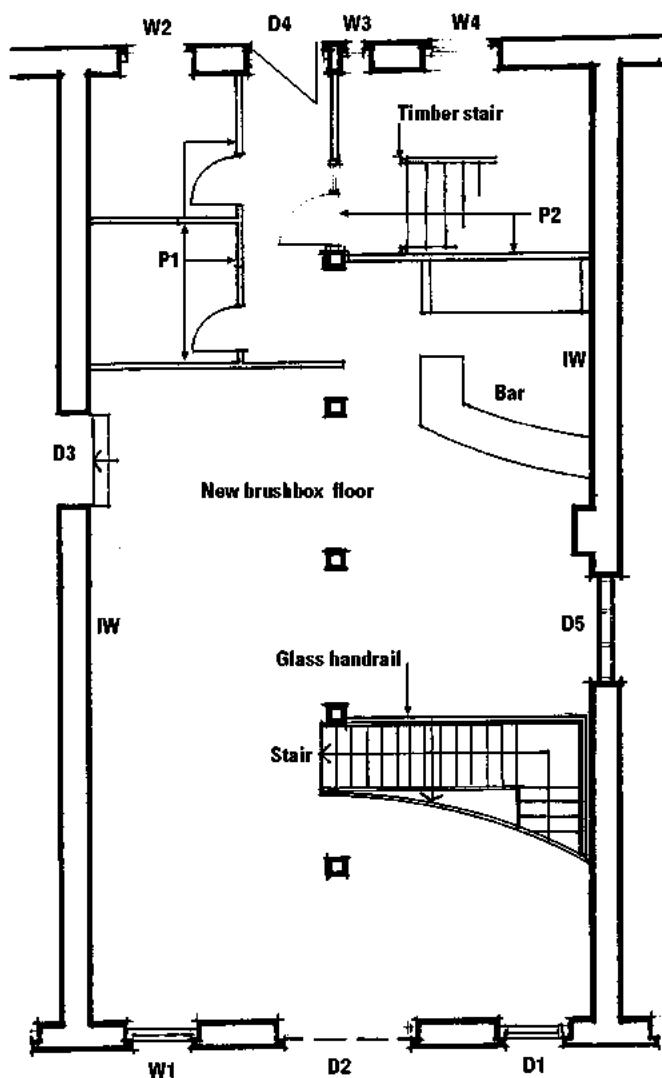
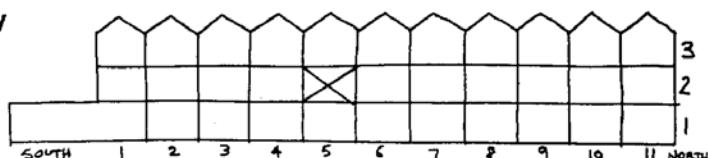
Bay: 5

Level: 2

Assumed North →

Date: December 2010

Key



Significance Gradings

Exceptional

All posts

All beams

All joists

IW Internal sandstone walls

D1, door opening, reveals and gudgeons (doors missing)

D2, door opening, reveals, gudgeons and handles (doors missing)

W1, window opening, reveals and bars

High

Doorway 4

W2, double-hung sash, window opening, reveal, architraves and bars

W3, window opening, reveal, architraves and bars

W4, double-hung sash, window opening, reveal, architraves and bars

Moderate

Doorway 3

Doorway 5

New brushbox floor

Little

Timber stair

Intrusive

D1 fixed perspex sheet

D2 aluminium sliding window

W1 fixed perspex sheet

Bar

Lowered ceiling at west end of bay

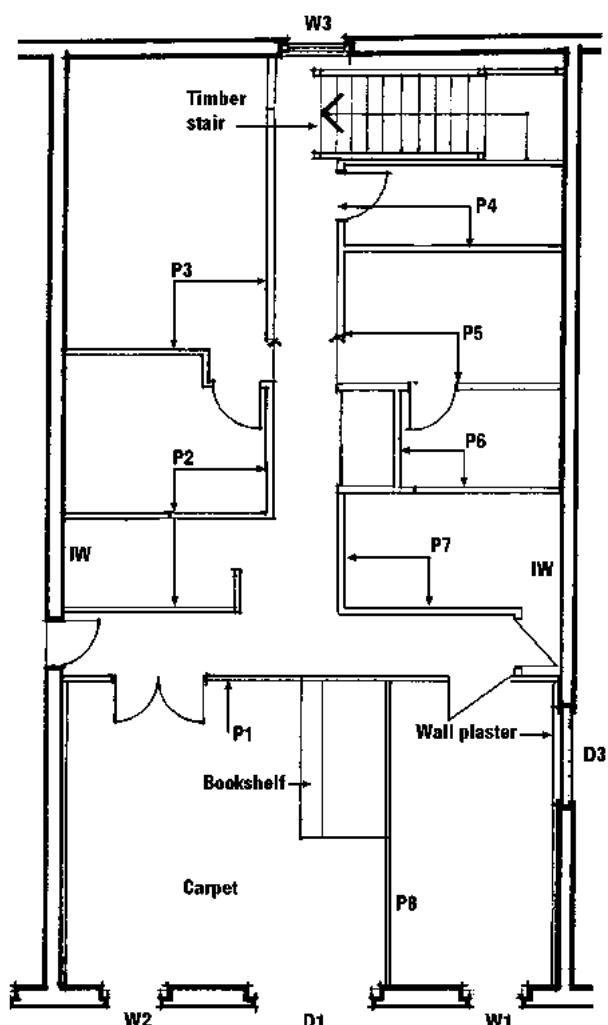
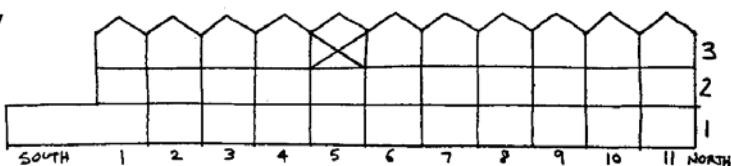
Stair at centre of bay

Glass railing around stair

Furniture fixed to walls

Partitions (P1-P2)

Bay: 5
Level: 3
Assumed North →
Date: December 2010

Key**Significance Gradings**

Exceptional
All trusses
All purlins
All floorboards
Cathead beam
IW Internal brick walls
D1, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing)
W1, double-hung sash, window opening and reveals
W2, double-hung sash, window opening and reveals
W3, double-hung sash, window opening, reveals and bars

Moderate
Doorway 2
Doorway 3

Little
Timber stair
New timber ceiling lining

Intrusive
D1 aluminium sliding window
Wall plaster
Partitions (P1–P8)

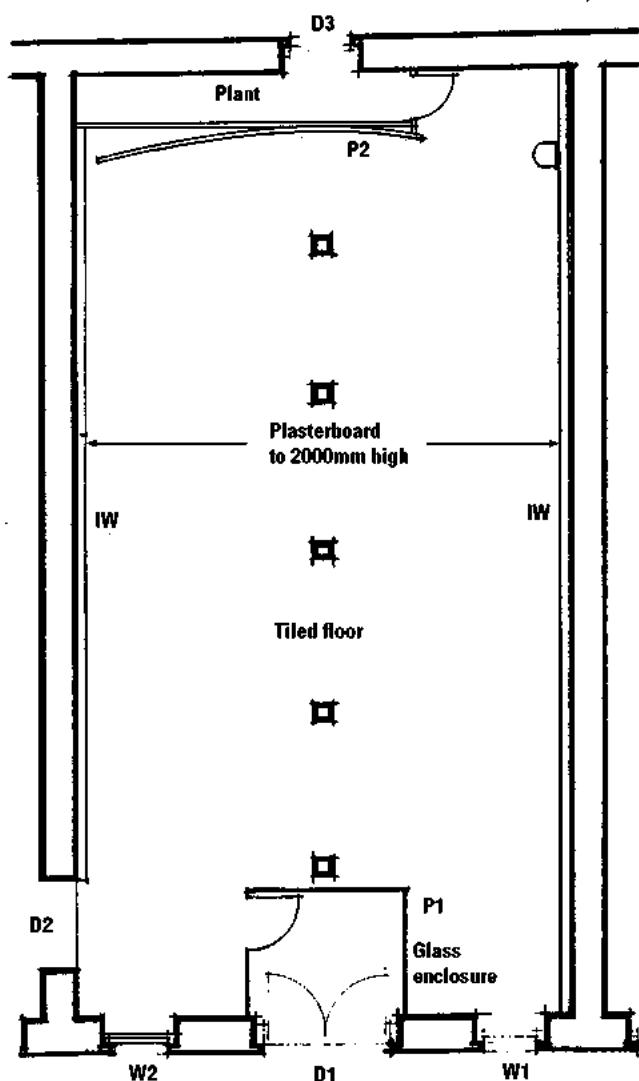
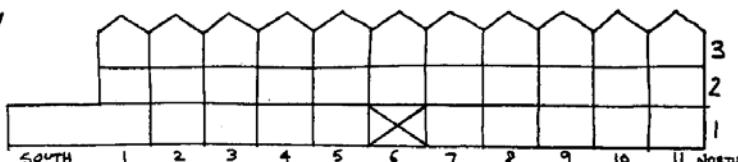
Bay: 6

Level: 1

Assumed North →

Date: December 2010

Key



Significance Gradings

Exceptional

All posts

All beams

All joists

IW Internal sandstone walls

D1, doors, door opening, reveals and gudgeons

W1, window opening, reveals, bars and gudgeons

W2, window opening, reveals, bars and gudgeons

Moderate

Doorway 2

Doorway 3

Little

Concrete slab floor

Intrusive

W1 fixed single-pane window

W2 fixed single-pane window

2000mm high plasterboard on all walls

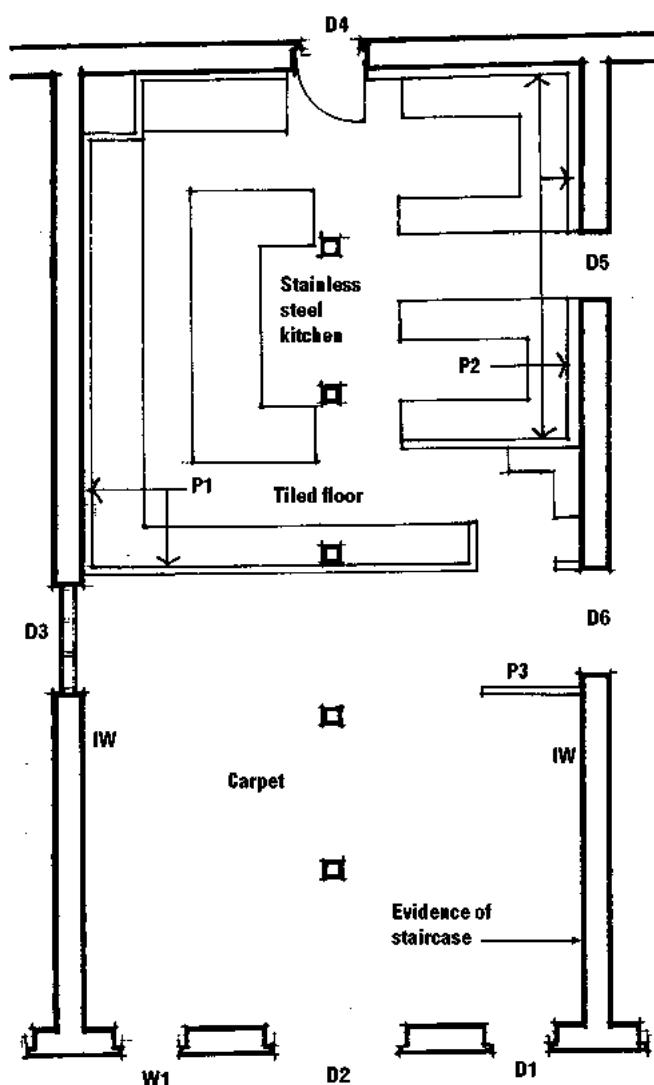
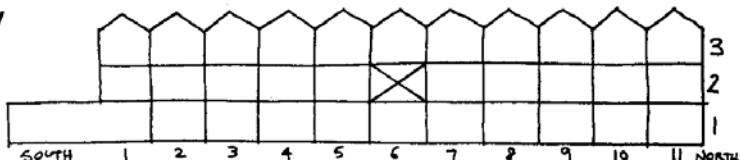
Partitions (P1–P2)

Bay: 6

Level: 2

Assumed North →

Date: December 2010

Key**Significance Gradings**

- Exceptional
 - All posts
 - All beams
 - All joists
 - All floorboards
 - IW Internal sandstone walls
 - D1, door opening, reveals and gudgeons (doors missing)
 - D2, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing)
 - W1, window opening, reveals, bars and gudgeons
 - Evidence of staircase, north wall (east end)

- High
 - Doorway 4

- Moderate
 - Doorway 3
 - Doorway 5
 - Doorway 6
 - W2 single casement

- Intrusive
 - Internal
 - D1 aluminium double-hung window
 - D2 aluminium sliding window
 - Stainless steel kitchen
 - Partitions (P1-P3)
 - Tiled floor in kitchen
 - External
 - Wooden bracket

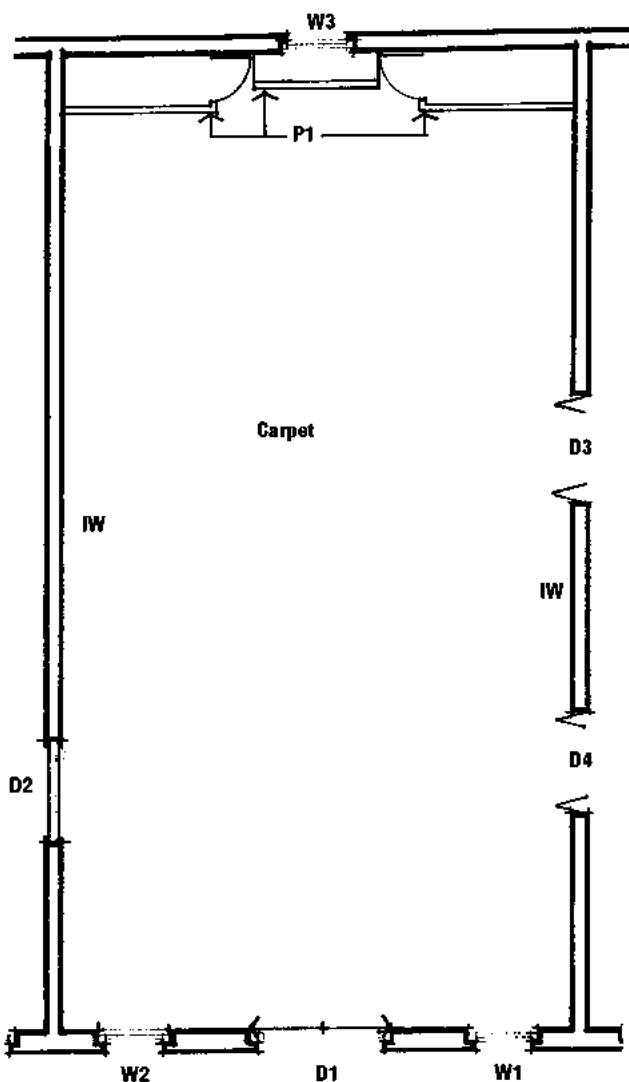
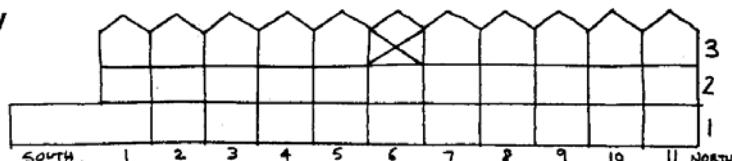
Bay: 6

Level: 3

Assumed North →

Date: December 2010

Key



Significance Gradings

Exceptional

All trusses

All purlins

All floorboards

Cathead beam

Timber ceiling lining

IW Internal brick walls

D1, sliding door, door opening, reveals, gudgeons, goods apron, chains and handles

Doorway 4

W1, double-hung sash, window opening and reveals

W2, double-hung sash, window opening, reveals and bars

Winch

Moderate

Doorway 2

Doorway 3

Straw mat ceiling lining

Intrusive

D1 aluminium sliding window

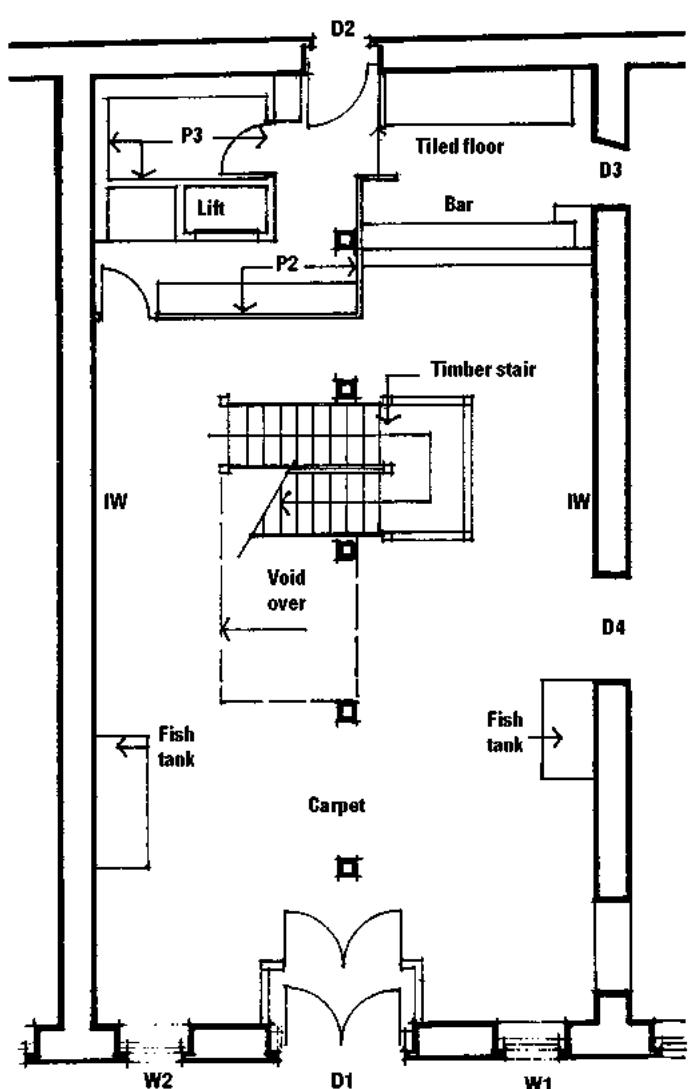
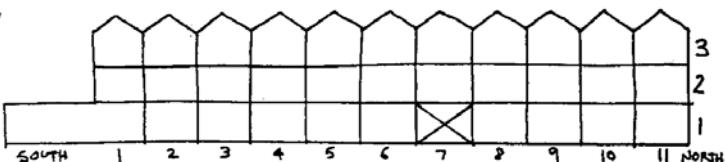
Partition (P1)

Bay: 7

Level: 1

Assumed North →

Date: December 2010

Key**Significance Gradings****Exceptional**

- All posts
- All beams
- All joists
- IW Internal sandstone walls
- D1, doors, door opening, reveals and gudgeons
- W1, window opening, reveals, bars and gudgeons
- W2, window opening, reveals, bars and gudgeons

Moderate

- Doorway 2
- Doorway 3

Little

- Concrete slab floor
- Doorway
- Penetration 1
- Stair

Intrusive

- W1 aluminium sliding window
- W2 aluminium sliding window
- Fish tanks
- Bar
- Partitions (P1-P3)

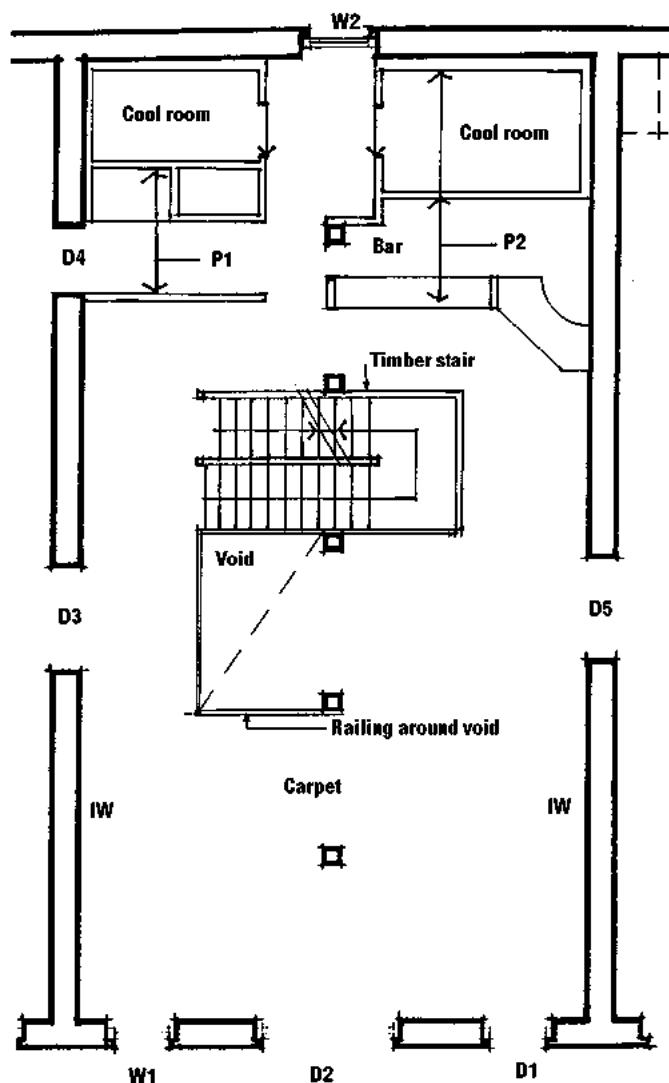
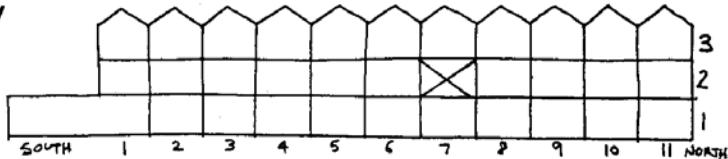
Bay: 7

Level: 2

Assumed North →

Date: December 2010

Key



Significance Gradings

Exceptional
All posts
All beams
All joists
All floorboards
IW Internal sandstone walls
D1, door opening, reveals and gudgeons (door missing)
D2, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing)
W1, window opening, reveals and gudgeons
W2, double-hung sash, window opening, reveals and bars

Moderate
Doorway 3
Doorway 4
Doorway 5

Little
W1 single casement
Stair

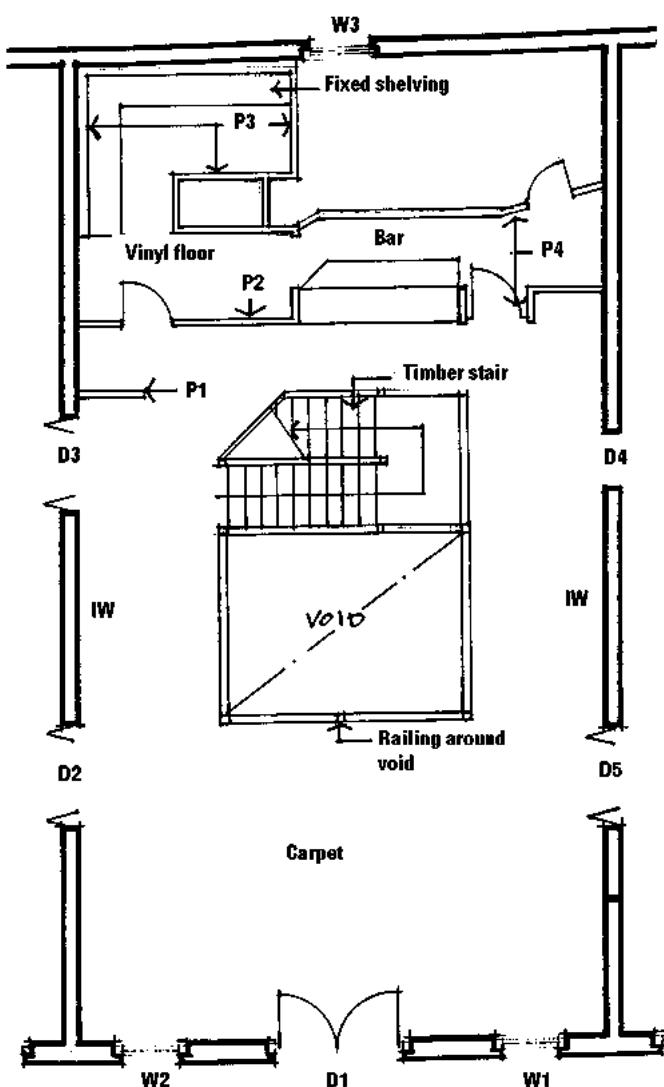
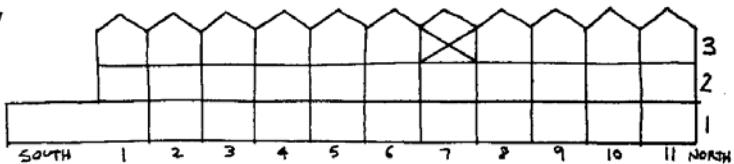
Intrusive
D1 aluminium double-hung window
D2 aluminium sliding window
Void
Railing around void
Bar
Partitions (P1–P2)

Bay: 7

Level: 3

Assumed North →

Date: December 2010

Key**Significance Gradings**

Exceptional
All trusses
All purlin
All floorboards
Cathead beam
Timber ceiling lining
IW Internal brick walls
D1, doors, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing)
Doorway 2
W1, double-hung sash, window opening and reveals
W2, double-hung sash, window opening, reveals and bars
Little
Doorway 3
Doorway 4
Doorway 5
Stair
Straw mat ceiling lining
Intrusive
D1 aluminium sliding window
Void
Railing around void
Fixed shelving
Bar
Partitions (P1–P4)

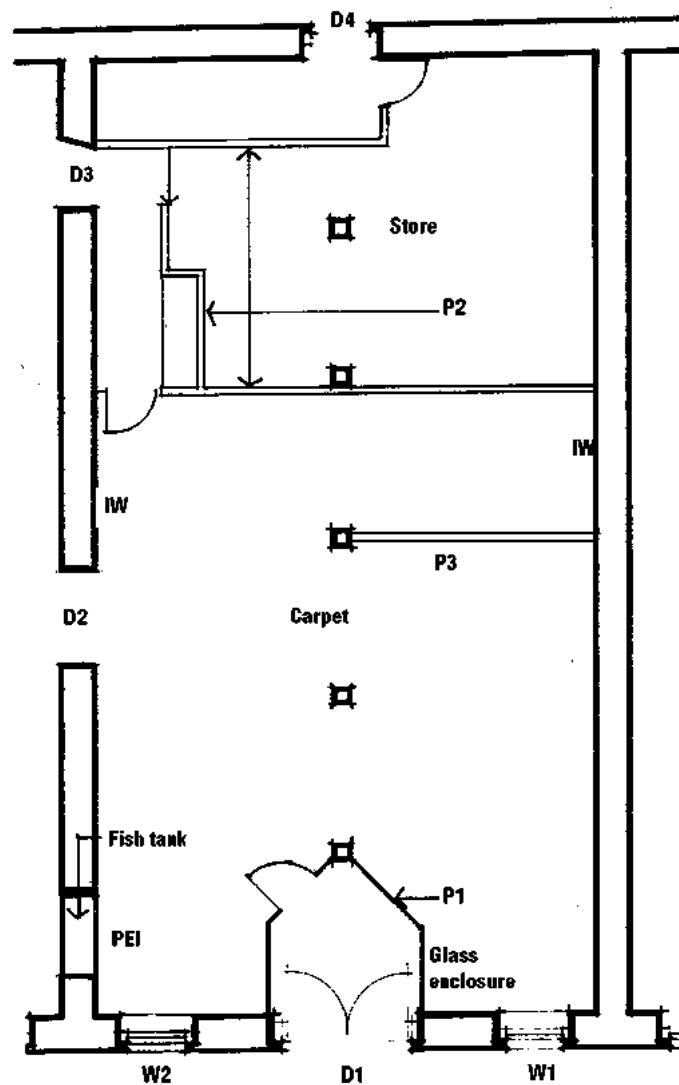
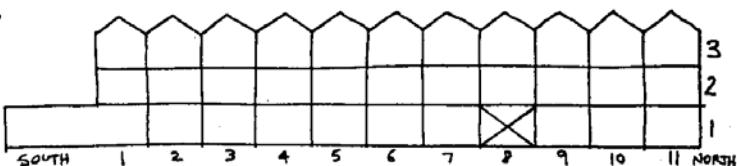
Bay: 8

Level: 1

Assumed North →

Date: December 2010

Key



Significance Gradings

Exceptional

All posts

All beams

All joists

IW Internal sandstone walls

D1, doors, door opening, reveals and gudgeons

W1, window opening, reveals, bars and gudgeons

W2, window opening, reveals, bars and gudgeons

Moderate

Doorway 2

Doorway 3

Doorway 4

Little

Concrete slab floor

Penetration 1

Intrusive

W1 fixed perspex sheet

W2 fixed perspex sheet

Fish tank

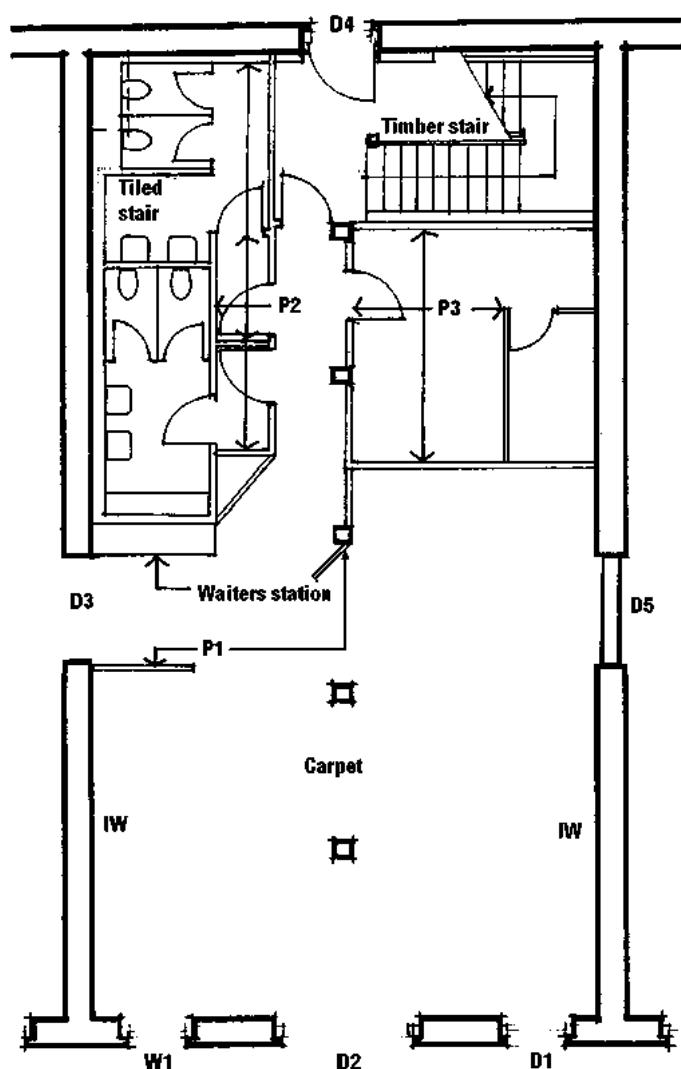
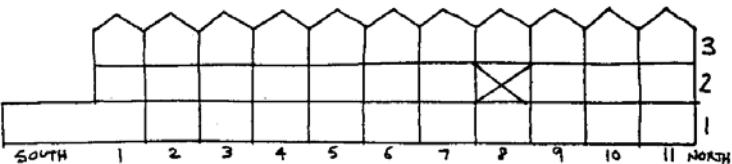
Partitions (P1–P3)

Bay: 8

Level: 2

Assumed North →

Date: December 2010

Key**Significance Gradings**

<i>Exceptional Internal</i>
All posts
All beams
All joists
IW Internal sandstone walls
D1, door opening, reveals and gudgeons (door missing)
D2, door opening, reveals and gudgeons (doors missing)
W1, window opening and reveals
<i>External</i>
Hydraulic ram
 <i>High</i>
Doorway 4
 <i>Moderate</i>
Doorway 3
Doorway 5
 <i>Little</i>
Stair
 <i>Intrusive Internal</i>
D1 aluminium double-hung window
D2 aluminium sliding window
W1 single casement
Toilets
Partitions (P1–P3)
Decorative ceiling joists
<i>External</i>
Wooden bracket

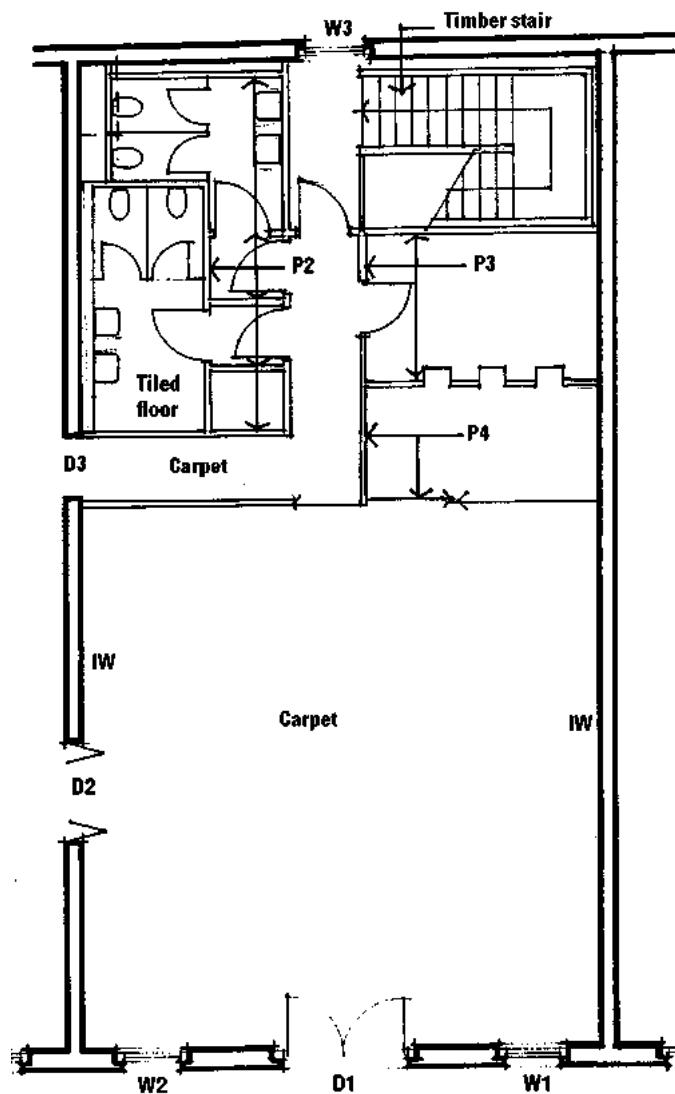
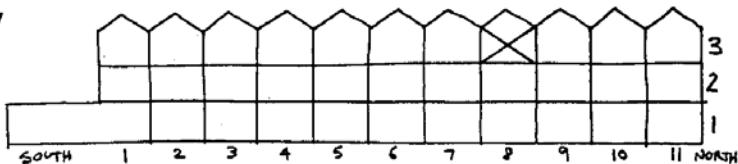
Bay: 8

Level: 3

Assumed North →

Date: December 2010

Key



Significance Gradings

Exceptional
Internal
 All trusses
 All purlins
 All floorboards
 Cathead beam
 Timber ceiling lining
 IW Internal brick walls
 D1, doors, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing)
 W1, double-hung sash, window opening and reveals
 W2, double-hung sash, window opening, reveals and bars
External
 Hydraulic ram

Moderate
 Doorway 2
 Doorway 3

Little
 Stair
 Straw mat ceiling lining

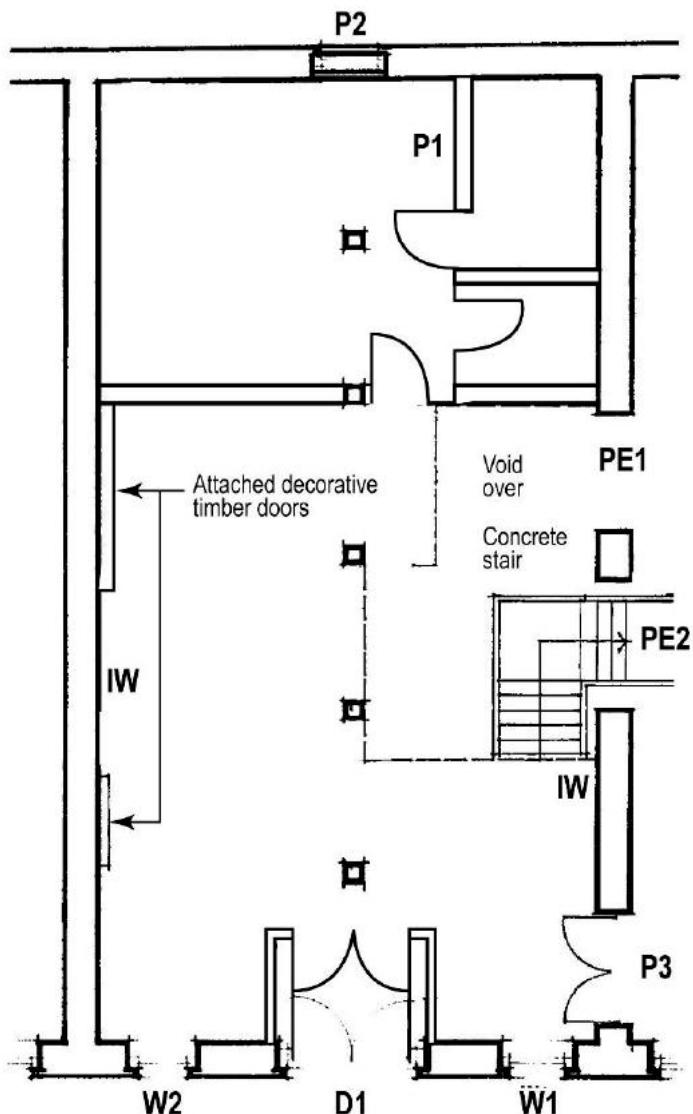
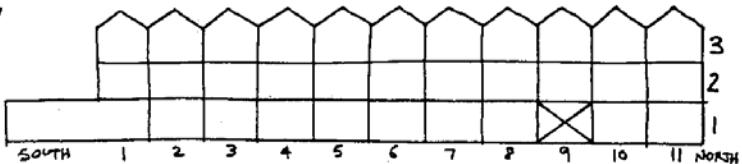
Intrusive
 D1 aluminium sliding window
 Partitions (P1–P4)

Bay: 9

Level: 1

Assumed North →

Date: December 2010

Key**Significance Gradings**

Exceptional

All posts

All beams

All joists

IW Internal sandstone walls

D1, doors, door opening, reveals and gudgeons

W1, window opening, reveals, bars and gudgeons

W2, window opening, reveals, bars and gudgeons

Moderate

Doorway 2

Doorway 3

Little

Concrete slab floor

Intrusive

W1 fixed perspex sheet

W2 fixed perspex sheet

Penetration 1 and 2

Stair

Fixed furniture

Decorative timber doors attached to stone walls

Partitions P1

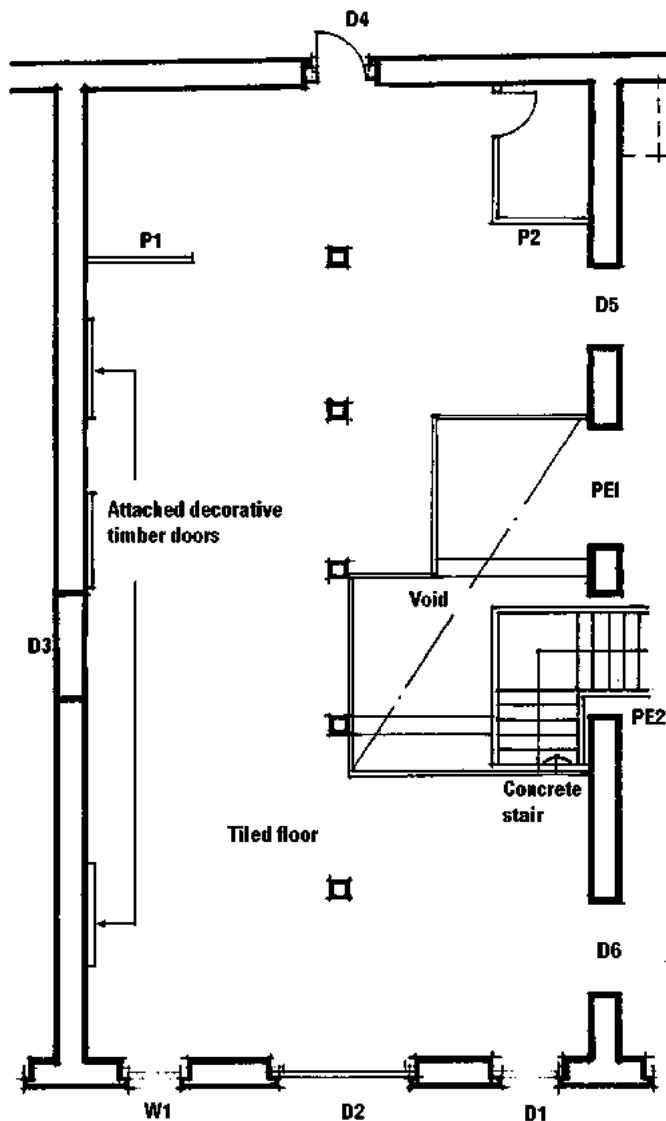
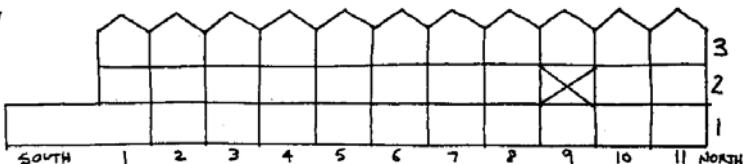
Bay: 9

Level: 2

Assumed North →

Date: December 2010

Key



Significance Gradings

Exceptional
All posts
All beams
All joists

All floorboards (where intact)
IW Internal sandstone walls
D1, doors, door opening, reveals and gudgeons (doors missing)
D2, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing)
W1, window opening, reveals and bars

High
Doorway 4

Moderate
Doorway 3
Doorway 5
Doorway 6
W1, double-hung sash

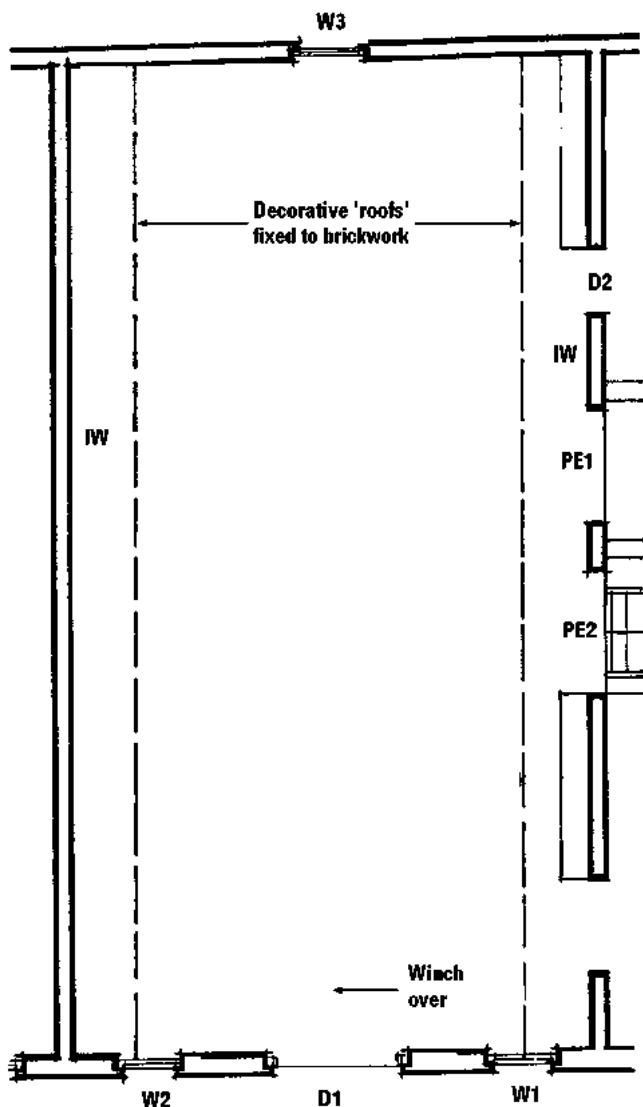
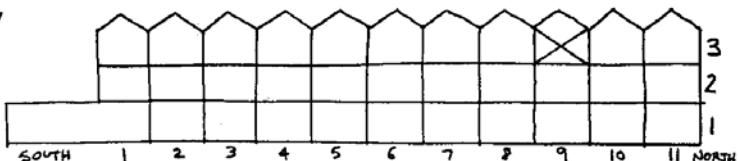
Intrusive
D1 fixed light window
D2 aluminium sliding window
Stair and void
Furniture fixed to walls
Decorative timber doors attached to stone walls
Partitions (P1-P2) — brick
Tiled floor

Bay: 9

Level: 3

Assumed North →

Date: December 2010

Key**Significance Gradings**

- | | |
|--|--|
| Exceptional | |
| All trusses | |
| All purlins | |
| All floorboards (where intact) | |
| Cathead beam | |
| IW Internal brick walls | |
| D1, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing) | |
| W1, double casement, window opening and reveals | |
| W2, double casement, window opening and reveals | |
| W3, window opening, reveals and bars | |
| Winch | |
| Moderate | |
| Doorway 2 | |
| Ceiling lining boards | |
| Intrusive | |
| D1 aluminium sliding window | |
| W3 replacement shutters | |
| Penetration 1 and 2 | |
| Decorative mezzanines fixed to brick walls | |
| Furniture fixed to walls | |
| Tiled floor | |

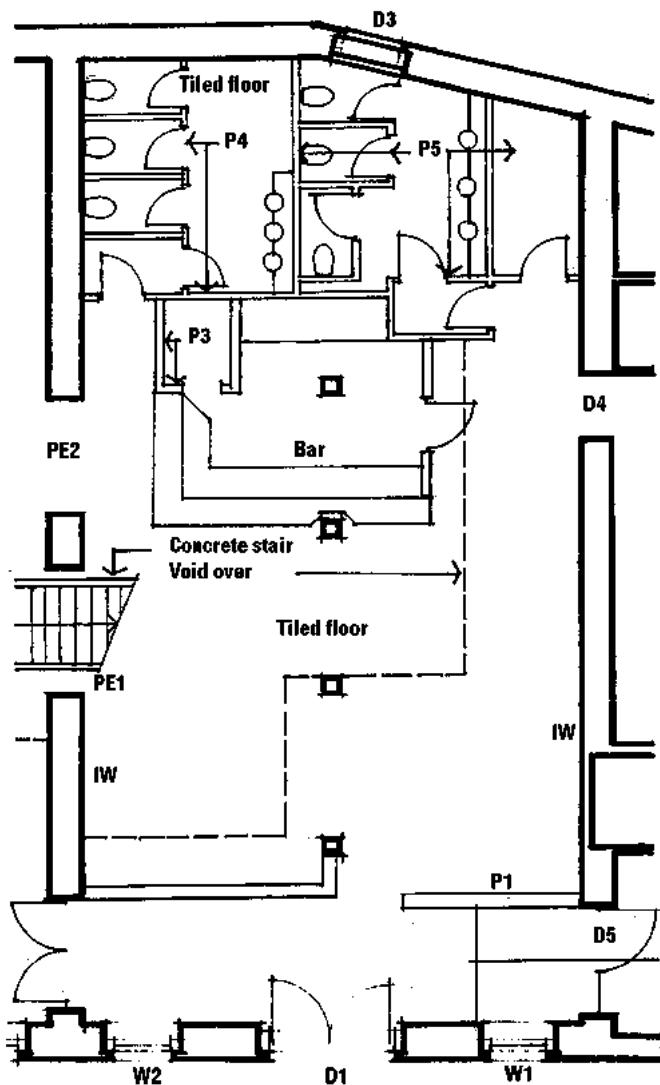
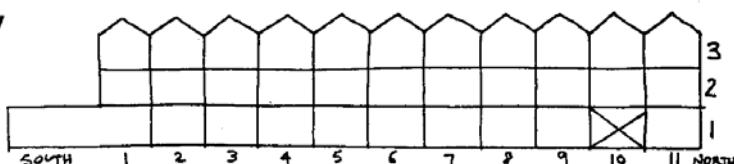
Bay: 10

Level: 1

Assumed North →

Date: December 2010

Key



Significance Gradings

Exceptional

All posts

All beams

All joists

IW Internal sandstone walls

D1, doors, door opening, reveals and gudgeons

W1, window opening, reveals, bars and gudgeons

W2, window opening, reveals, bars and gudgeons

High

Doorway 3

Moderate

Doorway 2

Doorway 4

Doorway 5

Intrusive

W1 fixed perspex sheet

W2 fixed perspex sheet

Penetration 1 and 2

Stair

Bar

Toilets

Raised slab floor

Furniture fixed to walls

Partitions (P1–P5)

Infilling of door opening 3

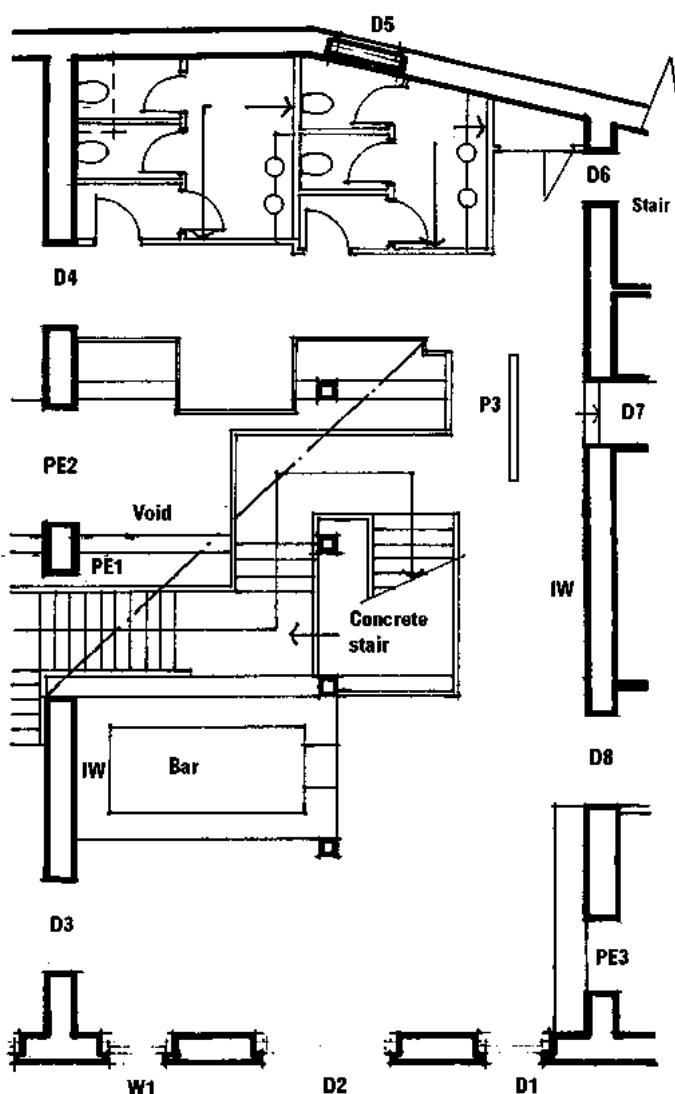
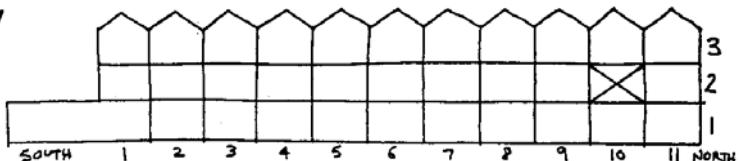
Tiled floor

Bay: 10

Level: 2

Assumed North →

Date: December 2010

Key**Significance Gradings**

Exceptional
 All posts
 All beams
 All joists
 All floorboards (where intact)
 IW Internal sandstone walls
 D1, door opening, reveals and gudgeons (door missing)
 D2, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing)
 W1, window opening, reveals and bars

High
 W1 double-hung sash

Moderate
 Doorway 3
 Doorway 4
 Doorway 5
 Doorway 6
 Doorway 7
 Doorway 8
 Penetration 3

Intrusive
Internal
 D1 fixed glass panel
 D2 aluminium sliding window
 Penetration 1 and 2
 Stair and void
 Toilets
 Bar
 Furniture fixed to walls
 Partitions (P1-P2)
 Tiled floor
External
 Wooden bracket

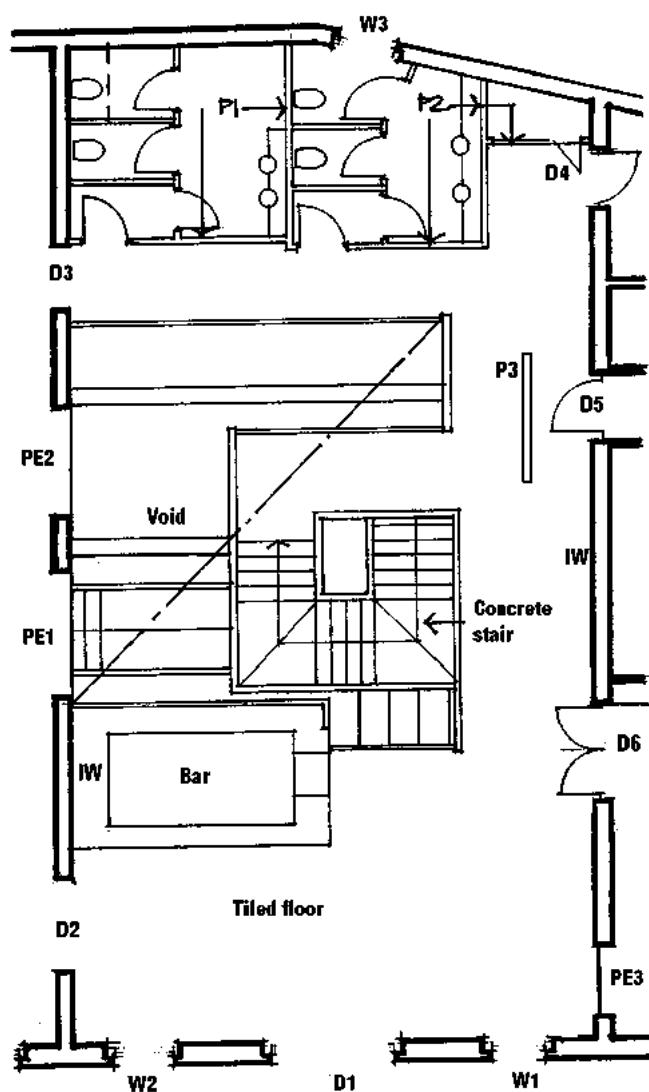
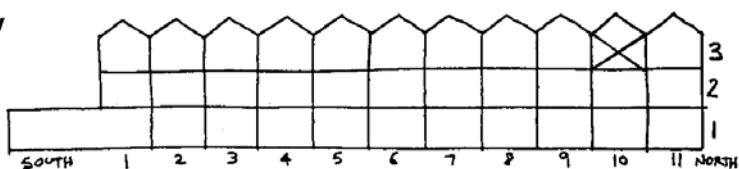
Bay: 10

Level: 3

Assumed North →

Date: December 2010

Key



Significance Gradings

Exceptional

- All trusses
- All purlins
- All floorboards
- Cathead beam
- IW Internal brick walls
- D1, door opening, reveals, gudgeons, goods apron, chains and handles (doors missing)
- W1, double casement, window opening and reveals
- W2, double casement, window opening and reveals
- W3, double-hung sash, window opening, reveals and bars

Moderate

- Doorway 2
- Doorway 3
- Doorway 4
- Doorway 5
- Doorway 6
- Penetration 3

Intrusive

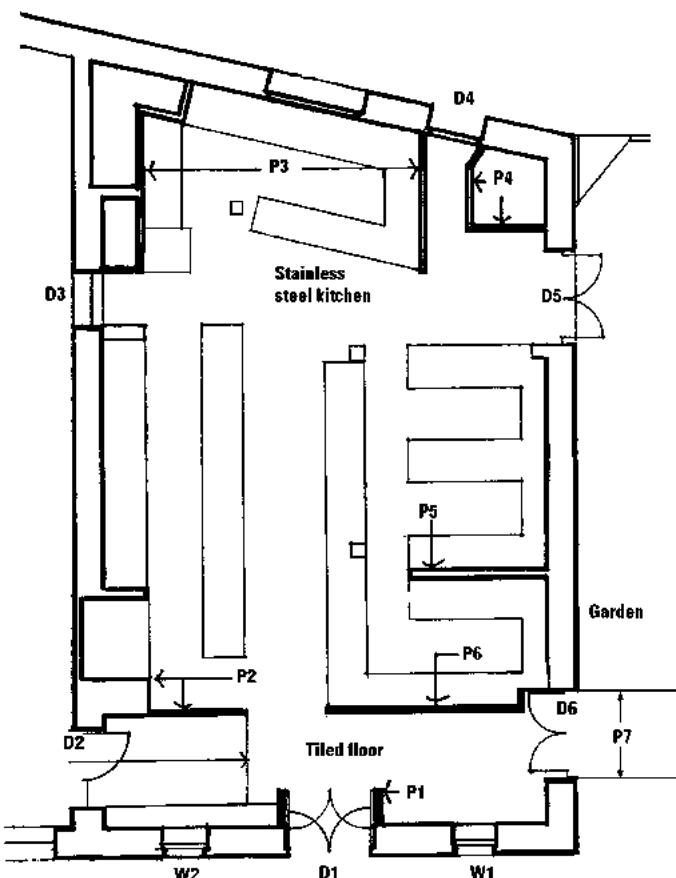
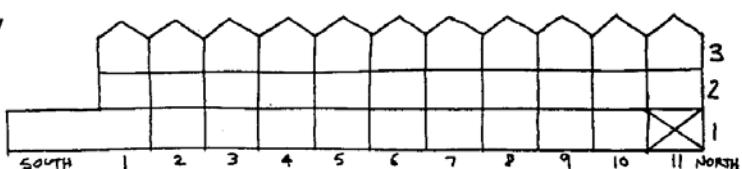
- D1 aluminium sliding window
- Penetration 1 and 2
- Stair and void
- Bar
- Toilets
- Partitions (P1-P2)
- Tiled floor

Bay: 11

Level: 1

Assumed North →

Date: December 2010

Key**Significance Gradings**

Exceptional

High

Intact areas of original walls
 D1, door opening and reveals
 W1, double-hung sash, window opening,
 reveals and bars
 W2, double-hung sash, window opening,
 reveals and bars
 Penetration 1 (PE1)

Moderate

Doorway 2
 Doorway 3
 Doorway 4, bricked-in former opening
 Doorway 5
 Doorway 6

Little

Concrete slab floors

Intrusive

Stainless steel kitchen
 Partitions (P1-P6)

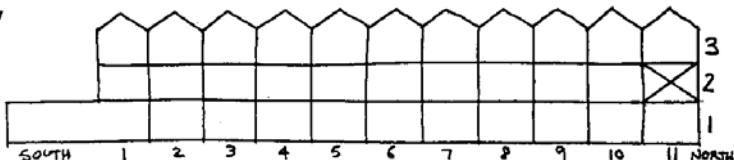
Bay: 11

Level: 2

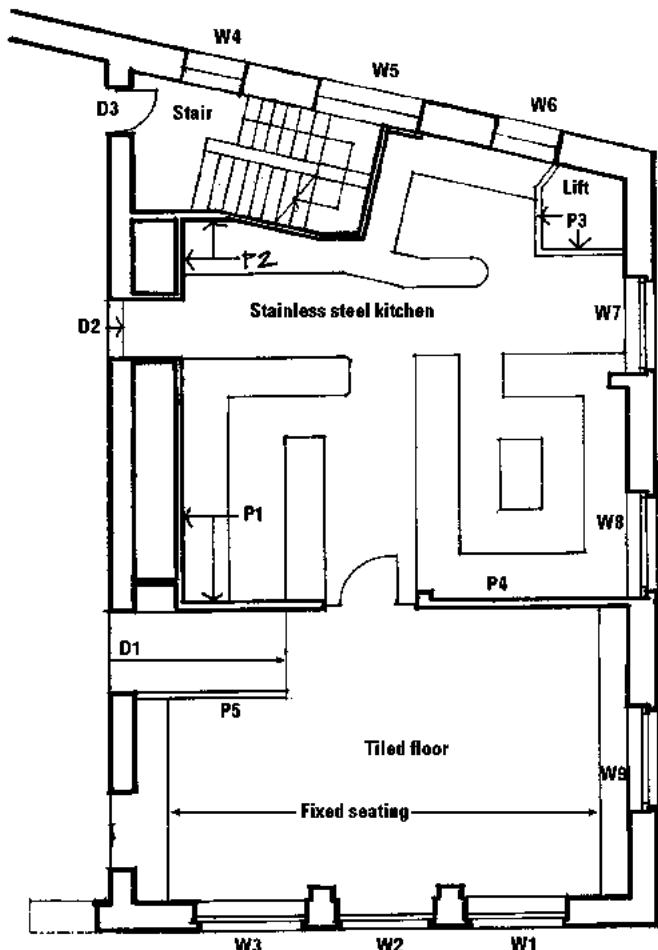
Assumed North →

Date: December 2010

Key



Significance Gradings



High

Intact areas of original walls
W2, window opening, reveals, bars, goods
apron, chains and handles
W4, double-hung sash, window opening,
reveals, architraves and bars
W5, double-hung sash, window opening,
reveals, architraves and bars
W6, double-hung sash, window opening,
reveals, architraves and bars

Moderate

Doorway 1
Doorway 2
Doorway 3
W1, double-hung sash and opening
W3, double-hung sash and opening
W7, double-hung sash and opening
W8, double-hung sash and opening
W9, double-hung sash and opening

Little

Penetration 1
Stair

Intrusive

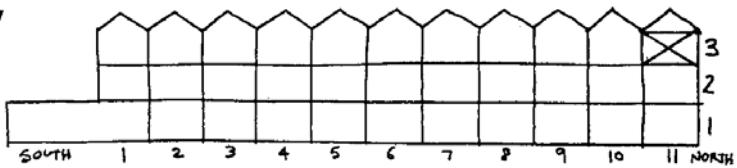
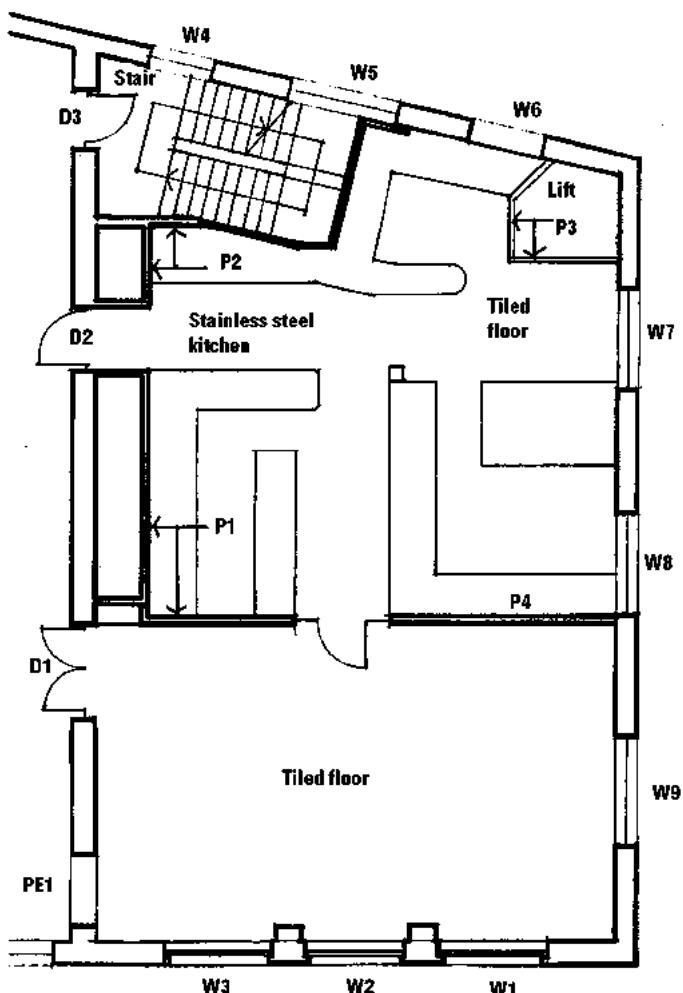
W2 fixed glass panel
Stainless steel kitchen
Furniture fixed to walls
Partitions (P1–P4)
Tiled floor

Bay: 11

Level: 3

Assumed North →

Date: December 2010

Key**Significance Gradings****High**

Intact areas of original walls
W2, window opening, reveals, bars, goods apron, chains and handles
W4, double-hung sash, window opening, reveals, architraves and bars
W5, double-hung sash, window opening, reveals, architraves and bars
W6, double-hung sash, window opening, reveals, architraves and bars

Moderate

Doorway 1
Doorway 2
Doorway 3
W1, double-hung sash and opening
W3, double-hung sash and opening
W7, double-hung sash and opening
W8, double-hung sash and opening
W9, double-hung sash and opening

Little

Penetration 1 (PE1)
Stair

Intrusive

W2 aluminium sliding window
W8 airconditioning unit
Stainless steel kitchen
Furniture fixed to walls
Partition (P1-P4)
Tiled floor

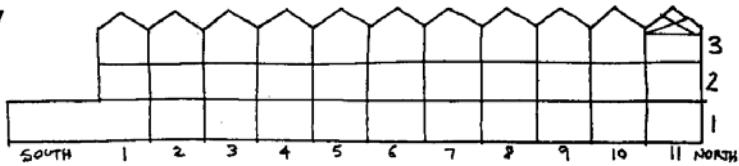
Bay: 11

Level: 4

Assumed North →

Date: December 2010

Key



Significance Gradings

High

All trusses (where intact)
All purlins (where intact)
Cathead beam
Intact areas of original walls
W1, window opening, reveals and bars

Little

Stair

Intrusive

Partitions (P1–P5)
Tiled floor

